

of each settlement to which it applies must be considered when determining whether the discretion is appropriate. In this instance Norwich Drive has the following characteristics:

- Typically 20ha minimum lot size;
- Lots located in a bushland landscape with suitable area for a house plus the ability to undertake rural lifestyle activities associated with rural living lots i.e run a few sheep, cattle or horses;
- Located on a no- through road (Note this is not a through road as claimed in the applicants documents);
- The bushland setting creates a residential environment in a bushfire prone area;
- Lots not connected to reticulated services;
- Subdivision not located within 5km of an existing urban settlement. This is important as typically you would see Rural Living lots with smaller lot sizes i.e 1ha within close proximity to an urban settlement acting as a buffer between urban and rural resource land;
- The settlement is characterised by lots with rocky dolerite soils; and
- Accessed via a no-through from Pateena Road which is partly sealed and partly gravel.

It is clear that the above characteristics of the existing settlement mean that subdivision into 1ha lots will result in the character of the settlement significantly changing and that given the bushland setting on rocky dolerite soils on a no-through road (which is in part gravel), a change of this magnitude is not appropriate and should not be approved.

## 2 Bushfire Prone Areas Code

The application is accompanied by a bushfire hazard assessment, however some of the statements and assessments contained within it are concerning and we submit it would be prudent of Council to have it reviewed prior to making a decision of such importance as the future lot density of Norwich Drive.

The Bushfire hazard assessment states on pg 6 that '*... Norwich Drive may appear to be a No Through Road but there are existing fire trails and access roads that would be able to be utilised by fire appliance access and emergency access during a fire event and provide connectivity and multiple evacuation points. This is consistent with Clause 1.6.1.2 A1 (b) of the Code.*'

I submit that Norwich Drive IS a NO THROUGH ROAD. The public do not have access beyond the end point of Norwich Drive through PRIVATE property. Yes, emergency vehicles could use the private properties my clients own to gain access in an emergency and no landowner permission would be required, but use of these properties as a suitable evacuation point for the general public has not been granted nor would it be appropriate, particularly if the population of Norwich Drive significantly increases, which may well occur if this 1ha subdivision is approved and the 'door' is opened for all lots within Norwich Drive to be subdivided into 1ha parcels.

The evacuation point is shown through two private properties. With respect to the Strathroy property, whilst there is a gravel track for some of the distance from the Midland Highway to the sheepyards, beyond that point the track is little more than a 'goat track' and users need to be familiar with the route to

know where to go. Council needs to consider the implications of a bushfire event in which Norwich Drive is cut-off and no access back to Pateena Road can occur. There would be a situation of panic amongst residents (and there will be significantly more residents if the 1ha precedent is set) trying to use an escape route through multiple properties when they are not familiar with the route nor gate locations and it is not clearly marked. A mass evacuation through the Strathroy property under these circumstances would also have impacts for the property owners and their management of their property and livestock in such an event.

I have included the relevant excerpt from the Bushfire Code and provided an alternate assessment. Whilst I am not an accredited bushfire practitioner, I believe that the assessment has not fully taken into account the objectives relating to E1.6.1.2 and Council is urged to have it reviewed by a bushfire accredited practitioner. It is important to note that neither of my clients, whose properties are the subject of the emergency access gave permission for the bushfire practitioner to be on their property to view the suitability of the access so it is questioned how an assessment can be made as to its appropriateness.

**E1.6.1.2 Subdivision: Public access**

*Victoria's Bushfire Prone Areas Planning Scheme 2019*

<p><b>Objective:</b>                  Access roads to, and the layout of roads, tracks and trails, in a subdivision:</p> <ul style="list-style-type: none"> <li>• allow safe access for occupants, fire fighters and emergency service personnel;</li> <li>• provide access to the bushfire-prone vegetation that enables both property to be defended when under attack and hazard management procedures to be undertaken;</li> <li>• are designed and constructed to allow for fire fighting vehicles to be manoeuvred;</li> <li>• provide access to water supplies for fire-fighting vehicles; and</li> <li>• are designed to allow connectivity, and where needed, offering multiple evacuation points.</li> </ul>	
<b>Acceptable Solutions</b>	<b>Performance Criteria</b>
<p>A1                  (a) The TFS or an accredited person certifies, having regard to the objective, that there is an insufficient increase in risk from bushfire to warrant specific measures for public access in subdivision for the purposes of fire fighting; or                  (b) A proposed plan of subdivision showing the layout of roads and fire trails, and the location of private access to building areas, is included in a bushfire hazard management plan approved by the TFS or accredited person as being consistent with the objective; or                  (c) A proposed plan of subdivision:</p> <ol style="list-style-type: none"> <li>1. (i) shows that at any stage of a staged subdivision, all building areas are within 200m of a road that is a through road; and</li> <li>2. (ii) shows a perimeter road, private access or fire trail between the lots and bushfire-prone</li> </ol>	<p>P1 A proposed plan of subdivision must show the road layout and demonstrate that safe access and egress for occupants, fire-fighting vehicles and emergency service personnel is achieved through the use of appropriate design measures, including:</p> <ol style="list-style-type: none"> <li>(i) two-way traffic;</li> <li>(ii) provision of passing bays;</li> <li>(iii) geometry, alignment and slope;</li> <li>(iv) use of through roads to provide connectivity;</li> <li>(v) limits on the length of cul-de-sacs and provision of turning areas;</li> <li>(vi) access to water supply points for fire fighting vehicles;</li> <li>(vii) perimeter access;</li> <li>(viii) fire trails.</li> </ol>

<p>vegetation, which road, access or trail is linked to an internal road system; and.</p> <p>3. (iii) shows all roads as through roads unless:</p> <ol style="list-style-type: none"> <li>1. they are not more than 200m in length and incorporate a minimum 12m outer radius turning area; or</li> <li>2. the road is located within an area of vegetation that is not bushfire-prone vegetation; and</li> </ol> <p>(iv) shows vehicular access to any water supply point identified for fire fighting.</p>	
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The bushfire assessment has stated that the proposal meets (b) of the above clause on the basis that Norwich Drive is not a no through road due to the existence of fire trails. We accept that use of the Strathroy and Mr Gardiner's property for access by emergency services would occur in a fire event but in determining that the plan of subdivision is consistent with the standard, the practitioner needs to certify that it meets the objectives. I submit that the objectives are not met, specifically the objective that states that access roads, tracks and trails '*are designed to allow connectivity, and where needed, offering multiple evacuation points.*'

There is not multiple evacuation points for the public in the Norwich Drive subdivision and the proposed two lot subdivision does not remedy this situation. Norwich Drive is a no through road and for the reasons stated above, reliance on Mr Gardiners and the Strathroy property as an evacuation point is problematic and nor has there been landowner consent for use of their properties as access during an emergency. My clients are concerned as to how a bushfire practitioner can certify that a fire trail is suitable for evacuation when he hasn't actually seen the condition of the trail as neither Mr Gardiner or Mr Grubb have given permission for anyone to assess the suitability of their land for bushfire trails.

The trails certainly are not suitable for public evacuation and it is questioned as to whether they are up to standard for use by emergency vehicles and whose responsibility is it to ensure they are maintained.

If the bushfire practitioner sign-off in relation to this clause is found to be deficient then the application must be assessed against A1 (c) which clearly requires subdivision to have access to a through road which it does not. I would also submit that the application would fail to meet the corresponding performance criteria on that basis and the fact that the 'fire trail' wouldn't meet relevant standards. In this instance Council must refuse the application.

### 3 Rural Living/Rural Resource Zone conflicts

My client Mr Peter Hughes' farming property 'Jessiefield' shares two boundaries with the subject site. Mr Hughes runs a viable agricultural enterprise with a mixture of livestock grazing and cropping on his property, including the lot immediately to the north-east which is within the Rural Living Zone. His land to the east of Norwich Drive is within the Rural Resource Zone and shares boundaries with a large number of the Norwich Drive lots. He is very concerned that Norwich Drive is being considered suitable for subdivision into 1ha lots and the impact the creation of even more adjoining lots will have on his agricultural operation.

His family has farmed on the property for 85 years and has had ongoing issues with adjoining rural living lots. These issues are summarised as follows:

- My client runs a stud cattle and sheep operation and biosecurity is very important. He is faced with the issue of neighbouring livestock escaping onto his land and because the owners of these livestock don't have the means (ie a motorbike) to muster them, he has to spend time getting them back to their land. In some cases landowners have not been educated in the management of livestock. Whenever this occurs, my client's own livestock are put at risk of diseases including Johnes, footrot and lice, which if occur in his flock, put his whole business at risk. Additional lots means more instances of this occurring resulting in greater potential for bio security problems and disease in livestock.
- A greater number of rural living lots adjacent to a working farming property means a greater risk of livestock attack by dogs.
- A more dense settlement leads to conflict with spraying, noise, irrigation points.
- If owners have small blocks and run cattle, they tend not to have a dedicated bull paddock away from the boundary fence meaning the onus is all on my client to keep his cows away from paddocks adjoining rural living lots which leads to not being able to utilise his land properly.

Council should consider requiring all Rural Living lots that adjoin Rural Resource lots to be double fenced to reduce the occurrence of the above incidents.

#### **4 Diversity of Rural Living Lots on the market**

In a broader strategic sense, increasing the density of the Norwich Drive subdivision will reduce the diversity of Rural Living lots available on the market. Whilst a 1ha minimum is suitable in some areas (particularly on the urban fringe), the lot size means the ability of owners to run lifestyle rural operations (i.e keep a few sheep, cattle or horses) is very limited. The community should be afforded the choice of rural lifestyle lots of a larger size such as is the current pattern in Norwich Drive (20ha) to enable them to undertake hobby farming operations.

#### **5 Conclusion**

The application for a two lot subdivision at 372 Norwich Drive including one lot of 1ha should not be approved by Council on the basis that it isn't in keeping with the existing character of the settlement, is based on information in relation to bushfire management which is tenuous and if approved will create a precedent for 1ha lots in the settlement.

I urge Council to carefully consider this application and the broader implications it will have for the community including my client, Mr Peter Hughes whose farming property already abuts a large number of rural living lots.

1-305

Sincerely

A handwritten signature in black ink, appearing to be 'CL' with a long horizontal stroke extending to the right.

**Chloe Lyne (BUPD)**  
Senior Planner

8<sup>th</sup> December, 2014

The General Manager  
Northern Midlands Council  
PO Box 156  
LONGFORD TAS 7301

NORTHERN MIDLANDS COUNCIL					
Location					
File No.					
Property					
Attachments					
REC'D 9 DEC 2014					
GM			MYR		
P&DM			CRS		
CSM			PLAN		✓
E&DM			SLD		
WM			HLT		
HR					

Dear Sir

**LETTER OF REPRESENTATION - PLANNING APPLICATION P14-317  
2 LOT SUBDIVISION - 372 NORWICH DRIVE, LONGFORD**

I would like to lodge an objection to the above 2 lot subdivision in Norwich Drive.

Since purchasing our acreage in 2008 many blocks have been subdivided. However all have met the planning criteria of maintaining the required size, not adversely affecting the amenity of, or were out of character with the surrounding developments and streetscape.

The proposed dwelling is not of a concern our issue is the size of the lot as it is not consistent with all other developments in the area. The creation of a 1ha lot is not maintaining the current size requiring lots to be a minimum of 10ha.

All blocks consist of small hobby farms with families enjoying a rural lifestyle, choosing this environment over a suburban lifestyle. Allowing the proposed 1ha lots, will be drastically altering the streetscape and rural atmosphere of this area.

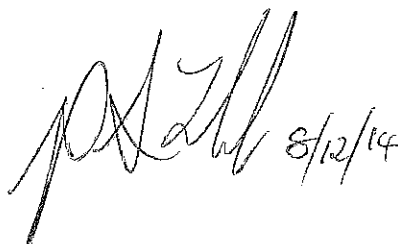
As residents in this area for the past 3 years, we believe that Norwich Drive does not have the facilities to accommodate small lot developments as allowed in Devon Hills; another Northern Midlands area. Norwich Drive is not fully sealed and the maintenance on the current gravel road is minimal.

We would like you to consider the relevant points made in this submission regarding the proposal of small lot subdivisions.

I will be interested in the outcome of the council so I can subdivide my block, upsetting the rural setting enjoyed by us here therefore upsetting the neighbor's then selling up.

Kind Regards,

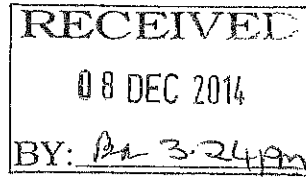
P S Tubb  
359 Norwich Drive,  
Longford, 7301



Mobile: 0419518199

**Strathroy Pastoral**

ABN: 95 312 873 319

**P O Box 102****Evandale 7212****E: [grubb.fernhill@gmail.com](mailto:grubb.fernhill@gmail.com)****M: 0407 515781****6<sup>th</sup> December, 2014**

The General Manager  
Northern Midlands Council  
P O Box 156  
Longford, 7301 TAS

Dear Sir,

NORTHERN MIDLANDS COUNCIL											
Location											
File No.											
Property											
Attachments											
REC'D 8 DEC 2014											
GM				A							A
P&DM							MYH				
CSM							CPS				
P&DM							PLAN				✓
WM							BLD				
HR							PLT				

**Re:- P14-317 Application for Two lot subdivision 372 Norwich Drive, Longford Representation**

I refer to the above application for subdivision at Norwich Drive, Longford.

Planning Consultant, Mrs Chloe Lyne (BUPD) will be making a formal submission on our behalf. However, as landowner of property "Strathroy" located at end of Norwich Drive, I felt it important to also make my own personal representation. I am concerned that in the planning application my property has been shown as an alternate access point "with existing fire trails and access roads that would be utilised for fire appliance access and emergency access during a fire event and provide connectivity and multiple evacuation points."

I understand that, and have no problem with emergency vehicles using private property without permission in order to access areas in emergency situations such as bushfire. However, I have two main concerns with this planning application and accompanying bushfire hazard assessment;

1. My property was shown as an alternate access point without any contact from the applicant, or their representative seeking my consent.
2. As landowner at end of Norwich Drive, I am concerned our track isn't suitable for fire egress and this application raises legal issue of responsibility for getting road up to "fire trail standard" and maintaining track to same standard. Furthermore, to access our existing track the public would have to enter the property of Mr Tim Gardiner (Lot No.14). The only access then onto my property is through a 12 foot gate into our sheep yards which would then require navigation through a further three gates to get onto existing track.

I submit that Norwich Drive is a NO THROUGH ROAD, and that the general public have not been granted permission to enter private property as a suitable evacuation point.

I urge council to take the above points into consideration when assessing this application for subdivision.

Yours Sincerely,

*B.P. Grubb***Beaumont Percival Grubb**

**From:** Hugh Mackinnon [mailto:mountford@netspace.net.au]  
**Sent:** Tuesday, 9 December 2014 1:21 PM  
**To:** Des Jennings  
**Subject:** Objection re P14-317372 Norich Drive

Dear Mr. Jennings,

This Norich Drive subdivision of a 20 ha lot into 1 ha and balance is precedent that abrogates" the purpose" of the adjacent Rural Resource Zone...26.1.2 and 26.1.3 where the longstanding productive farming units will be placed in jeopardy from the de facto creation of a new un-serviced settlement that will over time be subdivided into many 1 ha plus lots...this is in contravention of NMC policy, see para. 3.6.2 of the Scheme.

To allow this 1 ha subdivision will over time create a settlement that is un-serviceable cluster of rural living in a highly fire prone area. To allow this subdivision is in stark contravention of RLZ 13.1.3 as it does not further a strategic location to maximise efficiencies for services and infrastructure.

The 1 ha lot in question -and the inevitable further subdivision due to this precedent- does not meet the Bushfire Hazard Code E 1.5.1.1&2 as there is no acceptable escape strategy as it is a cul- du- sac in a highly combustible bush area and there are insufficient water supply points for bush fire fighting purposes.

2.2.2.6 of the principles and strategies is to "ensure undiminished availability" and inter alia "protecting the rural resources of the Municipality". To promote a further and new rural settlements is conta to the forgoing, as well as in contravention of the Settlement policy 2.2.2.1c ...." to actively discourage residential development" ... "unless clearly required for rural enterprise".

To ensure the long term sustainability and the rural resource, as well as negating future conflict and fettering, due to inappropriate development that conflicts with as of right agricultural uses now and in the future, I request that this application and others similar in the future be Refused.

Yours Sincerely,

Hugh C Mackinnon  
Director

CUILLINS PTY LTD  
"Mountford"  
390 Illawarra Road  
Longford 7301 Tasmania Australia

Mobile: +61 (0) 427 911 832  
Email: [mountford@netspace.net.au](mailto:mountford@netspace.net.au)



TP & PM Cotton  
346 Norwich Drive  
LONGFORD TAS 7301

8 December 2014

The General Manager  
Northern Midlands Council  
PO Box 156  
LONGFORD TAS 7301

NORTHERN MIDLANDS COUNCIL					
Location					
File No.					
Property					
Attachments					
REC'D 9 DEC 2014					
	I	A		I	A
GM			MYR		
P&DM			CBS		
CSM			PLAN		✓
E&DM			BLD		
WM			HLT		
HR					

Dear Sir,

**LETTER OF REPRESENTATION - PLANNING APPLICATION P14-317  
2 LOT SUBDIVISION - 372 NORWICH DRIVE, LONGFORD**

We would like to lodge an objection to the above 2 lot subdivision in Norwich Drive.

When we received the notification in the mail regarding the above subdivision it took us by surprise due to the historical property nature of Norwich Drive. Our land was purchased in 1997 and we were under the belief that blocks on Norwich Drive were not to be subdivided under 10 ha (previous scheme, **Low Density Residential Zone** and current scheme, **Rural Living Zone - Acceptable Solutions**).

The proposed dwelling is not of a concern, our issue is the size of the lot as it is not consistent with all other developments in the area. The creation of a 1ha lot is not maintaining the current setting requiring lots to be a minimum of 10ha.

Norwich Drive is a rural lifestyle area which is surrounded by profitable working farms. All blocks consist of small hobby farms with families enjoying a rural lifestyle, choosing this environment over a suburban lifestyle. Allowing the proposed 1ha lot will be drastically altering the character and rural atmosphere of this area.

This application is not consistent with the zone purpose:

13.1.1.1

*(To provide for residential use or development on large lots in a rural setting where services are limited) and*

13.1.1.2

*(To provide for compatible use and development that does not adversely impact on residential amenity).*

After reading this section of the scheme it is obvious that this application can only be assessed or passed by relying on the Performance Criteria and not the Acceptable Solutions section (highlighted in orange). I strongly believe that a mistake has occurred in the Performance Criteria section with the use of the word **OR** instead of **AND** (highlighted in the attached extract below in red).

13.4.2 Subdivision

**Objective**

To ensure that subdivision:

- a) Provides for appropriate wastewater disposal, and stormwater management in consideration of the characteristics or constraints of the land; and
- b) Provides area and dimensions of lots that are appropriate for the zone; and
- c) Provides frontage to a road at a standard appropriate for the use; and
- d) Furthers the local area objectives and desired future character statements for the area, if any.

**Acceptable Solutions**

A1.1 Each lot must:

- a) have a minimum area in accordance with Table 13.5.1.1 below; or Table 13.5.1.1 – Lot Size

Blackwood Creek, Deddington, Norwich Drive, Pateena Road	10 ha
Caledonia Drive, Kalangadoo	2 ha

- b) be required for public use by the Crown, an agency, or a corporation all the shares of which are held by Councils or a municipality; or
- c) be for the provision of utilities; or
- d) be for the consolidation of a lot with another lot with no additional titles created; or
- e) be to align existing titles with zone boundaries and no additional lots are created.

A1.2 Each lot must have new boundaries aligned from buildings that satisfy the relevant acceptable solutions for setbacks.

**Performance Criteria**

P1 Each lot must:

- a) be to facilitate protection of a place of Aboriginal, natural or cultural heritage; or
- b) provide for each lot, sufficient useable area and dimensions to allow for:
  - i) a dwelling to be erected in a convenient, appropriate and hazard free location; and
  - ii) appropriate disposal of wastewater and stormwater; and
  - iii) on-site parking and manoeuvrability; and
  - iv) adequate private open space; and
  - v) vehicular access from the carriageway of the road to a building area on the lot, if any; or
- c) be consistent with the local area having regard to:
  - i) the topographical or natural features of the site; and
  - ii) the ability of vegetation to provide buffering; and
  - iii) any features of natural or cultural significance; and
  - iv) the presence of any natural hazards; and
  - v) local area objectives, if any; and
- d) not create lots less than 1.0ha.

Since purchasing our acreage in 1997 many blocks have been subdivided. However all have met the planning criteria of maintaining the required size, not adversely affecting the amenity of, or were out of character with the surrounding developments.

As residents in this area for over 17 years, we believe that Norwich Drive does not have the facilities to accommodate small lot developments as allowed in Caledonia Drive, another Rural Living area. Norwich Drive is not fully sealed and the maintenance on the current gravel road is minimal. We do not have a reticulated water supply and there is only one entry/exit. These issues could have a tremendous negative effect on the residents of Norwich Drive under certain circumstances. For example, since residing here there have been numerous large bush fires requiring the Fire Brigade to send multiple units. If this Council allowed an increase in the population along Norwich Drive, there would be an increase amount of unnecessary risk to human life in the case of a bush fire, which commonly threatens this area during summer. Gorse, vegetation and large gum trees are prevalent in the area. This is a deadly combination when mixed with a flame and strong winds. Depending on the location of the fire and taking into consideration only having one exit, there could be human life lost due to panic and inability to escape.

We would like you to consider the relevant points we have made in this submission regarding the proposal of a small lot subdivision and possibility of future subdivisions.

Kind Regards,



T P Cotton

&



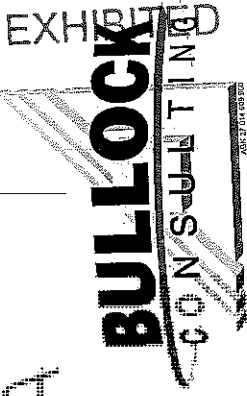
P M Cotton

**PLAN 3**

**PLANNING APPLICATION P14-235  
115 FAIRLOUGH STREET, PERTH**

**ATTACHMENTS**

- A Proposal plans & reports**
- B Representation**
- C TasWater consent**
- D Works & Infrastructure Department referral**
- E TasRail submission**



ARCHITECTS  
SURVEYORS  
ENGINEERS



APPROVED COMPANY  
ABP No. C04974 Architectural  
ABP No. C01628 Structural/Civil

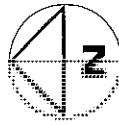
Postal Address  
PO Box 53  
Riverview  
Tasmania 7260

Tenax Suite 109  
The Charles  
Street  
Lancaster Tasmania  
Ph: (03) 6332 9300  
Fax: (03) 6331 7189

57 Beal Street  
Devonport Tasmania  
Ph: (03) 9424 7161  
Fax: (03) 9424 7161

e: [sales@bullock.com.au](mailto:sales@bullock.com.au)  
w: [www.bullock.com.au](http://www.bullock.com.au)

1-313



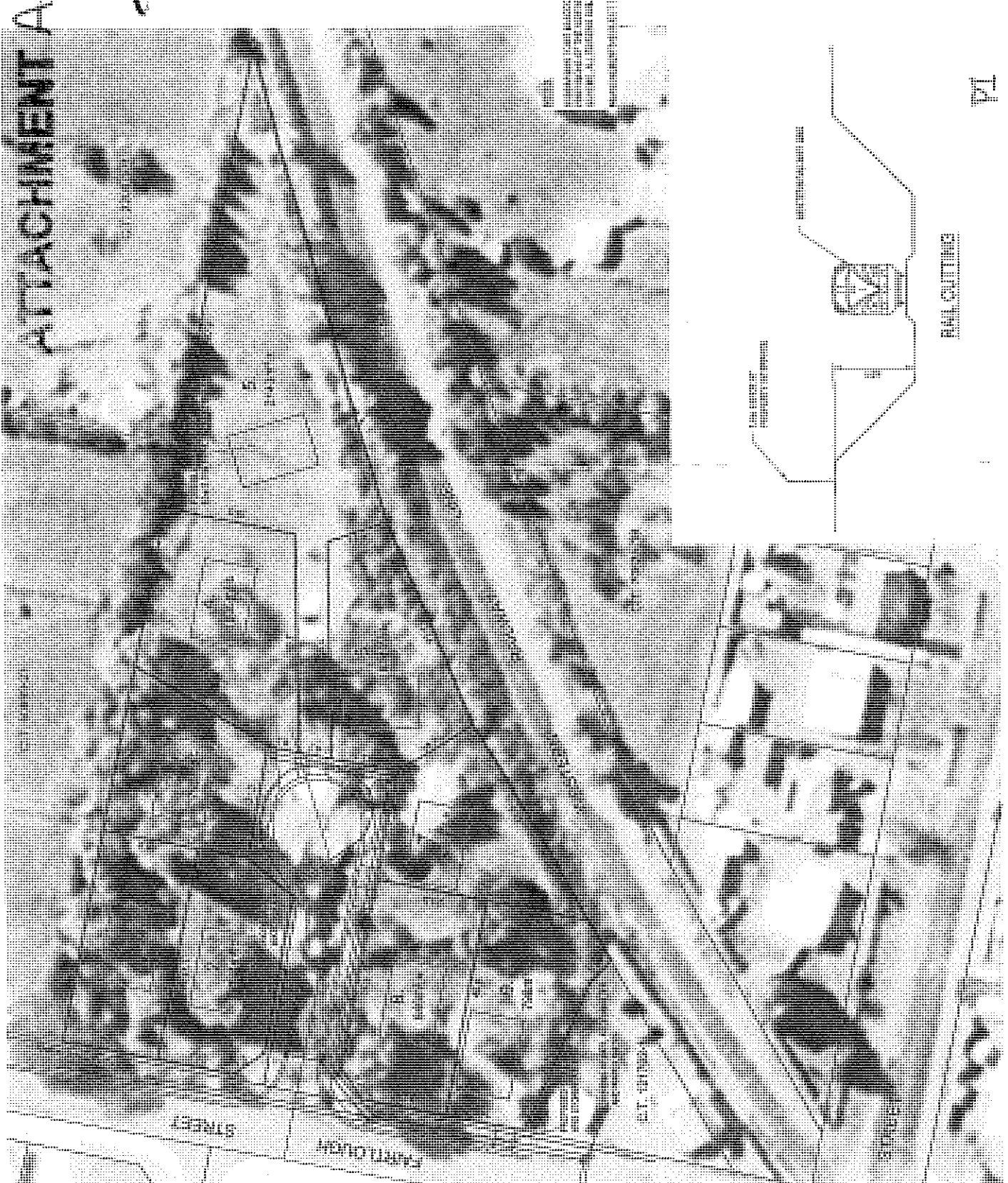
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NO.	DATE	REVISION	BY
1	14/11/2014	INFORMATION	A
2	14/11/2014	DEVELOPMENT APPLICATION	A

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ALL WORK TO BE CARRIED OUT IN ACCORDANCE WITH THE RELEVANT STANDARDS AND REGULATIONS.  
THE USER OF THIS DRAWING IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AUTHORITIES.

## LOT SUBDIVISION  
G.I. 207804-1  
115 FAIRTROUGH ST  
PERTH  
HERVAN DEVELOPMENTS  
SUBDIVISION PROPOSAL  
PLAN  
DRAWN: J.E.P. CHECKED: G.L.E.  
AT A242 DRAWING SHEET  
DATE

PI  
DRAWING NO: 14.155 DRAWING REV: PO1 A



6ty Pty Ltd  
ABN 27 014 609 800

Postal Address  
PO Box 63  
Riverside  
Tasmania 7250  
W 6ty.com.au  
E admin@6ty.com.au

Tamar Suite 103  
The Charles  
287 Charles Street  
Launceston 7250  
P (03) 6332 3300

57 Best Street  
PO Box 1202  
Devonport 7310  
P (03) 6424 7161

## 9 Lot Subdivision

105 Fairtlough Street  
Perth

EXHIBITED

## Supporting Submission

Prepared for:  
Northern Midlands Council

Issue - 01  
Date - November 2014  
Project name - 9 Lot Subdivision - 105 Fairtlough Street, Perth  
Project number - 14.155  
Document - I:\2014\14155\1 administration\6 authorities\2 council\planning application\r 14-11-04 da supporting submission.docx

D1

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## 1. INTRODUCTION

6ty° Pty Ltd is engaged by Shevran Developments Pty Ltd to prepare a planning application for subdivision of land at 105 Fairtlough Street, Perth.

This supporting submission is intended to assist with Council's assessment of the application against all applicable standards of the Northern Midlands Interim Planning Scheme (NMIPS).

### 1.1 Certificate of Title

The application applies to land identified on Certificate of Title 207804/1 (refer to Appendix A).

### 1.2 Planning Instrument

The planning instrument subject to this application is the Northern Midlands Interim Planning Scheme (NMIPS)

### 1.3 Zone and Overlay Map

The site is zoned General Residential under the NMIPS. The site is not subject to any Overlay Maps (refer to Figure 1).

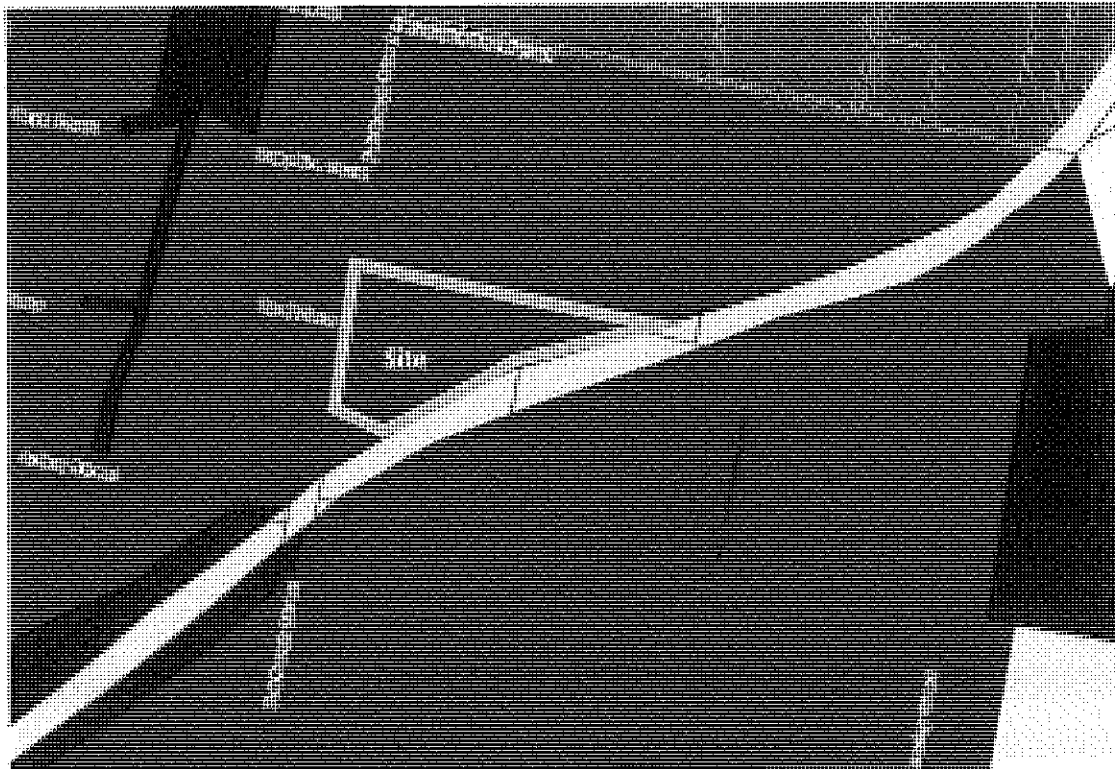


Figure 1: Zone and Overlay Map, Northern Midlands Interim Planning Scheme (source: theLIST)



## 2. PROPOSAL – 9 LOT SUBDIVISION

The application is seeking approval to create 9 serviced lots in accordance with the Proposal Plan drawing number 14.155 P01. This will involve the demolition of all existing buildings on the site. The lots proposed by this subdivision are intended for residential use. These are summarised below.

**Lot 1** will have:

- A lot area of approx. 720m<sup>2</sup>; and
- 18m frontage to Fairtlough Street.

**Lot 2** will have:

- A lot area of approx. 741m<sup>2</sup>; and
- A frontage of 14.5m to Fairtlough Street and 29.7m frontage to the proposed new road.

**Lot 3** will have:

- A lot area of approx. 722m<sup>2</sup>; and
- A frontage of 20.2m to the proposed new road.

**Lot 4** will have:

- A lot area of approx. 1035m<sup>2</sup>; and
- A frontage of 6.1m to the proposed new road.

**Lot 5** will have:

- A lot area of approx. 2143m<sup>2</sup>; and
- An access strip frontage of 6.1m to the proposed new road.

**Lot 6** will have:

- A lot area of approx. 854m<sup>2</sup>; and
- A frontage of 12.7m to the proposed new road.

**Lot 7** will have:

- A lot area of approx. 765m<sup>2</sup>; and
- A frontage of 20.5m to the proposed new road.

**Lot 8** will have:

- A lot area of approx. 846m<sup>2</sup>; and
- A frontage of 16m to Fairtlough Street and 15.9m frontage to the proposed new road.

**Lot 9** will have:

- A lot area of approx. 798m<sup>2</sup>; and
- 14.8m frontage to Fairtlough Street.

All lots will be connected to reticulated water and sewer system. Stormwater disposal will be in accordance with the Proposal Plan, drawing number 14.155 P01.

### 3. SITE AND SURROUNDING USES

The site comprises an area of 9,800m<sup>2</sup> and has frontage to Fairtlough Street of more than 60m. The irregular shaped parcel of land is located within the urban growth boundary area of Perth, north of Arthur Street between the Midland Highway and the South Esk River. This area is largely developed for residential purposes, characterised by single dwellings constructed on a range of lot sizes interspersed with multiple dwellings.

The lot boundaries of the site are defined by:

- The Western Railway Line to the south;
- Fairtlough Street and residential development to the west; and
- Two residential lots with areas of approximately 1.5ha each to the north.

The land west of Fairtlough Street, on the opposing side of the site, is characterised by residential lots at suburban densities consistent with the objectives of the General Residential zone. Land south of the site, divided by the railway cutting, also comprises residential development on a range of lot sizes. The lot areas of the land bounded by Arthur Street, Fairtlough Street, Seccombe Street East and Mulgrave Street range from around 600m<sup>2</sup> to over 1000m<sup>2</sup>. These are primarily developed with single dwellings interspersed with multiple dwellings.

Pedestrian and cycle linkages from the site to the public open spaces and local businesses of Perth is provided through the established road network. Pedestrian movement is also facilitated through the urban area to the west of the site through a series of cul-de-sacs connecting a centralised walkway. This provides accessibility to the public open space on Seccombe Street East. The Perth Riverbank Reserve is also within walking distance of the site, providing opportunities for passive and active recreational activity.

A single storey dwelling and associated outbuildings are contained on the site. The dwelling is setback approximately 50m from Fairtlough Street. The driveway servicing the dwelling is located in the middle of the existing lot, perpendicular to Fairtlough Street.

The site is serviced by reticulated mains water and sewer. Stormwater is piped via an existing drainage system located on Fairtlough Street.

A desktop analysis of the site, including land immediately adjoining the site, has been undertaken utilising the LIST map.

This analysis did not identify any concerns with respect to:

- Threatened fauna or flora;
- Local Heritage;
- Landslip; or
- Flooding.

#### 4. NORTHERN MIDLANDS INTERIM PLANNING SCHEME

The following section of this report examines the relevant provisions of the NMIPS with respect to the proposed subdivision of land. This assessment demonstrates that the approval sought for 9 lots suitable for residential purposes is consistent with the applicable standards of the General Residential zone and the provisions of the relevant codes.

##### 4.1 Zone Purpose Statements

The subdivision forms an extension to the existing developed residential area of Perth. This subdivision, if approved, will create 9 lots with areas ranging from 720m<sup>2</sup> to 2143m<sup>2</sup>, which in turn will facilitate a range of dwelling types. The proposed lot areas are consistent with recent development located on the western side of Fairtlough Street. The site is serviced by reticulated mains water and sewer.

The proposal is consistent with the purpose of the General Residential zone.

##### 4.2 Local Area Objectives

The proposed lots are located within the Urban Growth Boundary of Perth and are in a location not subject to a Heritage Precinct.

The proposal is consistent with the local area objectives.

##### 4.3 Desired Future Character Statement

There are no desired future character statements.

##### 4.4 Use Table

The proposed lots are intended for residential use. The Use Table lists 'residential' as a no permit use. 'Residential' if for multiple dwellings is a permitted use.

##### 4.5 General Residential Zone – Use and Development Standards

Table 1 assesses the objectives and applicable standards relevant to this proposed subdivision of land. Where the proposed subdivision cannot comply with an acceptable solution, this report provides further assessment against the relevant objective and performance criteria.

Table 1: Assessment of 10 General Residential Zone, Northern Midlands Interim Planning Scheme

<b>10.3 Use Standards</b>		
Scheme Standard	Comment	Assessment
<b>10.3.1 Amenity</b>		
A1	The lots are intended for residential use.	Complies with Acceptable Solution

A2	The proposal is seeking approval for subdivision. No commercial activities proposed.	Not Applicable
A3	The lots are intended for residential use.	Complies with Acceptable Solution
<b>10.3.2 Residential Character – Discretionary Uses</b>		
A1	The proposal is seeking approval for subdivision. No commercial activities proposed.	Not Applicable
A2	There is no discretionary use proposed.	Not Applicable
A3	There is no discretionary use proposed.	Not Applicable
<b>10.4 Development Standards</b>		
Clauses 10.4.1 – 10.4.12 are not applicable as the application is for subdivision only.		
Clauses 10.4.13 – 10.4.13.9 are not applicable as the application is for subdivision only.		
<b>10.4.15 Subdivision</b>		
<b>10.4.15.1 Lot Area, Building Envelopes and Frontage</b>		
A1 (a)	All lots have an area greater than 450m <sup>2</sup> .	Complies with Acceptable Solution
A1(a)(i)	All lots are capable of containing a rectangle measuring 10m by 15m as shown on the proposal plan.	Complies with Acceptable Solution
A1(a)(ii)	All buildings on the site will be demolished.	Not Applicable
A1(b)	Not required for public use by the Crown, an agency, or a corporation all the shares which are held by Councils or a municipality.	Not Applicable
A1(c)	The proposal is not for the provision of utilities.	Not Applicable

## Supporting Submission

A1(d)	The proposal is not for the consolidation of lots.	Not Applicable
A1(e)	The purpose of the proposal is not to align existing titles with zone boundaries and no additional lots created.	Not Applicable
A2	Each lot has a frontage of more than 3.6m.	Complies with Acceptable Solution
<b>10.4.15.2 Provision of Services</b>		
A1 a)	Each lot will be connected to a reticulated water supply.	Complies with Acceptable Solution
A1 b)	Each lot will be connected to a reticulated sewerage system.	Complies with Acceptable Solution
A2	Each lot will be connected to a reticulated stormwater system.	Complies with Acceptable Solution
<b>10.4.15.3 Solar Orientation of Lots</b>		
A1	More than 50% of the lots are in accordance with this acceptable solution.	Complies with Acceptable Solution
A2	There are no lots with an area of less than 500m <sup>2</sup> .	Complies with Acceptable Solution
<b>10.4.15.4 Interaction, Safety and Security</b>		
This clause is not utilised in the Northern Midlands Interim Planning Scheme.		
<b>10.4.15.5 Integrated Urban Landscape</b>		
A1	The proposal will create a new road.	<b>Relies on Performance Criteria</b>
<b>10.4.15.6 Walking and Cycling Network</b>		
A1	The proposal will create a new road and footpath.	<b>Relies on Performance Criteria</b>
<b>10.4.15.7 Neighbourhood Road Network</b>		
A1	The proposal will create a new road.	<b>Relies on Performance Criteria</b>

## 4.6 CODES

The relevant Codes applicable to the application are outlined below.

### 4.6.1 E1.0 Bushfire-Prone Areas Code

A bushfire assessment has been prepared by AK Consultants in accordance with the requirements of this Code. Please refer to Appendix C.

### 4.6.2 E4.0 Road and Railway Assets Code

The proposed use and development will intensify an existing access. This Code applies to the proposed subdivision of land. Accordingly, Table 2 assesses the application against the applicable standards of this Code.

Table 2: Road and Railway Assets Code, Use and Development Standards

<b>E4.6.1 Use Standards</b>		
<b>Scheme Standard</b>	<b>Comment</b>	<b>Assessment</b>
<b>E4.6.1 Use and road or rail infrastructure</b>		
A1	Dwellings are not proposed as part of this application.	Not Applicable
A2	Fairtlough Street has a speed limit in this location not exceeding 60km/hr. The lots will generate around 81 vehicle movements per day. This exceeds the standard of 40 vehicle entry and exit movements per day.	<b>Relies on performance criteria</b>
A3	Fairtlough Street is not subject to a speed limit of more than 60km/hr.	Not Applicable
<b>E4.7 Development Standards</b>		
<b>E4.7.1 Development on and adjacent to Existing and Future Arterial Roads and Railways</b>		
A1	The proposed lots are within 50m from the existing railway.	<b>Relies on Performance Criteria</b>
<b>E4.7.2 Management of Road Accesses and Junctions</b>		
A1	A new road will be constructed. This will have a speed limit of less than 60km/hr. Each lot will have a	Complies with Acceptable Solution.

	single access providing both entry and exit.	
A2	Fairtlough Street does not have a speed limit exceeding 60km/hr.	Not Applicable
<b>E4.7.3 Management of a Rail Level Crossing</b>		
A1	The proposed subdivision does not require access across a railway.	Not Applicable
<b>E4.7.4 Sight Distance at Accesses, Junctions and Level Crossings</b>		
A1 a) – c)	The proposed subdivision complies with the acceptable solution as demonstrated by the Traffic Impact Assessment attached in Appendix D.	Complies with Acceptable Solution

#### 4.6.3 E6.0 Car Parking and Sustainable Transport Code

This Code applies to all use and development of land. Accordingly, Table 3 assesses the application against the applicable standards of this Code.

Table 3: Car Parking and Sustainable Transport Code, Use and Development Standards

<b>23.3 Use Standards</b>		
Scheme Standard	Comment	Assessment
<b>E6.6.1 Car Parking Numbers</b>		
A1(a)	Each lot has sufficient area to provide on-site car parking for a residential use in accordance with Table E6.1.	Complies with Acceptable Solution.
<b>E6.6.2 Bicycle Parking Numbers</b>		
A1.1	Each lot is of sufficient area to provide bicycle parking and storage for a residential use in accordance with Table E6.1.	Complies with the Acceptable Solution
A1.2	Not Applicable	Not Applicable

<b>E6.6.3 Taxi Drop-off and Pickup</b>		
A1	Not Applicable	Not Applicable
<b>E6.6.4 Motorbike Parking Provisions</b>		
A1	Each lot has sufficient area to accommodate a motorbike parking space.	Complies with Acceptable Solution
<b>E6.7 Development Standards</b>		
<b>E6.7.1 Construction of Car Parking Spaces and Access Strips</b>		
A1 (a)-(c)	The proposed vehicle parking area, loading area, circulation aisle and manoeuvring area has been designed in accordance with all of the required Australian Standards.	Complies with Acceptable Solution
A2	Access will be in accordance with the requirements of the acceptable solution.	Complies with Acceptable Solution
<b>E6.7.2 Design and Layout of Car Parking</b>		
A1.1 and A1.2	The application is for subdivision only. There are no buildings proposed at this stage.	Not Applicable
A2.1 and A2.2	The application is for subdivision only. There are no buildings proposed at this stage.	Not Applicable
<b>E6.7.3 Car Parking Access, Safety and Security</b>		
A1	The application is for subdivision only. There are no buildings proposed at this stage.	Not Applicable
A2	The application is for subdivision only. There are no buildings proposed at this stage.	Not Applicable
<b>E6.7.4 Parking with a Person with a Disability</b>		



A1 and A2	The application is for subdivision only. There are no buildings proposed at this stage.	Not Applicable
A2	The application is for subdivision only. There are no buildings proposed at this stage.	Not Applicable
<b>E6.7.6 Loading and Unloading of Vehicles, Drop-off and Pickup</b>		
A1	The application is for subdivision only. There are no buildings proposed at this stage.	Not Applicable
<b>E6.8.2 Bicycle Parking Access, Safety and Security</b>		
A1	The application is for subdivision only. There are no buildings proposed at this stage.	Not Applicable
A2	The application is for subdivision only. There are no buildings proposed at this stage.	Not Applicable
<b>E6.8.5 Pedestrian Walkways</b>		
A1	The application is for subdivision only. There are no buildings proposed at this stage.	Not Applicable

#### 4.6.4 E10.0 Recreation and Open Space Code

A request has been made to the General Manager of the Council seeking written consent that no land is required for public open space, but that instead there is to be a cash payment in lieu.

#### 4.7 Performance Criteria

The above assessment of the applicable standards has highlighted that the proposed use and development relies on a number of performance criteria. Accordingly, further information in regard to these performance criteria is offered and intended to assist the Council with their assessment of this application.

**4.7.1 10.4.15.5 Integrated Urban Landscape****Objective:**

To provide attractive and continuous landscaping in roads and public open spaces that contribute to the:

- a) character and identify of new neighbourhoods and urban places; or
- b) to existing or preferred neighbourhood character, if any.

**P1**

For subdivision that creates roads, public open space or other reserves, the design must demonstrate that:

- (a) it has regard to existing, significant features; and
- (b) accessibility and mobility through public spaces and roads are protected or enhanced; and
- (c) connectivity through the urban environment is protected or enhanced; and
- (d) the visual amenity and attractiveness of the urban environment is enhanced; and
- (e) it furthers the local area objectives, if any.

The application proposes to construct a new road with a length of no more than 60m to service the proposed lots. The provision of public open space or other reserves is not proposed as part of this application. The proposal cannot comply with the acceptable solution A1, Clause 10.4.15.5 and accordingly relies on the corresponding performance criterion P1.

The location of the proposed road has had regard to the significant features of the site including the lot configuration and the adjacent Western Railway Line. These significant features of the site have left no alternative but to incorporate a cul-de-sac into the design of the subdivision to allow access from Fairtlough Street to lots 3, 4, 5, 6 and 7.

The proposed new road will not modify or alter the function of Fairtlough Street or any other adjoining road, pedestrian or cycle network in this locality. Traffic volumes generated from the development will not be significant and will not alter or modify the function of the existing road network. The accessibility and mobility through public spaces and roads will be protected.

The proposed subdivision is designed to ensure that the majority of lots have a frontage to either Fairtlough Street or the new road. This will facilitate visual amenity and attractiveness of the urban environment ensuring that dwellings are largely orientated to address the frontage. This is consistent with the neighbourhood character of the adjoining residential development located to the west of the site. Development controls

imposed on land zoned General Residential will further assist with achieving these desired outcomes.

The proposal is consistent with the local area objectives as outlined earlier in this report.

The application satisfies the performance criterion P1 and upholds the objectives of this clause.

#### 4.7.2 10.4.15.6 Walking and Cycling Network

##### **Objective:**

- (a) To provide safe, convenient and efficient movement through and between neighbourhoods by pedestrian and cyclists; and
- (b) To design footpaths, shared path and cycle path networks that are safe, comfortable, well constructed and accessible.
- (c) To provide adequate provision to accommodate wheelchairs, prams, scooters and other footpath bound vehicles.

##### **P1**

Subdivision that creates new roads, footpaths, or public open spaces must demonstrate that the walking and cycling network is designed to:

- a) Link to any existing pedestrian and cycling networks;
- b) Provide the most practicable direct access for cycling and walking to activity centres, community facilities, public transport stops and public open spaces; and
- c) Provide an interconnected and continuous network of safe, efficient and convenient footpaths, shared paths, cycle paths and cycle lands based primarily on the network of arterial roads, neighbourhood roads and regional public open spaces; and
- d) Promote surveillance along roads and from abutting dwellings.

The application proposes to construct a new road and footpaths to service the 9 lots. The provision of public open spaces is not proposed as part of this application. The proposal cannot comply with the acceptable solution A1, Clause 10.4.15.6 and accordingly the proposal is reliant on the corresponding performance criterion P1.

The site is an irregular shaped parcel of land contained within the urban growth boundary of Perth. The Western Railway Line defines the southern lot boundary of the site. Fairtlough Street forms part of the main vehicle, pedestrian and cycling route and provides direct access from the site to the Perth Recreation Ground (corner of Fairtlough Street and Elizabeth Street), community facilities and local businesses. Pedestrian linkages are also facilitated from the site to Mulgrave Street and the public open space located at Seccombe Street East via pedestrian walkway located within the residential development to the west of the site. Arthur Street also provides an important pedestrian and cycling link from the site to the Perth Riverbank Reserve.

The site is serviced by public transportation. Departure points are located at Clarence Street, Arthur Street and Main Road. A departure point is within walking distance of the site.

The proposed new road will build on the existing footpath network, providing connectivity to the footpath established on the western side of Fairtlough Street. The proposed footpath will be in accordance with the required standard and will be safe, comfortable and accessible to all lots fronting the new road and Fairtlough Street. This will be of a design standard that can accommodate wheelchairs, prams, scooters and other footpath bound vehicles.

The lots are designed to allow dwellings to be orientated towards the new road and Fairtlough Street, ensuring that surveillance over roads is promoted.

The application satisfies the performance criterion P1 and is consistent with the objectives of this clause.

#### 4.7.3 10.4.15.7 Neighbourhood Road Network

##### **Objective:**

- a) To provide for convenient, safe and efficient movement through and between neighbourhoods for pedestrians, cyclists, public transport and other motor vehicles using the neighbourhood road network; and
- b) To design and construct road carriageways and verges so that the road geometry and traffic speeds provide an accessible and safe neighbourhood road system for all users.

##### **P1**

The neighbourhood road network must:

- a) Take account of the existing mobility network of arterial roads, neighbourhood roads, cycle paths, shared paths, footpaths and public transport routes;
- b) Provide clear hierarchy of roads and physical distinctions between arterial roads and neighbourhood road types; and
- c) Provide an appropriate speed environment and movement priority for the safe and easy movement of pedestrians and cyclists and for accessing public transport; and
- d) Provide safe and efficient access to activity centres for commercial and freight vehicles; and
- e) Ensure connector roads align between neighbourhoods for safe, direct and efficient movement of pedestrians, cyclists, public transport and other motor vehicles; and
- f) Provide an interconnected and continuous network of roads within and between neighbourhoods for use by pedestrians, cyclists, public transport and other vehicles and minimise the provisions of cul-de-sacs; and
- g) Provide for service and emergency vehicles to safely turn at the end of a

- dead-end road; and  
h) Take into account of any identified significant features.

The application proposes to construct a new road to service the proposed lots. The proposal cannot comply with the acceptable solution A1, Clause 10.4.15.7 and accordingly the proposal is reliant on the corresponding performance criterion P1.

The proposed road will be a local road servicing the lots 3, 4, 5, 6 and 7 and is consistent with the established road hierarchy of this area. The new road and footpath will facilitate an appropriate speed environment and will provide for the safe and easy movement of pedestrians, cyclists, public transport and other motor vehicles to the extended road network. This is further substantiated by the Traffic Impact Assessment provided (refer to Appendix D). The cul-de-sac will be designed to provide the ability for emergency vehicles to turn safely.

The provision of a cul-de-sac is seen necessary in this instance as the site is restricted by existing urban development and the railway line.

As discussed earlier, the new road will form an extension to the existing road network and will continue to facilitate the pedestrian and cycle linkages through the provision of a footpath one side of the new road. Fairtlough Street connects with the local collector of Arthur Street and provides direct access to local businesses, public open spaces and the Midlands Highway.

The proposal is consistent with the objective and satisfies the performance criteria P1, clause 10.4.15.7.

#### 4.7.4 E4.6.1 Use and road or rail infrastructure

##### **Objective:**

To ensure that the safety and efficiency of road and rail infrastructure is not reduced by the creation of new accesses and junctions or increased use of existing accesses and junctions.

##### **P2**

For roads with a speed limit of 60km/h or less, the level of use, number, location, layout and design of accesses and junctions must maintain an acceptable level of safety for all road users, including pedestrians and cyclists.

Fairtlough Street has a speed limit in this location not exceeding 60km/hr. The proposed lots will generate around 81 vehicle movements per day. This exceeds the standard for vehicle movements per day. Accordingly, the proposal cannot comply with the acceptable solution A2, clause E4.6.1. The proposal is reliant on the corresponding performance criterion P2.

A Traffic Impact Assessment is provided as part of this application. This is contained in Appendix D. This demonstrates that the proposal will maintain an acceptable level of safety for all road users in accordance with this clause.

The proposal satisfies the performance criteria P2 and the objectives of clause E4.6.1.

#### 4.7.5 E4.7.1 Development on and adjacent to Existing and Future Arterial Roads and Railways

##### **Objective:**

To ensure that development on or adjacent to category 1 or 2 roads (outside 60km/h), railways and future roads and railways is managed to:

- a) ensure the safe and efficient operation of roads and railways; and
- b) allow for future road and rail widening, realignment and upgrading; and
- c) avoid undesirable interaction between roads and railways and other use or development.

##### **P1**

Development including buildings, road works, earthworks, landscaping works and level crossings on or within 50m of category 1 or 2 road, in an area subject to a speed limit of more than 60km/h, a railway or future road or railway must be sited, designed and landscaped to:

- a) maintain or improve the safety and efficiency of the road or railway or future road or railway, including line of sight from trains; and
- b) mitigate significant transport-related environmental impacts, including noise, air pollution and vibrations in accordance with a report from a suitable qualified person; and
- c) ensure that additions or extensions of buildings will not reduce the existing setback to the road, railway or future road or railway; and
- d) ensure that the temporary buildings and works are removed at the applicant's expense within three years or as otherwise agreed by the road or rail authority.

The acceptable solution E4.7.1 cannot be met as the building envelopes of the majority of the proposed lots are within 50m of the Western Line Railway. Accordingly, the proposal is reliant on the corresponding performance criterion.

The Western Railway Line forms the southern boundary of the land and crosses the Arthur Street and Fairtlough Street intersection as a level crossing. This is located approximately 50m south of the site. The Western Railway Line, adjacent to the southern lot boundary of the site, is set in a cutting. Any dwellings constructed on the lots 5, 6 and 7 will be elevated well above the railway line. Accordingly the proposed subdivision will not compromise the safety and efficiency of the railway line, including the line of sight from trains.

The transport-related environmental impacts, including noise, air pollution and vibrations have been assessed by VIPAC Engineers & Scientists Limited (refer to Appendix E) and specifically addresses the performance criterion P1 (b) of this clause. This demonstrates that the development can appropriately mitigate transport related impacts from the railway line. There are no specific recommendations that can be incorporated at the subdivision stage. The recommendations mainly relate to when approval is sought for the construction of individual dwellings. The proposal satisfies the performance criteria P1(b) of this clause.

There are no additions or extensions to existing buildings proposed. There are no temporary buildings.

The proposal satisfies the objective and performance criterion P1 of this clause.

## 5. CONCLUSION

The application is seeking approval for the subdivision of the land at 105 Fairtlough Street, Perth is in accordance with the requirements of the NMIPS. The supporting submission has demonstrated:

- The site can be developed in accordance with the purpose of the General Residential zone;
- The lots have sufficient area to facilitate a range of dwelling types;
- The proposed cul-de-sac is consistent with the established road hierarchy and will facilitate a safe and connected urban environment;
- The lots are orientated to provide surveillance over Fairtlough Street and the new road; and
- Transport related environmental impacts from the Western Railway Line can be appropriately managed.

For these reasons this application can be supported.

6ty Pty Ltd  
ABN 27 014 609 900

Postal Address  
PO Box 63  
Riverside  
Tasmania 7250  
W 6ty.com.au  
E admin@6ty.com.au

Tarjag Suite 103  
The Charles  
287 Charles Street  
Launceston 7250  
P (03) 6332 3300

57 Best Street  
PO Box 1202  
Devonport 7310  
P (03) 6424 7161

# Planning Report

**Shervan Developments Pty Ltd**  
**105 Fairtlough Street**  
**Perth**

EXHIBITED

# Traffic Impact Assessment

Date - 21<sup>st</sup> October 2014  
Project name - Traffic Impact Assessment – 105 Fairtlough Street, Perth  
Project number - 14.155  
Document - I:\2014\14155\1 Administration\6 Authorities\2 Council\14-11-13 Traffic Impact Assessment.docx

D2



## 1. INTRODUCTION

This traffic report has been prepared in conjunction with the Department of Transport's "Traffic Impact Assessment" (TIA) Guidelines (draft) by 6ty Pty Ltd on behalf of Shervan Developments, owner of the land.

## 2. EXISTING CONDITIONS

### Location:

105 Fairtlough Street, Perth (CT 207804/1).

The land is a 0.98 Ha parcel of land, located on the north eastern corner of the intersection of Fairtlough Street and Arthur Street in Perth. The Western Railway Line forms the southern boundary of the land and crosses the intersection as a level crossing. The land has no frontage to Arthur Street.

### The Existing Road:

Fairtlough Street is a two lane sealed road adjacent to the land, comprised of a northbound lane and a southbound lane. This road is a local road and services a number of residential lots to the north of Arthur Street and has only limited connectivity to other streets.

The street is a sealed road with a typical seal width of 7.5m. A relatively recent subdivision has created two small courts (Sapphire Court and Opal Place) on the adjacent land to the west of the street, which saw the construction of kerb and channel on the western side of the road, as well as a footpath.

There is no kerbing on the site frontage itself and this is proposed as part of the subdivisional works.

### Use of land:

The property contains a single residence.

## 3. PROPOSED DEVELOPMENT

The proposed development is to create a small residential court with a total of nine residential lots. Three of these lots are to be accessed off Fairtlough Street, whilst the remaining six from the new court.

## 4. FAIRTLOUGH STREET TRAFFIC

Fairtlough Street runs north from the Arthur Street intersection and provides access to an area of mostly Rural Living properties north of the Perth residential area. There is a minor local road connection to Mulgrave Street via Seccombe Street, but no other through road connections.

For design purposes, this northern leg of Fairtlough Street can be considered as a single access point for the residences north of Arthur Street, including the subject land. Examination of title information reveals that there are some 64 residential lots using Fairtlough Street to access the local collector of Arthur Street. Arthur Street provides ready access to Perth proper, Evandale and the airport, as well as the Midland Highway for access to Launceston or Hobart.

There is a local road connection to the parallel Mulgrave Street via Seccombe Street, but this linkage provides no significant advantages to a commuter and is disregarded from the analysis.

## 5. TRIP GENERATION

The development will create nine residential lots from the existing lot, suitable for the construction of individual residences.

Traffic movements are estimated to be generated at a typical rate of nine movements per house, per day.

*Total        81 vpd – 8 vph (peak hour)*

## 6. TRIP ASSIGNMENT

It is anticipated that virtually all traffic to and from the site will be from the Arthur Street approach.

## 7. VEHICLE TYPES

The predominant vehicle type will be passenger cars, with other low numbers of service vehicles, with the heaviest vehicle routinely visiting the land being the weekly garbage collection.

## 8. ASSESSMENT YEARS

Construction is likely to begin in 2015, with the site being fully developed in mid 2016. A 10 year assessment period is conventional for this form of development, resulting in a final assessment year of 2026.

## 9. TRAFFIC GROWTH

Traffic growth on the local street network is conservatively estimated at 2% per annum.

## 10. EXISTING TRAFFIC VOLUMES

Based on the 64 existing lots north of Arthur Street, it is calculated that the peak hour traffic volume passing the site will be some 58 vehicles, of which 90% will be southbound in the morning peak and northbound in the evening peak.

Council have provided a detailed traffic count for Arthur Street that dates from 2003. This indicates an average daily traffic on Arthur Street of 1083 vehicles per day, which equates to 1350 vehicles per day in 2014 or 135 passing vehicles in the peak hour.

## 11. FUTURE GROWTH

Subject to future land use rezoning, growth of traffic to Fairtlough Street is unlikely to significantly increase within the Assessment Period as there is limited potential for further subdivision. However, traffic within Arthur Street is assumed to increase at 2% per annum, to reflect ongoing consolidation and development within the greater Perth region.

## 12. EXISTING TRAFFIC ISSUES

There are no known traffic issues with either Fairtlough Street or the intersection of Fairtlough Street with Arthur Street.

## 13. ROAD SAFETY

A copy of the DIER Crash History for this section of Fairtlough Street is attached. This indicates that there was a single reported accident within the past 5 years at the intersection with Arthur Street, where a vehicle failed to give way during wet conditions. This was a property damage only accident and is a minor accident common in local street networks.

There are no known road safety issues with the entrance to the land, which has ample sight distance north along Fairtlough Street (approximately 950m) and is clearly visible from the T intersection with Arthur Street, some 100m to the south.

## 14. ACCESS POINTS

The development proposes no changes to the existing constructed access.

## 15. ACCESS PARAMETERS

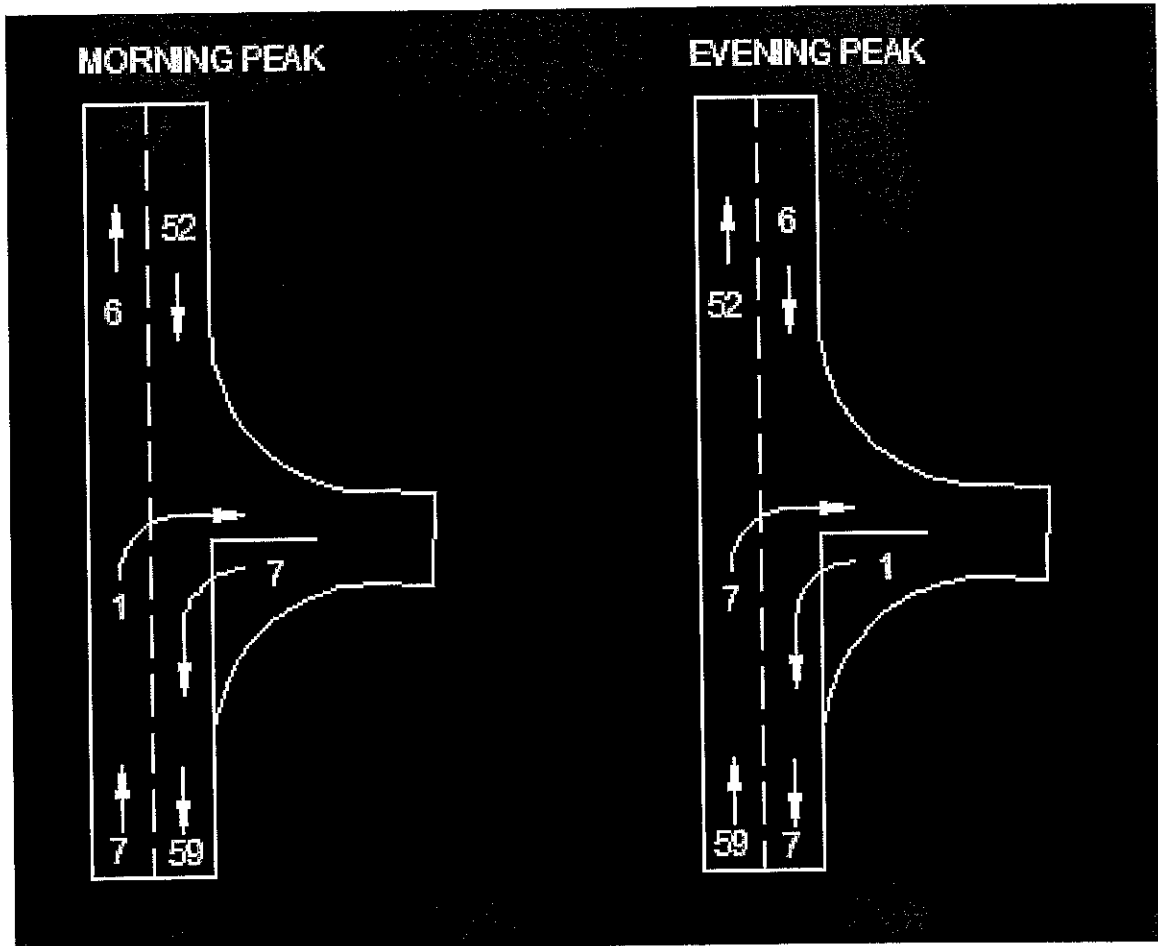
The proposed access is a conventional T intersection to Fairtlough Street.

The intersection is to be offset some 14m south of Opal Court, a small cul-de-sac situated on the opposite side of Fairtlough Street. A similar cul-de-sac, Sapphire Court, is located a further 96m to the north. The nearest significant intersection is the T intersection with Arthur Street, 110m to the south.

As stated previously, sight distance is ample in both directions.

## 16. TRAFFIC ANALYSIS

Looking at potential traffic movements at the new intersection with Fairtlough Street, the following minor and mayor flows can be predicted for the morning and evening peak hours, as shown on the following schematic:



*Comment: It can be seen that the right turn into the site poses no significant delays or need to provide a lengthy queue space. The traffic numbers are low and there are no capacity issues with the intersection.*

A more difficult manoeuvre is for vehicles leaving the site to turn right towards Perth, without causing traffic on the Midlands Highway to slow.

## 17. RECOMMENDED WORKS

The proposed intersection is considered to be ample for the existing and predicted traffic numbers.

**18. STREET FURNITURE**

No changes to street furniture are required by this development.

**19. PEDESTRIAN ACCESS**

The proposed subdivision is to provide a footpath on the southern side of the new court, with a pram crossing providing a connection to the existing footpath on the western side of Fairtlough Street.

**20. STATE ROADS**

No works are proposed that will affect State Roads.

**21. SUMMARY**

The development of the proposed subdivision is unlikely to affect traffic amenity or safety at the access point to Fairtlough Street, or to the intersection of Fairtlough Street with Arthur Street.

# Bushfire Hazard Management Report

EXHIBITED

**Report for:** Shervan Developments  
&  
Bullock Consulting

**Property Location:** 105 Fairtlough Street,  
Perth, TAS 7300

**Prepared by:** Scott Livingston  
AK Consultants,  
40 Tamar Street,  
LAUNCESTON, TAS. 7250

**Date:** 22<sup>nd</sup> August 2014



D3

**Summary**

**Client:** Shervan Developments

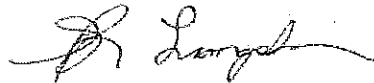
**Property identification:** Current zoning: General Residential  
CT 207804/1 PID 6743016

**Proposal:** A 9 lot subdivision for land at 105 Fairtlough Street, Perth

**Assessment comments:** A field inspection of the site was conducted to determine the Bushfire Attack Level and Risk.

**Conclusion:** Lot 9 is bushfire prone, being less than 100m from vegetation greater than 1 ha in size, however due to the threat being grasslands construction of habitable buildings on the lots may be to BAL Low, no internal access or water supply conditions are required for these lots.

The remaining lots are not bushfire prone, being greater than 100m from bushfire prone vegetation in accordance with Bushfire Prone Areas Advisory Note No 01 – 2014, where residential zoned land is to be considered low threat. Therefore the Bushfire Code does not apply to lots 1- 8.



**Assessment by:**

\_\_\_\_\_  
Scott Livingston,  
Master Environmental Management,  
Natural Resource Management Consultant.

Accredited Person under part 4A of the Fire Service Act 1979:  
Accreditation # BFP-105



## DESCRIPTION

A 9 lot subdivision proposed for land at 105 Fairtlough Street, Perth.

The site is bound by Fairtlough Street to the west and the Western Railway Line to the south east. The land immediately adjacent to the northern, eastern and western aspects of the subdivision is zoned General Residential and is not bushfire prone. The railway line south east of the subdivision is also not bushfire prone due to the vegetation strip not exceeding 20 metres in width.

A section of fire prone vegetation greater than 1 ha in size is situated to the south west. This Open Space zoned land is grassland vegetation. Only lot 9 is within 100m of this vegetation.

See Appendix 1 for maps. Appendix 2 for photographs.

## BAL AND RISK ASSESSMENT

Lot 9 is considered to be within a Bushfire Prone Area due to the proximity of a section of grassland vegetation to the south west greater than 1ha in area.

### VEGETATION AND SLOPE

Lot 9	North	East	South East	South West	West
Vegetation, within 100 m of subdivision boundary and proposed lots	0-100m Managed Land (General Residential)	0-5m Managed land, (residential) 5-20m Managed Land (Railway) 20-100m (General Residential)	0-20m Managed Land (Railway) 20-100m (General Residential)	0-75m Managed Land (General Residential), 75-100m Grassland (Open Space)	0-20m Managed Land (Road) 20-100m (General Residential)
Slope (degrees, over 100m)	Upslope/flat	Upslope/flat	Upslope/flat	Upslope/flat	Upslope/flat

### BUILDING AREA BAL RATING

Setback distances for BAL Ratings have been calculated on existing vegetation types. The setbacks shown do not account for any other setback requirements that may be applicable under planning scheme provisions, nor do they account for topographic or other constraints.

### BAL SETBACK REQUIREMENTS:



Direction	BAL Low Grassland	BAL 12.5 Grassland	BAL 19 Grassland
Upslopes and flat	50m	14m	10m
Downslopes 0-5°	50m	16m	11m

**BAL RATING:**

All lots including Lot 9, are greater than 50m from grassland and > 100m from other vegetation and therefore may be constructed to BAL Low.

**FIRE FIGHTING WATER SUPPLY**

The lots will be serviced by reticulated water supply. It is assumed under Bushfire Advisory Note 2, that the supply will provide adequate flows and pressure. Lot 9 has building areas with 120m of the existing fire plugs on Fairtlough St.

The furthest extent of future buildings on Lot 5, while not required to meet bushfire code provisions, will be greater than 120m from the nearest fire plug and it is recommended that a fire plug be installed at the end of the cul-de-sac to provide firefighting water supply to all lots, whether bushfire prone or not.

**ACCESS**

All lots are within 200m of a through road (Fairtlough Street). This meets the acceptable solutions in the Bushfire Code (E1.6.1.2.A1.c).

Bushfire Code E1.6.1.2.cii, acceptable solutions, requires a perimeter road between the lots and bushfire prone vegetation. This is provided by the through road of Arthur Street which lies between the lots and the bushfire prone grassland to the south west.

All bushfire prone lots must have access to within 30m of the furthest extent of the building area. Fairtlough St provides adequate access for lot 9 and no internal access is required to meet this condition. Lots 1-8 are not required to meet access provisions as they are not bushfire prone, but it is noted that Lots 4 & 5 would require internal access to meet the 30m hose lay requirement and consideration should be given to provision of access for fire appliances on these lots.

**CONCLUSIONS**

Lot 9 is bushfire prone, being less than 100m from vegetation greater than 1 ha in size, however due to the threat being grasslands construction of habitable buildings on the lots may be to BAL Low, no internal access or water supply conditions are required for these lots.

The remaining lots are not bushfire prone, being greater than 100m from bushfire prone vegetation in accordance with Bushfire Prone Areas Advisory Note No 01 – 2014, where residential zoned land is to be considered low threat. Therefore the Bushfire Code does not apply to lots 1- 8.

## REFERENCES

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- Northern Midlands Council (2013). *Northern Midlands Interim Planning Scheme. Bushfire Prone Areas Code.*
- Standards Australia. (2009). *AS 3959-2009 Construction of Buildings in Bushfire Prone Areas.*
- Tasmania Fire Service, (2014). *Bushfire Prone Areas Advisory Note 1-2014 v2 .*
- Tasmania Fire Service, (2014). *Bushfire Prone Areas Advisory Note 2-2014 v2 .*

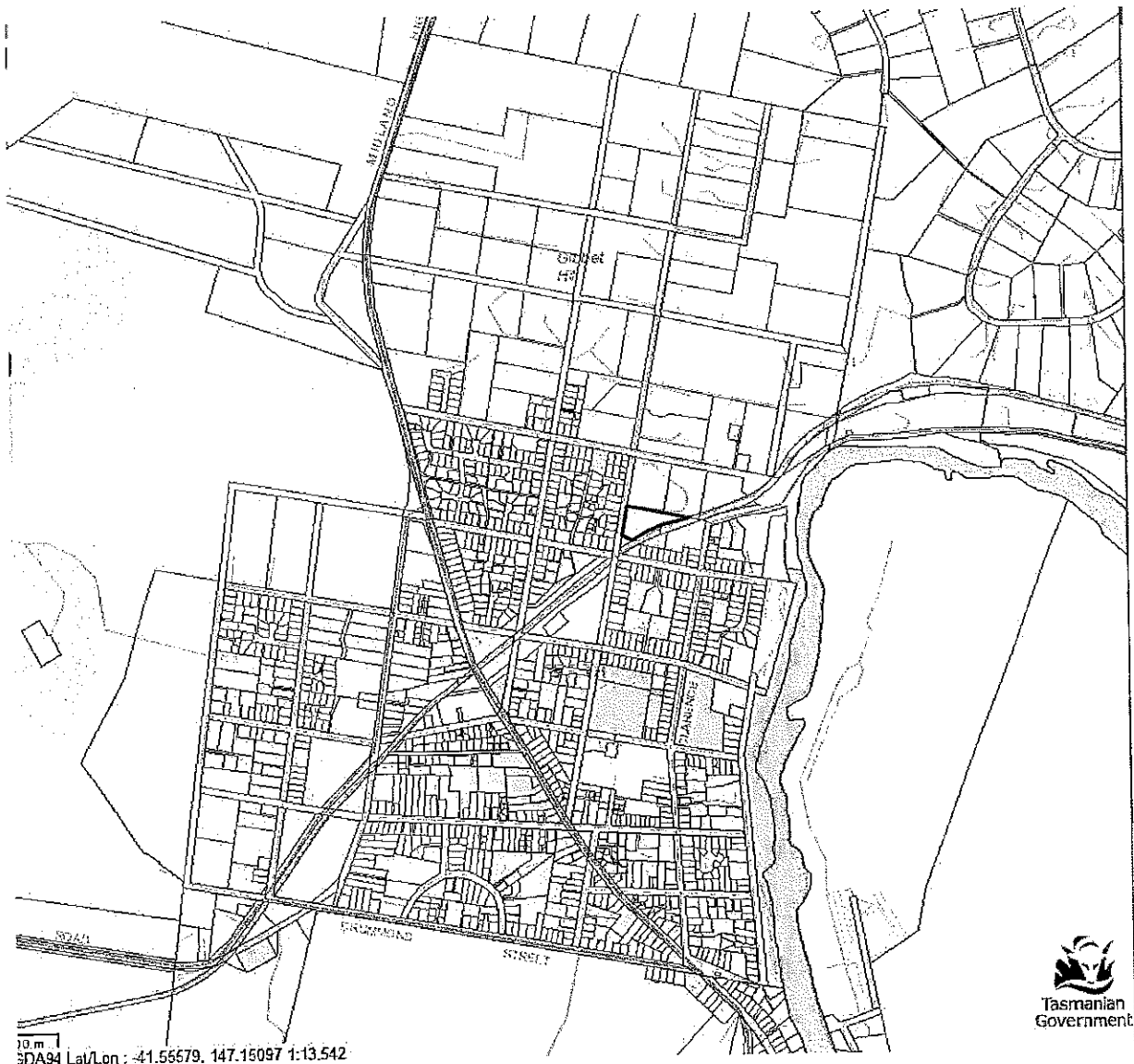


Figure 1: Location map, The LIST 19/08/14

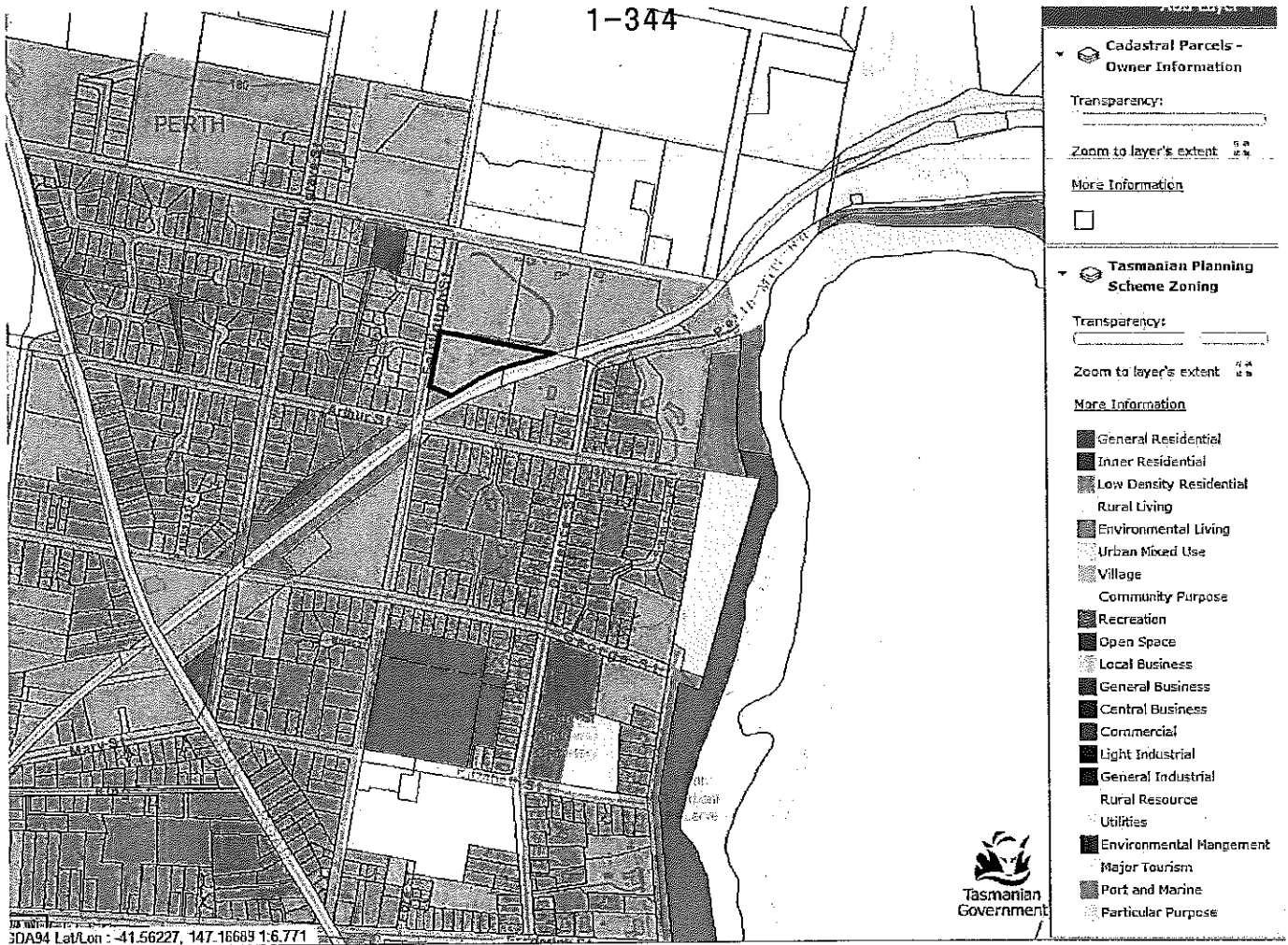
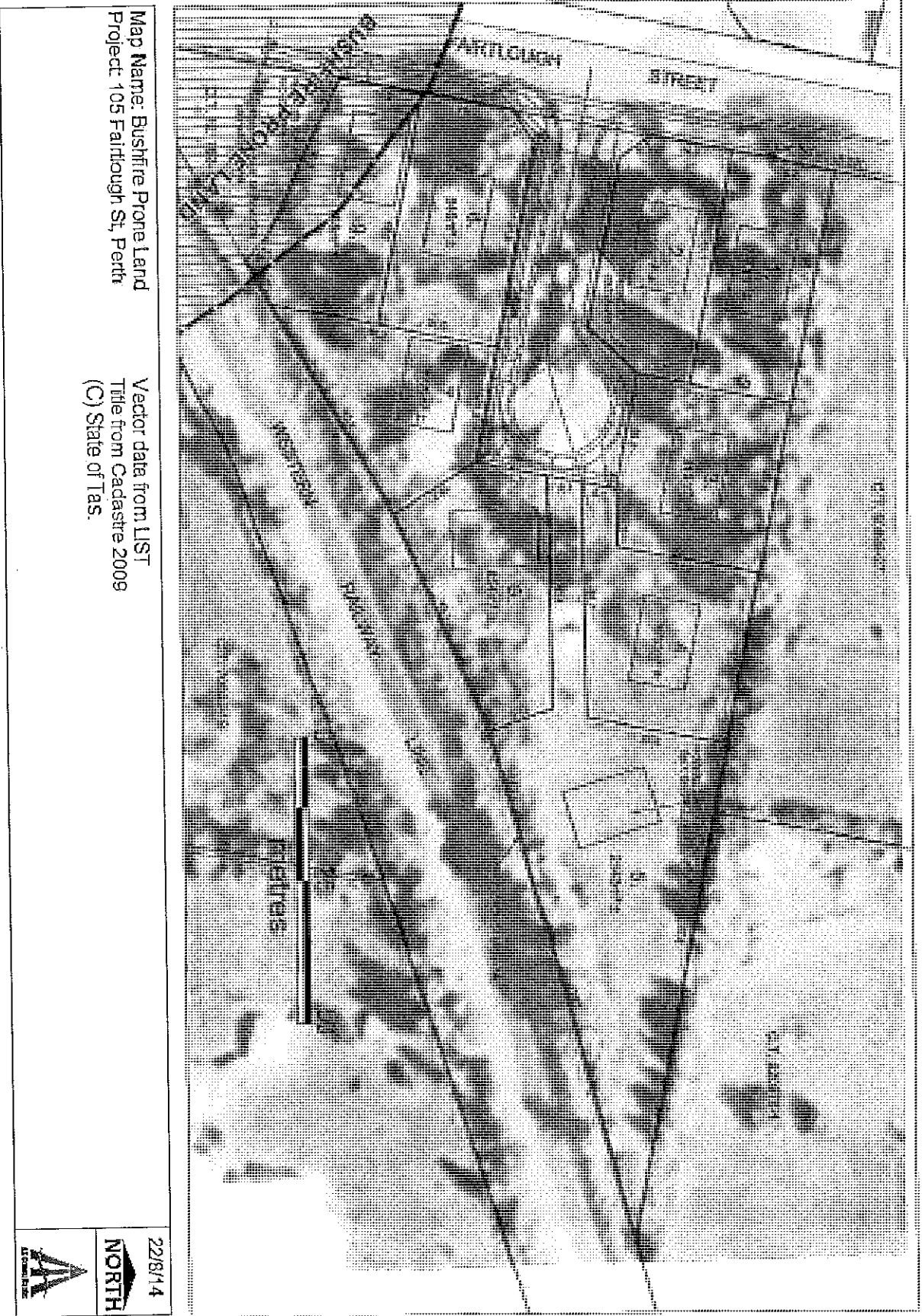


Figure 2: Zone Map, theLIST, 19/8/14



Map Name: Bushfire Prone Land  
Project: 105 Fairfough St, Perth

Vector data from LIST  
Title from Cadastre 2009  
(C) State of Tas.

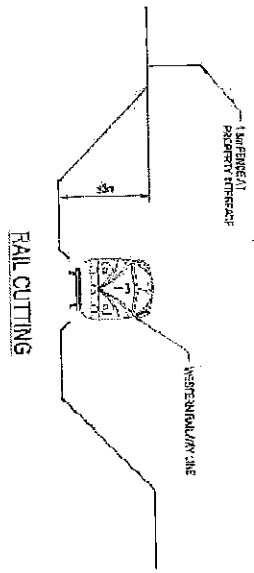
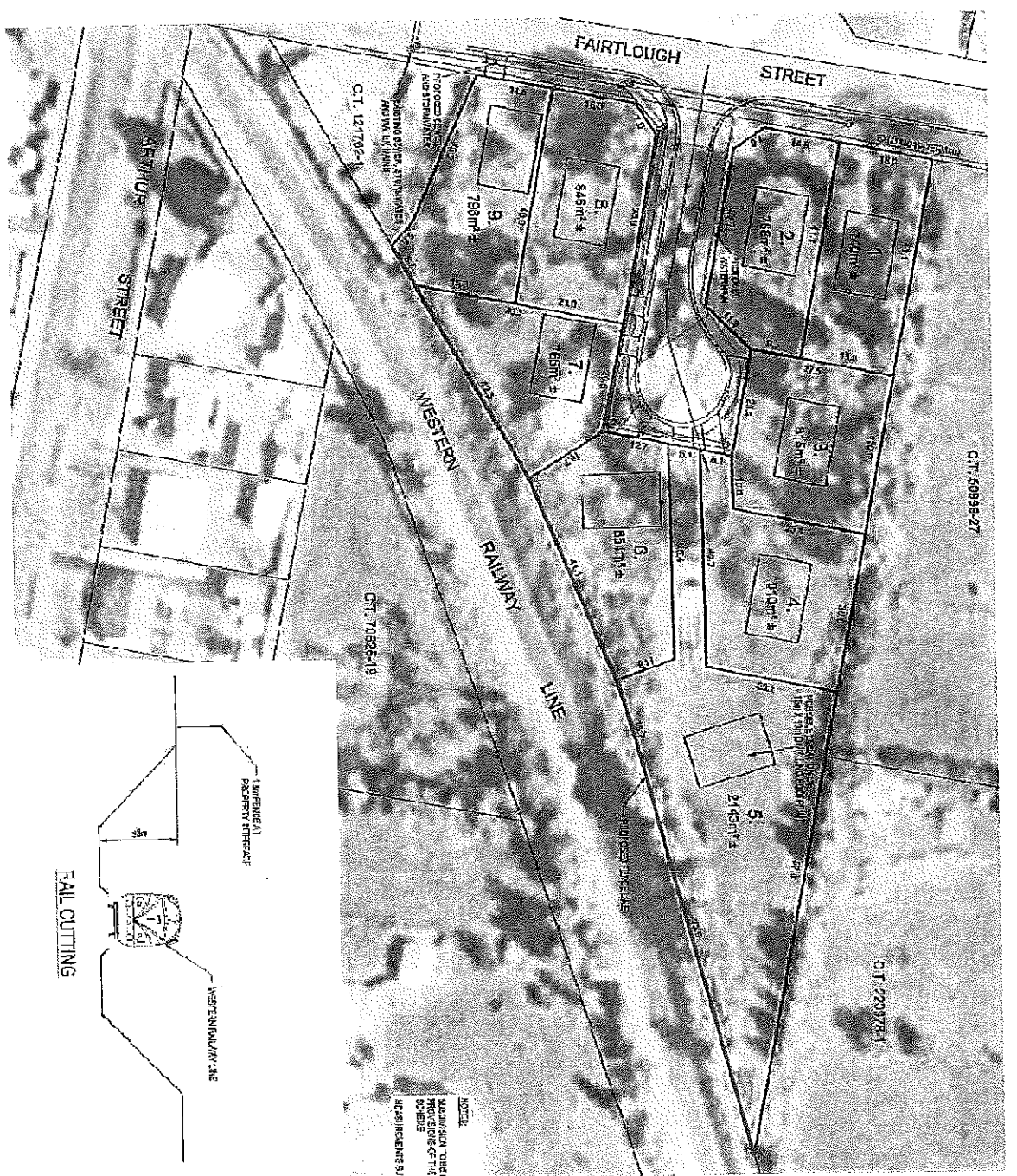
22/8/14  
NORTH  


Figure 3: Bushfire Prone Land



Figure 4: Aerial Image, The LIST 19/08/14





SCALE  
 SUBDIVISION TO BE MAJOR ROAD WITH THE  
 PROVISION OF THE NORTHERN LANDS PLANNING  
 SCHEME  
 DIMENSIONS SUBJECT TO FINAL SURVEY



**BULLOCK**  
 CONSULTING

ARCHITECTS  
 SURVEYORS  
 ENGINEERS

200 BULLOCK STREET  
 PERTH WESTERN AUSTRALIA 6150

200 BULLOCK STREET  
 PERTH WESTERN AUSTRALIA 6150  
 PHONE (08) 9447 5555  
 FAX (08) 9447 5556  
 WWW.BULLOCKCONSULTING.COM.AU

DATE: 01/05/2010  
 DRAWN: M. JENNINGS  
 CHECKED: M. JENNINGS

WARNING: THIS PLAN IS A PRELIMINARY PLAN AND SHOULD NOT BE USED FOR CONSTRUCTION OR FOR ANY OTHER PURPOSE WITHOUT THE WRITTEN APPROVAL OF THE SURVEYOR GENERAL. ANY UNLAWFUL USE OF THIS PLAN IS AT THE USER'S RISK.  
 THE SURVEYOR GENERAL'S OFFICE IS NOT RESPONSIBLE FOR ANY LOSS OR DAMAGE RESULTING FROM THE USE OF THIS PLAN.

0 LOT SUBDIVISION  
 REFERENCE: C.T. 207904-1  
 ADDRESS: 105 FAIRTLOUGH ST  
 PERTH  
 SHEWAN DEVELOPMENTS

SUBDIVISION PROPOSAL  
 PLAN  
 LAYOUT: J.E.P. SURVEY: J.E.P.  
 SCALE: 1:500  
 DATE: 14.155  
 PROJECT: PO1

Figure 5: Plan of Subdivision



Figure 6: Arthur St looking west, bushfire prone grassland adjacent to railway.



Figure 7: railway corridor to the south

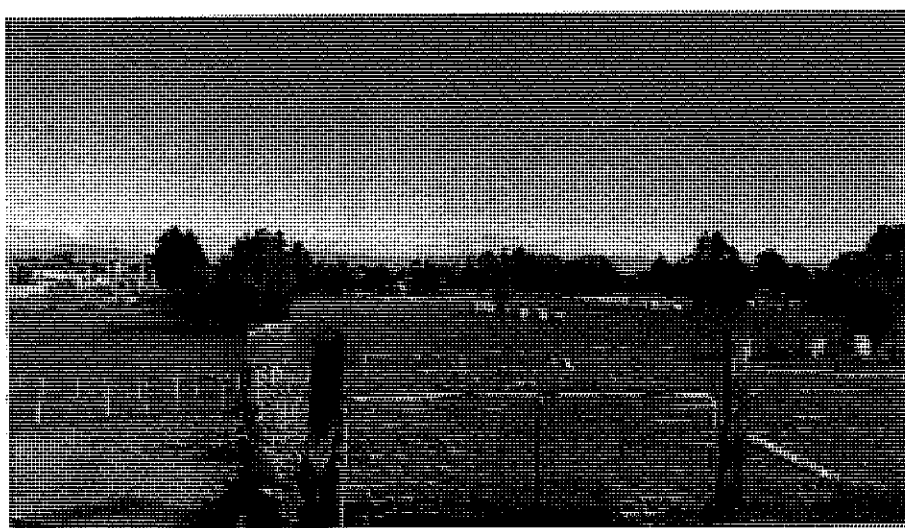


Figure 8: residential zoned land to the east (low threat)



**BUSHFIRE HAZARD MANAGEMENT PLAN: CT 207804/1, SUBDIVISION LOT 9, 105 FAIRTLOUGH ST,  
PERTH**

---

**BAL RATINGS**

Construction of Habitable buildings on Lot 9 may be to BAL Low.

**HAZARD MANAGEMENT AREAS**

All land within lot 9 must be maintained as managed lawns and gardens.

**ACCESS**

No internal access requirements

**WATER SUPPLY**


Existing fire plug locations meet requirements.



Tasmania Fire Service

1-350

## Approved Form of a Bushfire Hazard Management Plan

Chief Officer's requirements for a Bushfire Hazard Management Plan for compliance or exemption	
<i>Version:</i>	<i>1</i> <i>Issue Date:</i> <i>7 February 2014</i>
Purpose	<p>To provide an approved form for a Bushfire Hazard Management Plan in accordance with:</p> <p>Section 60A of the <i>Fire Service Act 1979</i> -</p> <p><i>bushfire hazard management plan</i> means a plan showing means of protection from bushfires in a form approved in writing by the Chief Officer.</p> <p>Section 3 <i>Land Use Planning and Approvals Act 1993</i></p> <p><i>bushfire hazard management plan</i> means a plan showing means of protection from bushfires in a form approved in writing by the Chief Officer;</p> <p><i>Chief Officer</i> means the person appointed as Chief Officer under <u>section 10 of the <i>Fire Service Act 1979</i></u>;</p>
Declaration	<p>A Bushfire Hazard Management Plan (BHMP) is in a form approved by the Chief Officer if:</p> <ol style="list-style-type: none"> <li>1. The BHMP is consistent with a Bushfire Report that has been prepared taking into consideration such of the matters identified in Schedule 1 as are applicable to the purpose of the BHMP; and</li> <li>2. The BHMP contains a map, plan or schedule identifying the specific measures required to provide a tolerable level of risk from bushfire for the purpose or activity described in the BHMP having regard to the considerations in Schedule 2; and</li> <li>3. The BHMP is consistent with all applicable Bushfire Hazard Management Advisory Notes issued by the Chief Officer.</li> </ol>
	 <p><b>Mike Brown AFSM</b> Chief Officer Tasmania Fire Service</p>

**Schedule 1 - Bushfire Report**

A Bushfire Report is an investigation and assessment of bushfire risk to establish the level of bushfire threat, vulnerability, options for mitigation measures, and the residual risk if such measures are applied on the land for the purpose or activity described in the assessment.

A Bushfire Report must include:

- a) A description of the characteristics of the land and of adjacent land;
- b) A description of the use or development that may be threatened by a bushfire on the site or on adjacent land; and
- c) Whether the use or development on the site is likely to cause or contribute to the occurrence or intensification of bushfire on the site or on adjacent land; and
- d) Whether the use or development on the site, and any associated use or development, can achieve and maintain a tolerable level of residual risk for the occupants and assets on the site and on adjacent land having regard for –
  - i. The nature, intensity and duration of the use;
  - ii. The type, form and duration of any development;
  - iii. A Bushfire Attack Level assessment to define the exposure to a use or development; and
  - iv. The nature of any bushfire hazard mitigation measures required on the site and/or on adjacent land.

**Schedule 2 - Bushfire Hazard Management Plan**

A BHMP is a document containing a map, plan or specification and must:-

- a) Identify the site to which the BHMP applies by address, Property Identifier (PID), and reference to a Certificate of Title under the *Land Titles Act 1980*;
- b) Identify the certifying Bushfire Hazard Practitioner, Accreditation Number, and Scope of Accreditation.
- c) Identify the proposed activity to which the BHMP applies by reference to any plans, specifications or other documents that are applicable for the purpose of describing the proposed use or development;
- d) Indicate the bushfire hazard management and protection measures required to be implemented by the Bushfire Report;
- e) If intended to be applied for the purpose of satisfying a regulatory requirement, identify the regulation by its statutory citation and indicate the applicable provisions for which the BHMP applies; and
- f) Have, as a schedule, the Bushfire Report that details specific bushfire hazard management and bushfire mitigation measures required to achieve a tolerable level of residual risk for the proposed activity and any building or development on the site, including:
  - i) Measures to achieve compliance with any mandatory land use planning requirement in a planning process required under the *Land Use Planning and Approvals Act 1993 (Attachment 1)*;
  - ii) Measures to achieve compliance with any mandatory outcome for a building or work undertaken in accordance with the *Building Act 2000* and the Building Regulations 2004 (Form 55).

**Attachment 1: Certificate of Compliance to the Bushfire-prone Area Code under Planning Directive No 5**

**Code E1 – Bushfire-prone Areas Code**

**Certificate under s51(2)(d) Land Use Planning and Approvals Act 1993**

<i>Office Use</i>
<i>Date Received</i>
<i>Permit Application No</i>
<i>PID</i>

<b>1. Land to which certificate applies<sup>1</sup></b>
<b>Name of planning scheme or instrument: Northern Midlands Interim Planning Scheme 2013</b>

Use or Development Site	Certificate of Title / PID
Street Address	CT 207804/1 PID 6743016
105 Fairtlough Street, Perth	
Land that is not the Use or Development Site relied upon for bushfire hazard management or protection	Certificate of Title / PID
Street Address	NA

<b>2. Proposed Use or Development (provide a description in the space below)</b>	
A 9 lot subdivision for land at 105 Fairtlough Street, Perth	

- Vulnerable Use
- Hazardous Use
- Subdivision
- New Habitable Building on a lot on a plan of subdivision approved in accordance with Bushfire-prone Areas Code.
- New habitable on a lot on a pre-existing plan of subdivision )
- Extension to an existing habitable building
- Habitable Building for a Vulnerable Use

<sup>1</sup> If the certificate relates to bushfire management or protection measures that rely on land that is not in the same lot as the site for the use or development described, the details of all of the applicable land must be provided.

3. Documents relied upon<sup>2</sup>

<i>Document or certificate description:</i>	
<input type="checkbox"/>	<p><b>Description of Use or Development<sup>3</sup> (Proposal or Land Use Permit Application)</b></p> <p><b>Documents, Plans and/or Specifications</b></p> <p><i>Title: Plan of Subdivision</i></p> <p><i>Author: Bullock Consulting</i></p> <p><i>Date: 05/06/2014</i></p>
<input type="checkbox"/>	<p><b>Bushfire Report<sup>4</sup></b></p> <p><i>Title: BHMP_105Fairtlough_220814</i></p> <p><i>Author: Scott Livingston</i></p> <p><i>Date: 22/08/2013</i></p>
<input type="checkbox"/>	<p><b>Bushfire Hazard Management Plan<sup>5</sup></b></p> <p><i>Title: BHMP_105Fairtlough_220814</i></p> <p><i>Author: Scott Livingston</i></p> <p><i>Date: 22/08/2013</i></p>
<input type="checkbox"/>	<p><b>Other documents</b></p> <p><i>Title: Northern Midlands Interim Planning Scheme 2013</i></p> <p><i>Author: Northern Midlands Council</i></p> <p><i>Date: 2013</i></p> <p><i>Title: AS 3959-2009 Construction of Buildings in Bushfire Prone Areas</i></p> <p><i>Author: Standards Australia.</i></p> <p><i>Date: 2009</i></p> <p><i>Title: Bushfire Prone Areas Advisory Note 1 - 2014</i></p> <p><i>Author: Tasmania Fire Service</i></p> <p><i>Date: 21/1/2014</i></p> <p><i>Title: Bushfire Prone Areas Advisory Note 2 - 2014</i></p> <p><i>Author: Tasmania Fire Service</i></p> <p><i>Date: 21/1/2014</i></p>

<sup>2</sup> List each document that is provided or relied upon to describe the use or development, or to assess and manage risk from bushfire, including its title, author, date, and version.

<sup>3</sup> Identify the use or development to which the certificate applies by reference to the documents, plans, and specifications to be provided with the permit application to describe the form and location of the proposed use or development. For habitable buildings, a reference to a nominated plan indicating location within the site and the form of development is required.

<sup>4</sup> If there is more than one Bushfire Report, each document must be identified by reference to its title, author, date and version.

<sup>5</sup> If there is more than one Bushfire Hazard Management Plan, each document must be identified by reference to its title, author, date and version

4. Nature of Certificate<sup>6</sup>

Applicable Standard	Assessment Criteria	Compliance Test: Certificate of Insufficient Increase in Risk	Compliance Test: Certified Bushfire Hazard Management Plan	Reference to applicable Bushfire Risk Assessment or Bushfire Hazard Management Plan <sup>7</sup>

<input checked="" type="checkbox"/> E1.4 - Use or development exempt from this code E1.4. (Identify which exemption applies)		No specific measures required because the use or development is consistent with the objective for each of the applicable standards identified in this Certificate	<input type="checkbox"/> Not Applicable	
--	--	---	---	--

<input checked="" type="checkbox"/> E1.5.1 - Vulnerable Use E1.5.1.1 - location on bushfire-prone land	A2	Not Applicable	Tolerable level of risk and provision for evacuation	<input type="checkbox"/>
---	----	----------------	--	--------------------------

<input checked="" type="checkbox"/> E1.5.2 - Hazardous Use E1.5.2.1 - location on bushfire-prone land	A2	Not Applicable	Tolerable level of risk from exposure to dangerous substances, ignition potential, and contribution to intensity fire	<input type="checkbox"/>
--	----	----------------	---	--------------------------

<input checked="" type="checkbox"/> E1.6.1 - Subdivision E1.6.1.1 - Hazard Management Area	A1	No specific measure for hazard management	<input type="checkbox"/> Provision for hazard management areas in accordance with BAL 19 Table 2.4.4 AS3959	<input checked="" type="checkbox"/> BHMP_105Fairtlough_220814
E1.6.1.2 - Public Access	A1	No specific public access measure for fire fighting	<input type="checkbox"/> Layout of roads and access is consistent with objective	<input checked="" type="checkbox"/> BHMP_105Fairtlough_220814
E1.6.1.3 - Water Supply	A1	No specific water supply for fight fighting	<input checked="" type="checkbox"/> Not Applicable	<input checked="" type="checkbox"/> BHMP_105Fairtlough_220814
		Reculcated water supply		

<sup>6</sup> The certificate must indicate by placing a ✓ in the corresponding  for each applicable standard and the corresponding compliance test within each standard that is relied upon to demonstrate compliance to Code E1

<sup>7</sup> Identify the Bushfire Risk Assessment report or Bushfire Hazard Management Plan that is relied upon to satisfy the compliance test

	A2 Non-reticulated water supply	No specific water supply measure for fight fighting	<input type="checkbox"/>	Water supply is consistent with objective	<input type="checkbox"/>	
--	------------------------------------	---	--------------------------	---	--------------------------	--

<input checked="" type="checkbox"/>	<b>E1.6.2 - Habitable Building on lot on a plan of subdivision approved in accordance with Code</b>					
	<i>E1.6.2.1 - Hazard Management Area</i>	A1	No specific measure for hazard management	<input type="checkbox"/>	Provision for hazard management areas in accordance with BAL 19 Table 2.4.4 AS3959 and managed consistent with objective	<input type="checkbox"/>
	<i>E1.6.2.2 - Private Access</i>	A1	No specific private access for fire fighting	<input type="checkbox"/>	Private access is consistent with objective	<input type="checkbox"/>
		A2	Not Applicable			
	<i>E1.6.2.3 - Water Supply</i>	A1	No specific water supply measure for fight fighting	<input type="checkbox"/>	Private access to static water supply is consistent with objective Water supply is consistent with objective	<input type="checkbox"/>

<input checked="" type="checkbox"/>	<b>E1.6.3 - Habitable Building (pre-existing lot)</b>					
	<i>E1.6.3.1 - Hazard Management Area</i>	A1	No specific measure for hazard management	<input type="checkbox"/>	Provision for hazard management is consistent with objective; or	<input type="checkbox"/>
	<i>E1.6.3.2 - Private Access</i>	A1	No specific private access measure for fire fighting	<input type="checkbox"/>	Provision for hazard management areas in accordance with BAL 29 Table 2.4.4 AS3959 and managed consistent with objective	<input type="checkbox"/>
		A1	Private access is consistent with objective	<input type="checkbox"/>		<input type="checkbox"/>
		A2	Not applicable			
	<i>E1.6.3.3 - Water Supply</i>	A1	No specific water supply measure for fight fighting	<input type="checkbox"/>	Private access to static water supply is consistent with objective Water supply is consistent with objective	<input type="checkbox"/>



<input checked="" type="checkbox"/> E1.6.4 - Extension to Habitable Building E1.6.4.1 - hazard management	AI	No specific hazard management measure	<input type="checkbox"/> Provision for hazard management is consistent with objective; or	<input type="checkbox"/>
			<input type="checkbox"/> Provision for hazard management areas in accordance with BAL 12.5 Table 2.4.4 AS3959 and managed consistent with objective	<input type="checkbox"/>
E1.6.5 - Habitable Building for Vulnerable Use				
<input checked="" type="checkbox"/> E1.6.5.1 - hazard management	AI	No specific measure for hazard management	<input type="checkbox"/> Bushfire hazard management consistent with objective; or <input type="checkbox"/> Provision for hazard management areas in accordance with BAL 12.5 Table 2.4.4 AS3959 and managed consistent with objective	<input type="checkbox"/>

<b>5. Bushfire Hazard Practitioner – Accredited Person</b>	
Name	Scott Livingston
Address	40 Tamar St. Launceston, 7250
Phone No:	03 6334 1033
Fax No:	03 6334 1117
Email address:	scott@akconsultants.com.au
Fire Service Act 1979 Accreditation No:	BFP-105
Scope:	1, 2, 3A, 3B, 3C

**6. Certification**

I, Scott Livingston certify that in accordance with the authority given under the Part 4A of the Fire Service Act 1979 –

<i>The use or development described in this certificate is exempt from application of Code E1 – Bushfire-Prone Areas in accordance with Clause E1.4(a) because there is an insufficient increase in risk to warrant specific measures for bushfire hazard management and/or bushfire protection in order to be consistent with the objective for all of the applicable standards identified in Section 4 of this Certificate</i>	✓
--	---

or

<i>There is an insufficient increase in risk to warrant specific measures for bushfire hazard management and/or bushfire protection in order for the use or development described to be consistent with the objective for each of the applicable standards identified in Section 4 of this Certificate.</i>	<input type="checkbox"/>
---	--------------------------

and/or

<i>The Bushfire Hazard Management Plan/s identified in Section 4 of this certificate is/are in accordance with the Chief Officer's requirements and can deliver an outcome for the use or development described that is consistent with the objective and the relevant compliance test for each of the applicable standards identified in Section 4 of this Certificate</i>	✓
---	---

Signed



Date 22/08/14

12 November 2014

6ty°  
Tamar Suite 103, The Charles  
287 Charles Street  
Launceston 7250421333-01  
AJM/BWB

Attn: Ms Heidi Goess

Dear Madam,

RE: Fairtlough St, Perth, subdivision rail noise, ground vibration and air quality assessment

## 1. INTRODUCTION

Vipac was commissioned by 6ty° to undertake a noise, ground vibration and air quality assessment of a proposed residential subdivision at 105 Fairtlough St, Perth, in relation to an adjacent rail corridor. The proposed subdivision bounds northern side of TasRail's Western Line to the east of Fairtlough St and north of Arthur St. This follows a request from the Northern Midlands Council as follows:-

The acceptable solution of clause E4.7.1 A1 has not been met as the building envelopes on the majority of the proposed lot are within 50m the Western Line Railway. As part of compliance with the Performance Criteria, a report addressing clause E4.7.1 P1 (b) of the Road and Railway Assets Code is required.

**Performance Criteria states:**

P1 Development including buildings, road works, earthworks, landscaping works and level crossings on or within 50m of a category 1 or 2 road, in an area subject to a speed limit of more than 60km/h, a railway or future road or railway must be sited, designed and landscaped to:

**b)** mitigate significant transport-related environmental impacts, including noise, air pollution and vibrations in accordance with a report from a suitably qualified person; and"

Vipac proposed the following to address Performance Criteria requirements outlined above:-

- Measure noise levels from rail pass-by events at the site of the proposed subdivision and assess against *NSW Office of Environment and Heritage (2012) Rail Infrastructure Noise Guideline (Draft for Consultation)* criteria. Provide recommendations for mitigation if required.
- Measure ground vibration levels from rail pass-by events at the site of the proposed subdivision and assess against *'NSW Department of Environment and Conservation (2006) Assessing Vibration: a technical guideline'* criteria. Provide recommendations for mitigation if required
- Predict potential air contaminant concentrations at the site from rail pass-by events and assess against *Tasmanian Environmental Protection Policy (EPP) Air 2004* criteria. Provide recommendations for mitigation if required.

Figure 1 presents an aerial view of the proposed subdivision (provided by 6ty°) with the approx. measurement location indicated in yellow.

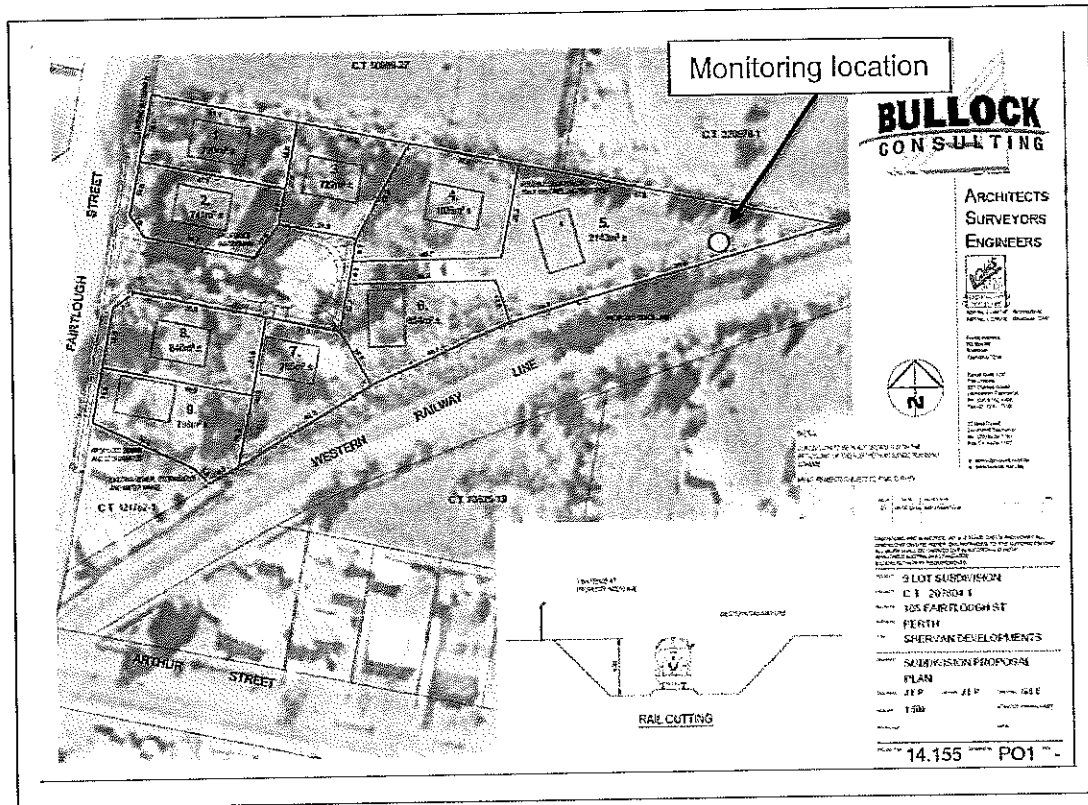


Figure 1 – Aerial view of the proposed Fairtlough St subdivision (Provided by 6ty°).

## 2. MEASUREMENT PROCEDURE

A logging sound level meter (SLM) and two ground vibration levels were located towards the eastern boundary of the Fairtlough St subdivision for a period of approximately one week (see figure 1 for approx. location). The meters were located at the following approx. distances from the rail corridor track centreline:-

- SLM: 14.2 m
- Ground vibration meter: 12.2 m and 22.1 m.

Figure 2 shows the sound level meter and ground vibration meter geophone locations on the eastern boundary of the subdivision. The following instrumentation was utilised:-

- Larson Davis 824 SLM measuring 1/3-octave band A-weighted Ln-statistics over a 15-minute period.
- InstanTel Minimate Plus ground vibration meter measuring peak particle velocity in mm/s.

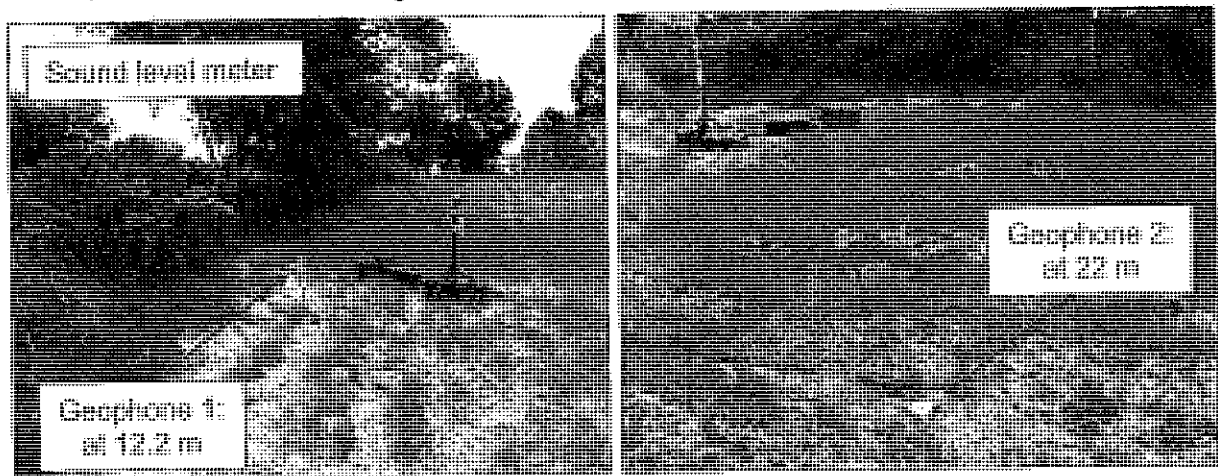


Figure 2 – Sound level and ground vibration meters located towards eastern boundary.

### 3. NOISE ASSESSMENT

#### 3.1. Assessment criteria

For the assessment of the train pass-by noise measured guidance is taken from *NSW Office of Environment and Heritage (2013) Rail Infrastructure Noise Guideline*. Under this guideline the following trigger level applies for heavy rail noise:-

- 80  $L_{Amax}$  (New rail line development)

**NB:**  $L_{Aeq}$  limits that apply under this guideline are not considered here due to the infrequent nature of train pass-bys on the Western Line.

**NB:** The above guideline states that  $L_{Amax}$  trigger levels exclude safety warning devices such as warning horns and bells at level crossings and as train pass-by measurements for  $L_{Amax}$  were controlled by train horn noise,  $L_{A1,15min}$  measurements have been used for this review to represent maximum noise levels generated by locomotive noise.

#### 3.2. Measured levels

Figure 2 below provides a graph of measured  $L_{A1,15min}$  levels with the 80 dBA trigger level marked in red.

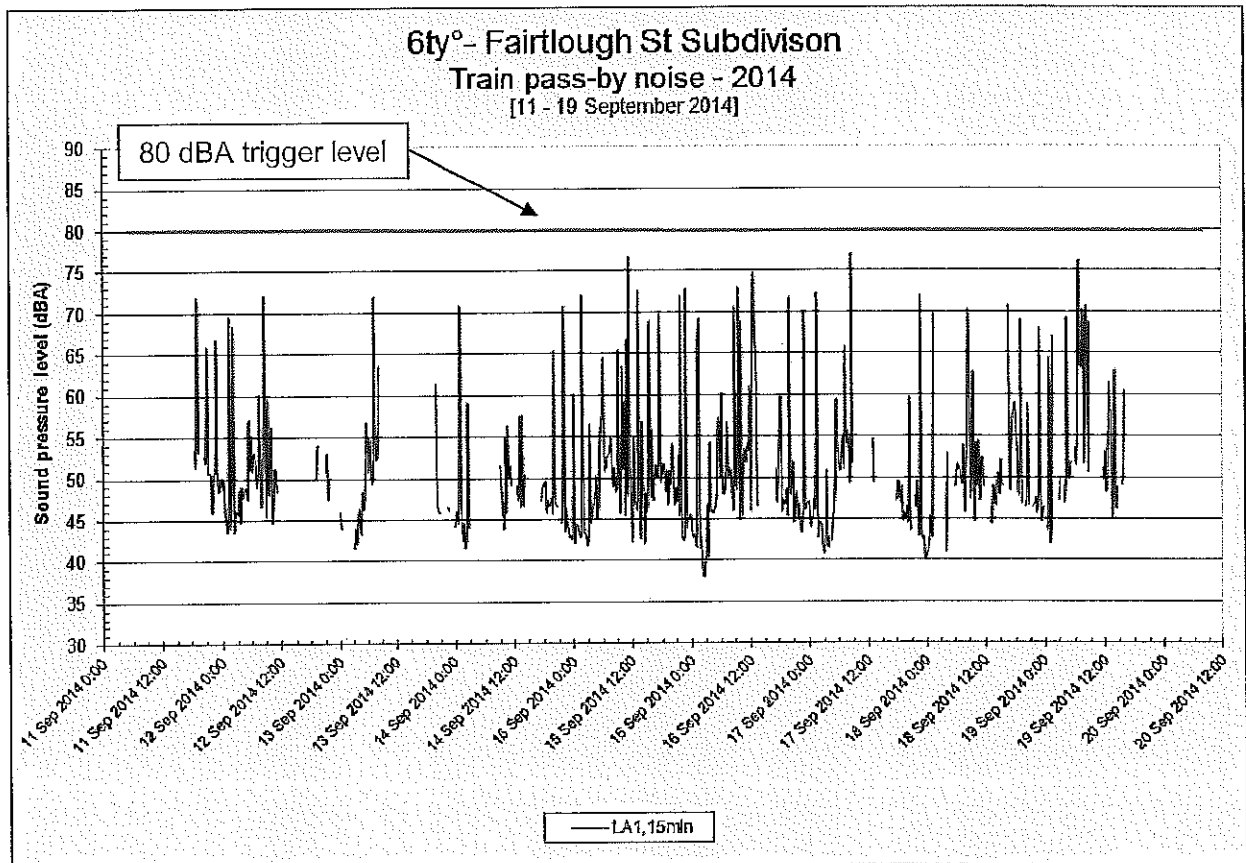


Figure 3 – Measured  $L_{A1,15min}$  levels with trigger level indicated.

From the above we note the following:-

- The highest  $L_{A1,15min}$  level measured was 77 dBA, below the assessment criteria.

**NB:** Horn blow noise is not assessed here against the trigger level outlined above. However,  $L_{Amax}$  horn noise levels measured during this assessment were as high as 116 dBA.

### 3.3. Recommendations

The measured levels  $L_{A1,15min}$  levels were below 80 dBA and therefore an increased sound transmission loss beyond that of a standard building envelope is not required under the assessment criteria adopted here. Standard building envelopes typically have a transmission loss of 20 to 25 dBA (lightweight constructions such as fibre cement cladding or fully glazed facades typically have a lower performance than this). Vipac recommends that in the design of facade building elements for living and sleeping areas, in particular for buildings constructed on lots 5, 6, 7 and 9, the following constructions are considered to further reduce train pass-by noise intrusion:-

- **Walls:** Brick veneer wall of 110mm bricks, 90 X 45 mm studs and 10mm plasterboard with 75 mm fibreglass insulation.
- **Ceiling/roof:** Either of the following constructions:-
  - Concrete tiles and sarking foil over the rafters; 13mm plasterboard ceiling; R 4.0 fibreglass insulation over plasterboard.
  - Colorbond roof; 2 X 13mm plasterboard ceiling (lapped joints between layers); R 4.0 fibreglass insulation over plasterboard.

**NB:** To maintain the performance of the above construction lights should be surface mounted only, no down lights that penetrate the plasterboard ceiling.

- **Windows:** Glazed with 10.38 mm laminated glass (up to  $R_w$  37, value depends on window dimensions with larger panes having lower  $R_w$  values). Equivalent acoustic performance from a double glazed system would require 6 mm thick and 4 mm thick panes and a 15 mm air gap.

**NB:** The glazing must be in frames to suit the glazing weight and thickness with appropriate acoustic seals such that the glazing transmission loss performance is not compromised. The frames must also be well sealed to the brick wall to ensure there is no weak acoustic path between the frames and the wall.

- **Doors:** Solid core doors with appropriate acoustic seals to give the required acoustic performance. Glazed doors and sliding or by-fold patio doors would require specialist acoustic consideration.

**NB:** The above recommendations are only general and performance may vary. The relative surface area of each element is an important factor in determining the overall sound transmission loss performance of a building facade and this can only be done with a specific building design.

## 4. GROUND VIBRATION ASSESSMENT

### 4.1. Assessment criteria

Under the *NSW Office of Environment and Heritage (2013) Rail Infrastructure Noise Guideline* for the assessment of vibration generated by train movements assessors are redirected to the *NSW Department of Environment and Conservation (2006) Assessing Vibration: a technical guideline* and advised to consider rail generated vibration as intermittent.

Under *Assessing Vibration: a technical guideline* exposure criteria apply for impulsive vibration at a residence at night as follows:-

- Preferred: 2.8 mm/s (peak velocity)
- Maximum: 5.6 mm/s (peak velocity)

### 4.2. Measured levels

Figures 4 and 5 below present longitudinal peak particle velocity levels measured by geophones 1 and 2 respectively. The preferred and maximum trigger levels are marked in blue and red respectively on each graph.

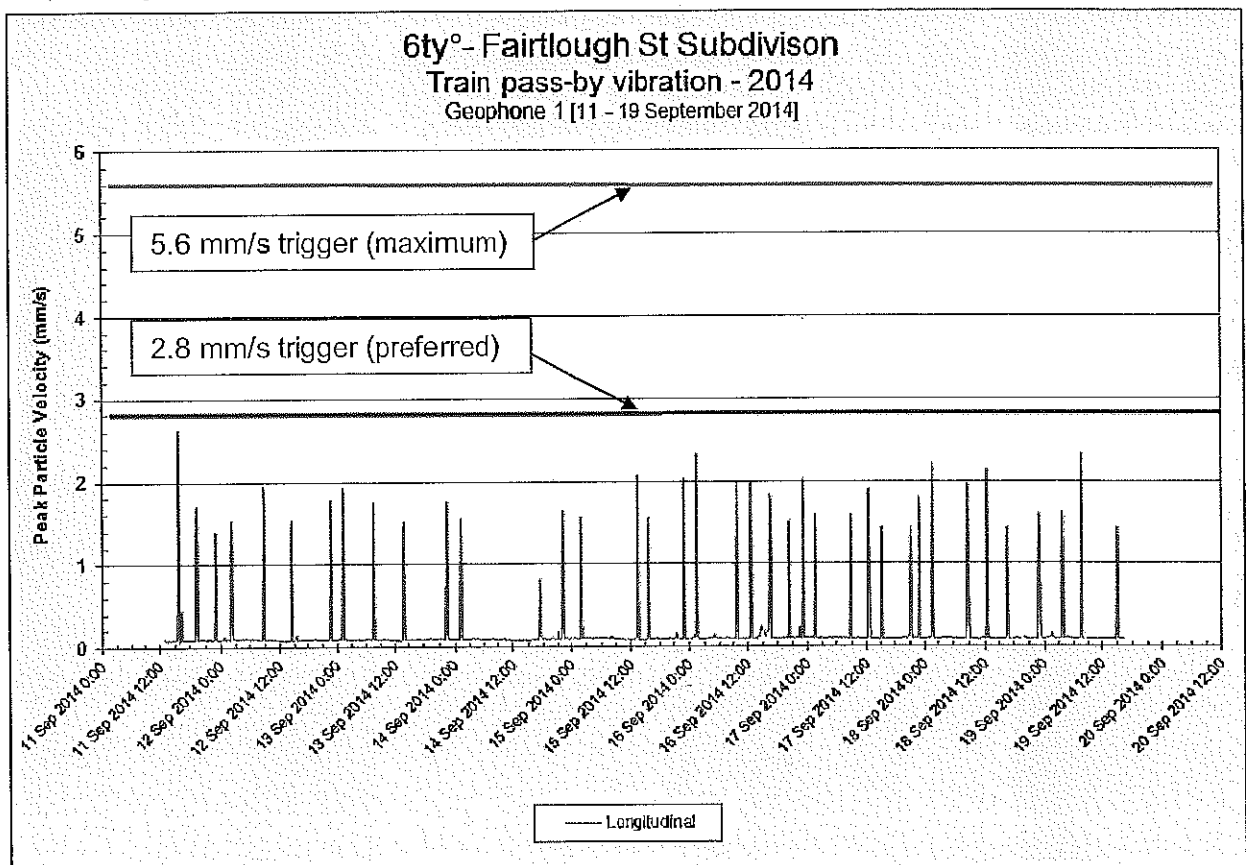


Figure 4 – Geophone 1 measured longitudinal PPV levels with guideline trigger levels.

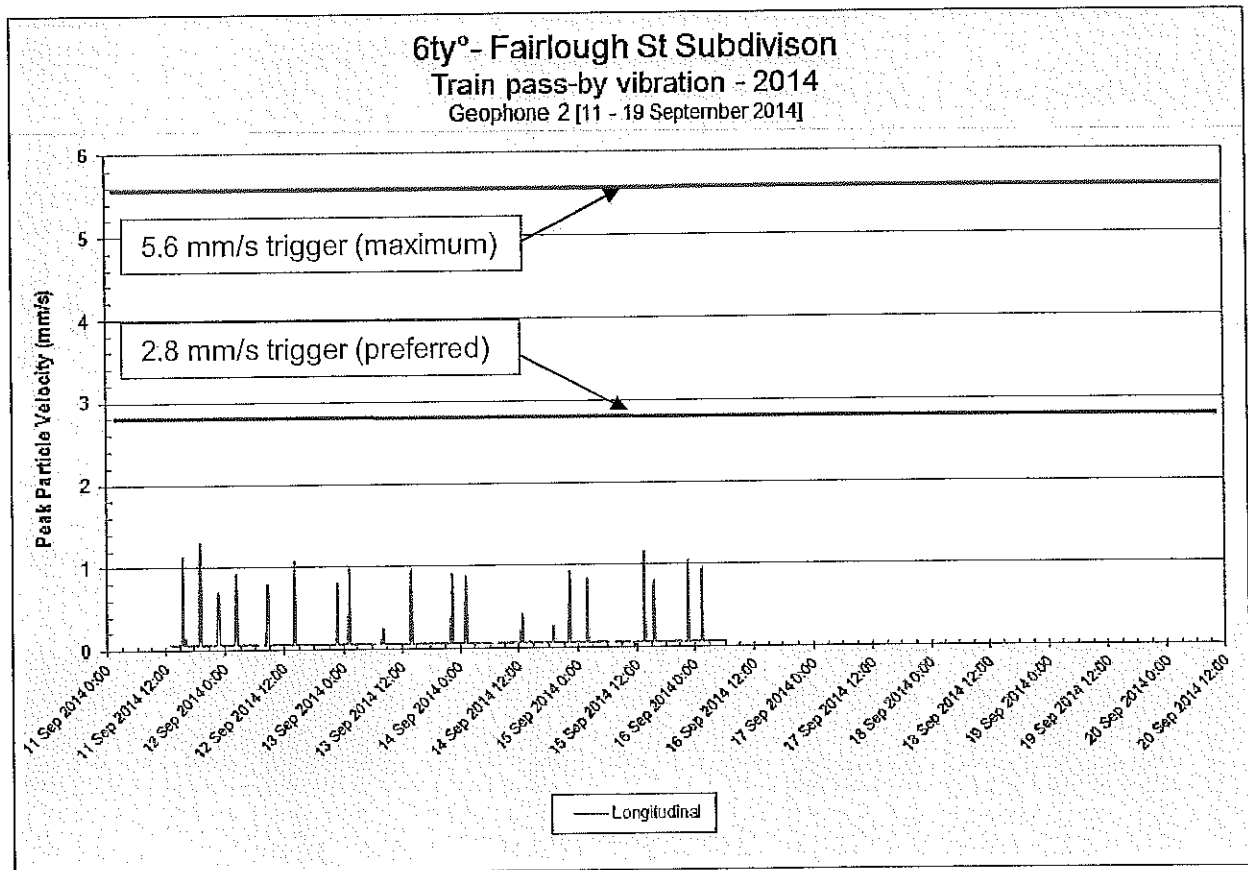


Figure 5 – Geophone 2 measured longitudinal PPV levels with guideline trigger levels.

From the above we note the following:-

- The highest vibration level measured at the site on train pass-by was 2.6 mm/s (peak velocity, longitude direction).
- The average longitudinal attenuation between the two geophone locations was 0.8 mm/s with a standard deviation of 0.3 mm/s.

#### 4.3. Recommendations

The measured ground vibration levels are within the criteria outlined above and therefore no recommendations are given here. At the levels measured vibration may be perceptible but highly unlikely to result in adverse health effects or structural damage to buildings.



## 5. AIR QUALITY ASSESSMENT

Air quality prediction was conducted using AusRoads a Gaussian plume line-source model developed by the Victorian EPA. Contaminants of concern from the combustion of diesel fuel by diesel electric locomotives on the western line were considered in this assessment.

**NB:** Coal dust emissions from rail transport of materials was not predicted in this assessment. No more than one coal train is likely on a given day and previous investigations in other jurisdictions have shown emission of dust from coal trains to be of minimal impact (see *QLD Dept of Science, Information Technology, Innovation and Arts: Tennyson Dust Monitoring Investigation, September 2012 to October 2012*).

### 5.1. Assessment criteria

Contaminants of concern were determined from the Federal Governments *National Pollution Inventory (NPI) - Emissions Estimation Technique Manual for Aggregated Emissions from Railways (EETMAER)*. Criteria for each contaminant were then determined from the *Tasmanian EPP (Air Quality) 2004* (where criteria were not available in the Tasmanian EPP then criteria were determined from the *Victorian State Environment Protection Policy-Air Quality Management [SEPP-AQM] 2001*).

Table 1 provides criteria for the major contaminants listed in the NPI-EETMAER.

Air quality criteria			
Contaminant	Tas EPP Criteria	Averaging time	Comments
Nitrogen dioxide (NO <sub>2</sub> )	0.16 ppm	1-hour	
Sulphur dioxide (SO <sub>2</sub> )	0.2 ppm	1-hour	
Carbon monoxide (CO)	9 ppm	8-hours	
Particulate matter (PM10)	0.15 mg/m <sup>3</sup>	24-hours	
Benzene	0.033 ppm	3-minutes	
Polycyclic Aromatic Hydrocarbons (PAHs)	0.5 µg/m <sup>3</sup>	1-hour	From Vic SEPP-AQM 2001

Table 1 – Air quality criteria.

### 5.2. Predicted concentrations

The model settings were as follows in table 1 below.

Model parameter settings			
Parameter	Setting	Parameter	Setting
<i>Anemometer height</i>	10 m	<i>Horizontal dispersion</i>	Pasquill Gifford
<i>Met. site roughness height</i>	0.3 m	<i>Wind Exponent</i>	Irwin Rural
<i>Sigma theta av. period</i>	60-mins	<i>Land use surface roughness</i>	Flat rural

Table 1 – model parameter settings.

Figure 6 below presents an aerial view of the proposed subdivision with the track length modelled for air emission dispersion marked.



Figure 6 – Aerial view of the proposed subdivision with the track length modelled for air emission dispersion marked.

Weather data (Ausplume Metfile) for the model was predicted using a The Air Pollution Model (TAPM), developed by the CSIRO, with the run centred on the proposed subdivision for 2013. (Vipac can provide this file upon request).

Emission rates for locomotives operating on the Western Line were determined from fuel usage data for the new TasRail TR locomotives (data supplied by TasRail) and emission factors provided in the NPI-EETMAER. Fuel usage data for other locomotive models operating on the Western Line was not made available to Vipac.

The following assumptions were made with regard to locomotive operations along the modelled track length:-

- Three locomotives operating per train.
- All locomotives operating at notch 4.
- Train speed along the track length 35 km/h.
- Max no. trains per hour 1.

Two scenarios were modelled for the prediction of contaminant concentration over a year depending on the averaging time required under the assessment criteria:-

1. **1-hr and 3-min averaging:** 1 locomotive every hour.
2. **8-hr and 24-hr averaging:** 1 locomotive at each of the following times; 10 am, 2 pm; 10 pm and 2 am.

Train scheduling information for the Western Line was not provided to Vipac.

Figure 7 provides an aerial view of the proposed subdivision with receptor locations at which ground level concentrations (glc) of contaminants were predicted for the above scenarios.

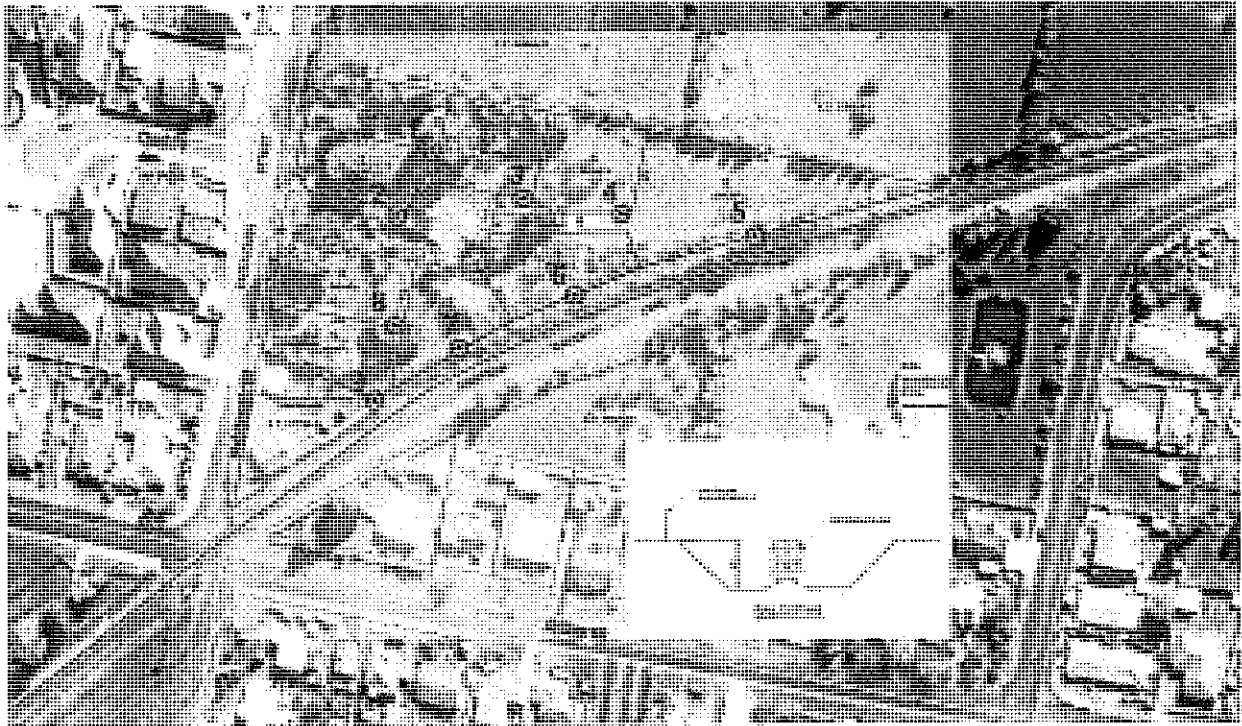


Figure 7 – Aerial view of the proposed subdivision with receptor locations for the prediction of glc's.

Table 2 below provides 100<sup>th</sup> percentile glc's for the major contaminant of concern for each of the receptor locations shown in figure 7.

Air quality prediction results						
Receptor	NO <sub>2</sub> * (1-hr av, ppm)	SO <sub>2</sub> (1-hr av, ppm)	CO (8-hr av, ppm)	PM10 (24-hr av, mg/m <sup>3</sup> )	Benzene (3-min av, ppm)	PAH's (1-hr av, µg/m <sup>3</sup> )
1	0.003	0.001	0.001	0.0001	0.00003	0.021
2	0.004	0.001	0.001	0.0001	0.00003	0.026
3	0.005	0.002	0.001	0.0002	0.00004	0.035
4	0.008	0.003	0.002	0.0002	0.00007	0.055
5	0.017	0.005	0.005	0.0006	0.00013	0.109
6	0.016	0.005	0.005	0.0005	0.00013	0.107
7	0.017	0.005	0.004	0.0005	0.00014	0.111
8	0.008	0.002	0.002	0.0002	0.00006	0.049
9	0.017	0.005	0.004	0.0004	0.00014	0.114

\* NO<sub>2</sub> assumed to be 10% of NO<sub>x</sub>

Table 2 – 100<sup>th</sup> percentile glc's for the major contaminant of concern at each receptor.

From the above we note the following:-

- All glc's are below the assessment criteria.

**NB:** This was also seen for the remaining contaminants listed in the NPI-EETMAER where assessment criteria were available for a contaminant.

### 5.3. Recommendations

The predicted glc's presented above are well below the assessment criteria and therefore no mitigation recommendations are given here.

6ty° – Fairtlough St subdivision rail noise, ground vibration and air quality assessment.

I hope this information meets your immediate requirements.

Please contact me directly if you have any questions concerning this work.

Yours faithfully,  
VIPAC ENGINEERS & SCIENTISTS LTD

Alex McLeod

Dr. Alex McLeod  
Senior Consultant – Tasmania  
p. +61 3 6343 2077  
m. +61(0)439 357 297  
f. +61 3 6343 4849  
email: [alex.mcleod@tarkarri.com](mailto:alex.mcleod@tarkarri.com)

ATTACHMENT <sup>1-369</sup> B

To The General Manger  
Northern Midlands Council

From  
Brian & Michelle Calder  
13-15 Seccombe Street  
Perth. Tas. 7300  
ref: P14-235

I am writing in relation to the above application of which we share 100 metres of boundary on the southern side of Perth. Whilst we do not have a problem with the subdivision & the removal of the trees on the property of which some gum trees are in our opinion dangerous. Some large limbs have fallen towards our property and we are lucky not to have lost any fences, we are worried about the issue of having our property boundary open and our property susceptible to any dogs or people having open access. Also the noise from the work being done is a worry to us. The existing

post and wire fence is falling down and once work begins on the trees it will fall down.

As we will adjoin 4 blocks our property could be open for years, some blocks may sell and some may be on the market for years. Our request to the developer is that they erect either a colour bond or paling fence the length of our boundary at their cost. We have some smallish trees and an internal 4 wire fence which will give us no privacy or our animals any safety from the development. As we keep horses we are very concerned about their safety. A fence would give us peace of mind that they will be safe during any work that will be done. This to us doesn't seem to be an exorbitant request.

During the work on other subdivisions around us we have had rubbish blown onto our property from building sites and a fence would alleviate this problem as well. The amount of rubbish we have had to pick up

over the years has been huge.

The only other request we have is that when the tree felling starts we be given 24 hours notice to allow us to put our ponies in safe yards to prevent injuries to them, again I don't think this is too much to ask.

As we have put a lot of time and effort to get our property to where we want it we believe the gesture of a fence by the developer to be an act of goodwill to help us maintain our lifestyle and the safety of our animals.

Should you wish to discuss this matter or if the developer would like to talk about the above matters we can be contacted on the following numbers

Home - evenings 63981083

Brian 0458 396 857

Michelle mobile 0408 127 008

Regards

Brian and Michelle Calder

## Submission to Planning Authority Notice

Council Planning Permit No.	P14-235	Council notice date	17/11/2014
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### TasWater details

TasWater Reference No.	TWDA 2014/01279-NMC	Date of response	01/12/2014
TasWater Contact	David Boyle	Phone No.	6345 6323

### Response issued to

Council name	NORTHERN MIDLANDS COUNCIL
Contact details	planning@northmidlands.tas.gov.au

### Development details

Address	105 FAIRTLOUGH ST, PERTH	Property ID (PID)	6743016
Description of development	Subdivision of 9 lots		

### Schedule of drawings/documents

Prepared by	Drawing/document No.	Revision No.	Date of Issue
6ty ° (Bullock Consulting)	14.155 Dwg PO1	A (issue 2)	13/11/2014

### Conditions

Pursuant to the *Water and Sewerage Industry Act 2008 (TAS)* Section 56P(1) TasWater imposes the following conditions on the permit for this application:

#### CONNECTIONS, METERING

1. A suitably sized water supply with metered connections / sewerage system and connections to each lot of the development must be designed and constructed to TasWater's satisfaction and be in accordance with any other conditions in this permit.
- Any removal/supply and installation of water meters and/or the removal of redundant and/or installation of new and modified property service connections must be carried out by TasWater at the developer's cost.

#### ASSET CREATION & INFRASTRUCTURE WORKS

3. Plans submitted with the application for Engineering Design Approval must show all existing, redundant and/or proposed property services and mains, to the satisfaction of TasWater.
4. Prior to applying for a Permit to Construct to construct new infrastructure the developer must obtain from TasWater Engineering Design Approval for new TasWater infrastructure. The application for Engineering Design Approval must include engineering design plans prepared by a registered professional engineer showing the hydraulic servicing requirements for water and sewerage to TasWater's satisfaction.
5. Prior to works commencing, a Permit to Construct must be applied for and issued by TasWater. All infrastructure works must be inspected by TasWater and be to TasWater's satisfaction.
6. All works must be constructed under the supervision of a qualified engineer in accordance with TasWater's requirements.
7. Prior to the issue of a Consent to Register a Legal Document additions, extensions, alterations or upgrades to TasWater's water and sewerage infrastructure required to service the development, generally as shown on the concept servicing plan "14.155 PO1 Rev A", are to be at the expense of the developer and performed a contractor approved by TasWater, to the satisfaction of TasWater.
8. After testing/disinfection, to TasWater's requirements, of newly created works, the developer must apply to TasWater for connection of these works to existing TasWater infrastructure, at the developer's cost.
9. At practical completion of the water and sewerage works and prior to TasWater issuing a Consent to a



Register Legal Document, the developer must obtain a Certificate of Practical Completion from TasWater for the works that will be transferred to TasWater. After the Certificate of Practical Completion has been issued, a 12 month defects liability period applies to this infrastructure. During this period all defects must be rectified at the developer's cost and to the satisfaction of TasWater. A further 12 month maintenance period may be applied to defects after rectification. TasWater may, at its discretion, undertake rectification of any defects at the developer's cost. To obtain a Certificate of Practical Completion:

- a) Written confirmation from the supervising qualified engineer certifying that the works have been constructed in accordance with the TasWater approved plans and specifications and that the appropriate level of workmanship has been achieved;
  - b) A request for a joint on-site inspection with TasWater's authorised representative must be made;
  - c) Security for the twelve (12) month defects liability period to the value of 10% of the works must be lodged with TasWater. This security must be in the form of a bank guarantee;
  - d) As constructed drawings must be prepared by a qualified surveyor to TasWater's satisfaction and forwarded to TasWater.
10. Upon completion, to TasWater's satisfaction, of the defects liability period the developer must request TasWater to issue a "Certificate of Final Acceptance". The newly constructed infrastructure will be transferred to TasWater upon issue of this certificate and TasWater will release any security held for the defects liability period.
  11. The developer must take all precautions to protect TasWater infrastructure. Any damage caused to TasWater infrastructure during the construction period must be promptly reported to TasWater and repaired by TasWater at the developer's cost.
  12. Ground levels over the TasWater assets and/or easements must not be altered without the written approval of TasWater.

## FINAL PLANS, EASEMENTS & ENDORSEMENTS

13. Prior to the Sealing of the Final Plan of Survey, the developer must obtain a Consent to Register a Legal Document from TasWater and the certificate must be submitted to the Council as evidence of compliance with these conditions when application for sealing is made.
14. Pipeline easements, to TasWater's satisfaction, must be created over any existing or proposed TasWater infrastructure and be in accordance with TasWater's standard pipeline easement conditions.

## HEADWORKS CHARGES

### ADVICE

If the final plan of survey is lodged with Council and practical completion for water and sewerage infrastructure has been met for the relevant stage(s) in the period 1 April 2014 to 31 March 2016 the headworks amount(s) will be waived in line with the prevailing State Government Policy. Please visit [www.development.tas.gov.au](http://www.development.tas.gov.au) for further information.

### CONDITION

15. Prior to TasWater issuing a Consent to Register Legal Document, the applicant or landowner as the case may be, must pay a headworks charge of \$12,895.92 to TasWater for water infrastructure for 8.0 additional Equivalent Tenements, indexed as approved by the Economic Regulator from the date of this Submission to Planning Authority Notice until the date it is paid to TasWater.
16. Prior to TasWater issuing a Consent to Register Legal Document, the applicant or landowner as the case may be, must pay a headworks charge of \$13,209.36 to TasWater for sewerage infrastructure for 8.0 additional Equivalent Tenements, indexed as approved by the Economic Regulator from the date of this Submission to Planning Authority Notice until the date it is paid to TasWater.

## DEVELOPMENT ASSESSMENT FEES

17. The applicant or landowner as the case may be, must pay a development assessment and Consent to register a Legal Document fee to TasWater for this proposal of:
  1. \$543.10 for a medium development assessment; and
  2. \$257.00 for Consent to register a Legal Document

# TasWater

as approved by the Economic Regulator and the fees will be indexed as approved by the Economic Regulator until the date they are paid to TasWater. The payment is required within 30 days of the issue of an invoice by TasWater which will be when the Consent to Register a Legal Document is issued / an application for Engineering Design Approval is made.

20. In the event Council approves a staging plan, a Consent to Register a Legal Document fee for each stage, must be paid commensurate with the number of Equivalent Tenements in each stage, as approved by Council.

## Advice

For information on TasWater development standards, please visit <http://www.taswater.com.au/Development/Development-Standards>

For information regarding headworks, further assessment fees and other miscellaneous fees, please visit <http://www.taswater.com.au/Development/Fees---Charges>

For detailed information on how headworks have been calculated for this development please contact the TasWater contact as listed above.

For application forms please visit <http://www.taswater.com.au/Development/Forms>

The developer is responsible for arranging to locate existing TasWater infrastructure and clearly showing it on any drawings. Existing TasWater infrastructure may be located by TasWater (call 136 992) on site at the developer's cost, alternatively a surveyor and/or a private contractor may be engaged at the developers cost to locate the infrastructure.

## Declaration

The drawings/documents and conditions stated above constitute TasWater's Submission to Planning Authority Notice.

If you need any clarification in relation to this document, please contact TasWater. Please quote the TasWater reference number. Phone: 13 6992, Email: [development@taswater.com.au](mailto:development@taswater.com.au)

## Authorised by



Jason Taylor

Development Assessment Manager

**REFERRAL OF DEVELOPMENT APPLICATION P14-235  
to Works & Infrastructure Department**

Subdivision No: **27/003/712**  
 Property no: 104900.07  
 Date: 17-Nov-2014  
 Applicant: 6tyo (obo Shervan)  
 Proposal: 9-lot subdivision including cul de sac (within 50m of railway) & dwelling demolition  
 Location: 115 Fairtlough Street, Perth  
 Engineering fees of \$1000 paid.

Please inspect the property and advise regarding stormwater/drainage, traffic/access, and any other engineering concerns.

Is there is a house on one of the lots?	<i>Yes, to be demolished</i>
Is it connected to all Council services?	<i>No</i>
Are any changes / works required to the house lot?	<i>No</i>
Are the discharge points for stormwater, infrastucture that is maintained by Council? (This requires a check to ensure the downstream infrastructure is entirely owned, maintained, operated by Council and have been taken over as Council assets.)	<i>Yes</i>

**Stormwater:**

Is the property connected to Council's stormwater services?	<i>No</i>
If so, where is the current connection/s?	<i>N/A</i>
Can all lots access stormwater services?	<i>Unkown</i>
If so, are any works required?	<i>No</i>
<b>Stormwater works required:</b>	
<i>Works to be in accordance with Standard Drawing-- TSD SW 27, a 100mm stormwater connection.</i>	
Is there kerb and gutter at the front of the property?	<i>No</i>
Are any kerb-and-gutter works required?	<i>Yes, design plan to be provided by applicant</i>

**Road Access:**

Does the property have access to a made road?	<i>Yes</i>
If so, is the existing access suitable?	<i>Yes</i>
Does the new lot/s have access to a made road?	<i>Yes</i>
If so, are any works required?	<i>No</i>
Is off-street parking available/provided?	<i>Yes</i>
<b>Road / access works required:</b>	
<i>Works to be in accordance with Standard Drawing TSD-R09 - concrete driveway crossover &amp; hotmix sealed apron from the edge of Road to the property boundary of each lot</i>	
Is a vehicular crossing application form required?	<i>Yes</i>
Is a footpath required?	<i>Yes, to be shown on design plan</i>
Extra information required regarding driveway	<i>No</i>

approach and departure angles	
<b>Are any road works required:</b>	<i>Yes, to be shown on design plan</i>
<b>Are Street Trees required?</b>	No
<b><u>Additional Comments:</u></b>	An Engineer's design is required.

Engineer's comment:

**Works & Infrastructure Department conditions – access & stormwater**

W.1 Stormwater

- o Each lot shall be provided with a connection to the Council's stormwater system, constructed in accordance with Council standards and to the satisfaction of Council's Works & Infrastructure Department.
- o A stormwater design plan including long sections and the depth, size and grade of all mains is to be provided to Council prior to the commencement of any works on site.
- o Calculations shall be provided to demonstrate that the system is of sufficient capacity to drain the road and all lots to be created.

W.2 Access (Urban)

- o A concrete driveway crossover and concrete apron shall be constructed for each lot from the edge of the street to the property boundary in accordance with Council standards.

W.3 Roadworks

- o Road widening shall be carried out in Fairtlough St in accordance with Tasmanian Standard Drawing TSD-R06 and kerb and channel shall be installed at the frontage of each lot.
- o A 1.8m wide concrete footpath shall be constructed on one side of the Cul de Sac to service all lots.
- o An engineering design of the road and drainage system including pavement long sections and cross sections is to be approved by Council before the commencement of works on site

W.3 As constructed information

As Constructed Plans and Asset Management Information shall be provided in accordance with Council's standard requirements.

W.4 Municipal standards & certification of works

Unless otherwise specified within a condition, all works shall comply with the Municipal Standards including specifications and standard drawings. Any design shall be completed in accordance with Council's subdivision design guidelines to the satisfaction of the Works & Infrastructure Department. Any construction, including maintenance periods, shall also be completed to the approval of the Works & Infrastructure Department.

W.5 Works in road reserve

No works shall be undertaken within the public road reserve, including crossovers, driveways or kerb and guttering, without prior approval for the works by the Works & Infrastructure Manager. Twenty-four hours (24) notice shall to be given to the Works & Infrastructure Department to inspect works within road reserve and before placement of concrete or seal. Failure to do so may result in rejection of the vehicular access or other works and its reconstruction.

W.6 Hydraulic separation

- o Any existing pipes and stormwater connections shall be located where required pipes are to be rerouted to provide an independent system for each lot.
- o Certification shall be provided that hydraulic separation between the all lots has been achieved.

W.7 Easements to be created

Easements shall be created over all Council-owned services in favour of the Northern Midlands Council. Such easements shall be created on the final plan to the satisfaction of the Planning & Development Manager.

W.8 Pollutants

- o The developer/property owner shall be responsible for ensuring pollutants such as mud, silt or chemicals are not released from the site.
- o Prior to the commencement of the development works the developer/property owner must install all necessary silt fences and cut-off drains to prevent soil, gravel and other debris from escaping the site. No material or debris is to be transported onto the road reserve (including the naturestrip footpath and road pavement). Any material that is deposited on the road reserve shall be

removed by the applicant. Should Council be required to clean or carry out works on any of their infrastructure as a result of pollutants being released from the site the cost of these works may be charged to the developer/property owner.

W.9 Bonds

The subdivision shall be subject to a maintenance period and a bond shall be held by Council until the completion of the maintenance period. The bond shall be calculated based on 5% of the total cost of works based on Council's standard road construction rates.

W.10 Naturestrips

Any new naturestrips, or areas of naturestrip that are disturbed during construction, shall be topped with 100mm of good quality topsoil and sown with grass. Grass must be established and free of weeds prior to Council accepting the development.

*Jonathon Galbraith (Works & Infrastructure Officer)*

*Date: 20/11/14*

TR Ref: 2014 – TR/NMC – 0222

Council Reference: 104900.07;s712  
P14-235Level 12, 39 Murray Street  
Hobart TAS 7001  
PO Box 335  
Kings Meadows TAS 7249  
T 03 6227 5212  
F 03 6227 5220  
E [landmanagement@tasrail.com.au](mailto:landmanagement@tasrail.com.au)General Manager  
Northern Midlands Council  
PO Box 156  
Longford TAS 7301  
Email: [Planning@nmc.tas.gov.au](mailto:Planning@nmc.tas.gov.au)

Dear Council,

**RE: APPLICATION FOR PLANNING PERMIT FOR LAND ADJOINING RAILWAY ASSET****Note: This is advice and not a representation or an objection to the planning  
Application Number: P14-235**

Thank you for Council's notification received by email on 17 November 2014 regarding an application for a new development: **9-lot subdivision including cul de sac (within 50m of railway)**.

The rail infrastructure in the state is a critical part of the transport infrastructure and capacity. It must be assumed in the consideration of any development that rail services will be maintained and even increased. The railway at the location of the proposed development is currently part of Tasmania's operational rail and it should therefore be noted that property in proximity to the rail corridor can be adversely affected by a number of factors, these include:

**Night time noise**

- Train operations are undertaken at any time of the day or night. This is not expected to change.
- Any development must take this into consideration

**Vibration and noise**

Train operations create vibration and noise. Any new development must take this into consideration as it may impact on quality of life. Building structure design should consider these issues. We recommend that buildings are located as far as possible from the rail corridor;

- Please read the attached *Environment Protection Authority (noise) Policy 2009. Part 4*; transport infrastructure, public roads, railways, ports & airports.

**Drainage**

- Water creates major problems for rail formations and must be controlled within the rail corridor. As a result, a licence is required from TasRail for any proposed drainage onto the rail corridor from an adjoining property;
- In certain circumstances drainage from the rail corridor onto adjoining property is permitted and may impact the adjoining property.

### **Line of sight**

- From a safety perspective line of sight is important when operating a railway. New developments that are going to impact on the line of sight must be assessed by the rail operator. This can include proposed planting/growth of vegetation and building structures.

### **Level crossing**

- Line of sight is critical for level crossings and must be maintained.

### **Safety issues are addressed**

- Access to the rail corridor is limited to people with permission;
- A 3m safety exclusion zone exists along the railway line.

### **Traffic**

- We would recommend that the council consider any changes to traffic volumes in regards to current crossing capacity and design within the locality of any development proposal.

### **Access to corridor**

- Residents need to be aware that access to the rail corridor needs to be approved and that any use of the rail corridor needs to be licensed. This includes such issues as drainage, water pipes, electrical infrastructure, crossings or the like. It must also be stressed that there is, under all circumstances, a 3m safety exclusion zone exists from the edge of the nearest rail out each side;
- Access to the rail corridor requires relevant safety methods in place and subsequent approval by TasRail must be sought.

### **Access to property across the rail corridor**

- All crossings must be licensed; conditions appropriate to the location will apply. It is illegal to build and access a private rail crossing without a licence.

1-380

Providing that the applicant is made aware of the issues detailed above, TasRail can see no reason why this development should not proceed.

If Council or the applicant would like to discuss this matter further please contact me on (03) 6227 5212 or by email [landmanagement@tasrail.com.au](mailto:landmanagement@tasrail.com.au)

Yours sincerely,



Michael Ince  
**A/MANAGER**  
**PROPERTY SERVICES**

27 November 2014



## PART 4 – TRANSPORT INFRASTRUCTURE

### Public roads, railways, ports & airports

11. (1) it is recognised that although the operation or use of public roads, railways, ports or airports may prejudice protection of the environmental values, the function the transport network serves is necessary for the community's economic, environmental and social wellbeing.

(2) Notwithstanding sub-clause (1), it is intended that -

- (a) Transport planning initiatives for freight and passenger movement and new transport infrastructure be developed in a systematic way to achieve an optimal balance of economic, environmental and social benefits and costs with major criterion of minimising the number of people exposed to noise levels that would prejudice protection of the environmental values; and
  - (b) Where environmental values are acutely prejudiced, existing transport infrastructure noise should be reduced to the greatest extent that is reasonably practical, consistent with achieving an optimal balance of economic, environmental and social benefits and costs.
- (3) The allocation of any public resources to minimise noise impacts resulting from public roads, railways, ports or airports shall aim to achieve the most benefit for the greatest number of people exposed to those impacts.
- (4) A transport noise strategy will be developed to improve transport noise outcomes, further the objectives of the Act and assist in implementation of subclauses (2) and (3)

**PLAN 4**

**PLANNING APPLICATION P14-048  
643 RELBIA ROAD, RELBIA**

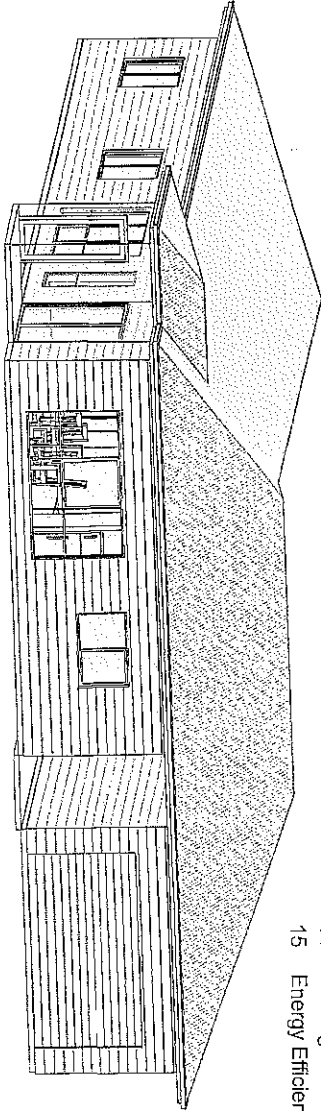
**ATTACHMENTS**

- A Proposal Plans and Reports**
- B Representation & applicant's response**

# ATTACHMENT A

Sheet List

01	Cover Page	11-11-14
02	Construction Notes	11-11-14
03	Site Plan	11-11-14
04	Blow Up Site Plan	11-11-14
05	Existing Floor Plan	11-11-14
06	Proposed Floor Plan	11-11-14
07	Proposed Roof Plan	11-11-14
08	North & East Elevations	11-11-14
09	South & West Elevations	11-11-14
10	Elevations Showing Shed & Containers	11-11-14
11	Section A	11-11-14
12	Section B	11-11-14
13	Slab Setout Plan	11-11-14
14	Drainage Plan	11-11-14
15	Energy Efficiency	11-11-14



## PROPOSED NEW RESIDENCE FOR MR & MRS SHEPHERDSON

- Soil Classification
- T.B.A
- Design Wind Speed
- T.B.A
- Energy Rating
- Refer Energy Assessment attached.
- BAL Rating BAL 19
- Refer details on page 02.

Climate Zone 7

EXHIBITED

ENGINEERING DISCLAIMER: STRUCTURAL ENGINEERS CERTIFICATES MAY BE REQUIRED TO COVER AREAS OF STRUCTURAL DESIGN AND CONDITIONS AS/OF SOIL CONDITIONS. THIS WORK IS PROVIDED AS GENERAL INFORMATION ONLY. IT IS NOT TO BE USED AS A BASIS FOR ANY OTHER DESIGN OR CONSTRUCTION. THE USER OF THIS DRAWING IS ADVISED THAT THIS PLAN HAS BEEN DRAWN TO COMPLY WITH THE REQUIREMENTS OF THE LOCAL AUTHORITIES. LATERAL ARCHITECTURE HOWEVER DOES NOT ACCEPT ANY RESPONSIBILITY FOR ANY CONSTRUCTION DEFECTS OR OMISSIONS IN THE PLANS DUE TO WORKING WITHOUT SUFFICIENT INFORMATION. NOR FOR MISINTERPRETATION OR INADEQUATE INFORMATION. IMPORTANT: IF DRAWING NOT LODGED OR PROCESSED WITH COUNCIL WITHIN 12 MONTHS OF LAST REVISION DATE, THEN PLEASE RETURN FOR REVIEW BEFORE PROCEEDING.

DO NOT SCALE DRAWINGS. IF IN DOUBT - ASK

LOCAL COUNCIL : LANCASTON CITY COUNCIL
ACREDITATION COMPLIANCE NUMBER=OC-2892 Z
TITLE: PROPOSED NEW RESIDENCE AT 643 RELBIA ROAD RELBIA FOR MR & MRS SHEPHERDSON
FOLIO REFERENCE : 130344

JOB NO:	LA-14-22
DRAWING NO:	P1
SCALE:	O 1
DATE:	22-04-14
REVISOR:	R. HALL
CHECKER:	R. HALL
DESIGNER:	R. HALL

**Lateral ARCHITECTURE**

Holloway Trust  
7/4 Lateral Architecture  
10/4 Invermay Road  
INVERMAY, Tairāwhiti, 7248  
New Zealand  
Telephone: (03) 6226 0560  
lateral@lateral.com.nz

**Lateral ARCHITECTURE**

**CONSTRUCTION NOTES:**

- PLASTER:
- PLASTERBOARD INTERIOR LININGS, ON METAL TOP/HAT SECTIONS @900CS.
- WALLBOARD 2 IN WET AREAS.
- SCOTIA CORNICE MOLDING TO CEILING AND WALL JOINTS WHERE REQUIRED BY OWNER.
- WINDOW AND EAVE LINETS:

- TIMBER WINDOW HEADS NOT UNDER ROOF POINT LOAD.
- 180 X 46 F17 SPAN 3000
- 140 X 46 F17 SPAN 2200
- FOR LINETS WHICH SUPPORT ROOF LOADS REFER TO ENGINEER
- PREVENTION OF FALLS:

WHERE A PERSON IS EXPOSED TO THE HAZARD OF FALLING FROM A STRUCTURE DURING CONSTRUCTION OR WHILE CLEANING OR MAINTENANCE WORK IS CARRIED OUT, THE BUILDER SHALL PROVIDE:

- A WORK SYSTEM DESIGNED TO PREVENT SUCH FALLS.
- WHERE A SAFETY ANCHORAGE POINTS ARE USED THEY MUST BE POSITIONED ON THE STRUCTURE SO THAT ALTHOUGH OR SAFETY HARNESS MAY BE ATTACHED BEFORE PROCEEDING TO A POINT WHERE IT IS POSSIBLE TO FALL.
- ANCHORAGE POINTS FOR THE ATTACHMENT OF SAFETY HARNESS MUST COMPLY WITH AS2686.
- THE ANCHORAGE POINTS AND ASSOCIATED STRUCTURE SHALL BE CAPABLE OF WITHSTANDING A FORCE OF AT LEAST 16KN (1500KG).
- THE BUILDER SHALL INFORM THE OWNER PRIOR TO OCCUPANCY OF THE BUILDING THAT A FALL ARREST SYSTEM IS CONSTRUCTED AND MUST BE USED IN ACCORDANCE WITH AS2686 WHEN EXPOSED TO THE HAZARD OF FALLING FROM THE BUILDING OR STRUCTURE.

**STAIR NOTE:**

- MINIMUM REQUIREMENTS:
- START TO COMPLY WITH LATEST N.C.C. CLAUSE 3.9.1 AND COUNCIL STANDARDS.
- 190mm MAXIMUM RISE, 280mm RUN, 275mm TREAD, 100mm BALLUSTRADE, TO COMPLY WITH N.C.C. PART 3.9.2
- ENSURE 200mm MINIMUM HEAD CLEARANCE FROM TOE OF TREAD TO OPENING ABOVE.

**SPECIFIC NOTES:**

- WATER RESISTANT PLASTERBOARD OR HERRIES WALLBOARD TO BE USED IN ALL WET AREAS.
- FLOOR TO BE TREATED WITH APPROPRIATE WATERPROOFING PRODUCTS TO ENSURE COMPLIANCE WITH N.C.C. PART 3.8.1 AND AUSTRALIAN STANDARD AS 3740.
- 190MM METAL VENTS (230 X 75) TO BE USED ALL AROUND SUB-FLOOR. MAXIMUM SPACING 1048mm, 800mm FROM EACH END, TO COMPLY WITH N.C.C. CLAUSE 3.4.1.
- COLOPOOND CLADDING FITTED TO ROOF AND WALL CLADDING AND N.C.C. 3.5.1.

ENGINEERING DESIGNER: SHEPHERDSON ARCHITECTURE  
 SHEPHERDSON ARCHITECTURE  
 17/2 Inverloch Road  
 Inverloch, Victoria 3798  
 Ph: 5242 7777  
 Fax: 5242 7778  
 Email: info@shepherdson.com.au  
 www.shepherdson.com.au

**BUILDERS NOTES:**

- DO NOT SCALE DRAWINGS
- CHECK ALL DIMENSIONS AND VERIFY LEVELS, PRIOR TO SETOUT AND COMMENCEMENT OF ANY BUILDING WORK
- ANY NOTED DISCREPANCIES ON ANY DRAWINGS PERTAINING TO THIS PROJECT SHOULD BE NOTIFIED TO THE DESIGNER BEFORE ANY FURTHER WORKS CONTINUE.
- ALL TIMBER FRAMING TO COMPLY WITH THE CURRENT AS1894 (RESIDENTIAL TIMBER FRAMED CONSTRUCTION SIMPLIFIED - NON CYCLONIC AREAS) AND THE N.C.C AND LOCAL COUNCIL REQUIREMENTS.
- ALL CONSTRUCTION TO COMPLY WITH THE LATEST N.C.C BUILDING CODE OF AUSTRALIA
- ALL DRAINAGE TO COMPLY WITH LATEST N.C.C PLUMBING CODE OF AUSTRALIA, STANDARDS AUSTRALIA AND LOCAL COUNCIL DRAINAGE AND PLUMBING REQUIREMENTS.
- SUPPLY HARDWARE AND LINKED SMOKE DETECTORS WITH BATTERY BACKUP\* WHERE REQUIRED UNDER N.C.C 3.7.2
- ALL WET AREA ROOMS, SUCH AS W.C, BATHROOMS AND LAUNDRIES WHICH DO NOT CONTAIN PERMANENT VENTILATED WINDOWS, MUST BE INSTALLED WITH A RIGID WIRE IN EXHAUST FANS TO COMPLY WITH N.C.C 3.8.5 FOR VENTILATION

- SLAB AND FOOTING NOTE
- SLABS AND FOOTINGS DESIGNED IN ACCORDANCE WITH AS 2870, TO 4<sup>th</sup> SOIL CLASSIFICATION, AND WIND CLASSIFICATION TO AS 4055 WIND RATING: NZ, V4 = 33m/s SHOULD SOIL TEST OR WIND CLASSIFICATION BE HIGHER THAN NOTED, REFER ENGINEER FOR DESIGN.

- WIND BRACING:
- ALL WALL AND ROOF BRACING TO COMPLY WITH CURRENT N.C.C PART 3.4.3 AND AS1894 (RESIDENTIAL TIMBER FRAMED CONSTRUCTION SIMPLIFIED - NON CYCLONIC AREAS)

- WIND BRACING:
- ALL WALL AND ROOF BRACING TO COMPLY WITH CURRENT N.C.C PART 3.4.3 AND AS1894 (RESIDENTIAL TIMBER FRAMED CONSTRUCTION SIMPLIFIED - NON CYCLONIC AREAS)

**WINDOW SCHEDULE NOTE:**

- 91816 = S: SLIDING, 18, 1800mm HIGH, 1515x10 WIDE
- A1509 = A: AWNING, 18, 1800mm HIGH, 605x910 WIDE
- D = DOUBLE HUNG, F = FIXED, C = CASEMENT.

**GROSS OVER NOTE:**

- NEW COUNCIL GROSS OVER TO BE DESIGNED AND CONSTRUCTED BY REGISTERED COUNCIL CONTRACTOR
- GROSS OVER DESIGNS ARE NOT IN LATERAL BUILDING DESIGNS OR SPATIAL ODDITY'S SCOPE OF WORK

ENGINEERING DESIGNER: SHEPHERDSON ARCHITECTURE  
 SHEPHERDSON ARCHITECTURE  
 17/2 Inverloch Road  
 Inverloch, Victoria 3798  
 Ph: 5242 7777  
 Fax: 5242 7778  
 Email: info@shepherdson.com.au  
 www.shepherdson.com.au

**BRACING AND TIEDOWN NOTES:**

- TIMBER FRAMING AND ATTACHMENTS:
- TIMBER FRAMING AND ATTACHMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH:
  - DETAILS IN SECTION 1 TO 7 OF AS1894.4
  - DETAILS SHOWN ON THE ENGINEER'S DRAWINGS AND MATERIALS SUPPLIERS DETAILS.
- BRACING OF THE TIMBER FRAME:
- BRACING OF THE TIMBER FRAME AND ATTACHMENT SHALL BE CONSTRUCTED IN ACCORDANCE WITH:
  - DETAILS IN SECTION 8 OF AS1894.4
  - DETAILS SHOWN ON THE ENGINEER'S DRAWINGS AND MATERIALS SUPPLIERS DETAILS.
- TIEDOWN OF THE TIMBER FRAME AND ATTACHMENTS:
- TIEDOWN OF THE TIMBER FRAME AND ATTACHMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH:
  - DETAILS IN SECTION 9 OF AS1894.4
  - DETAILS SHOWN ON THE ENGINEER'S DRAWINGS AND MATERIALS SUPPLIERS DETAILS.
  - NONNAIL FIXINGS AS SHOWN IN TABLE 8.3 SHALL APPLY AND ARE NOT REPEATED HERE.

- WIND BRACING:
- ALL WALL AND ROOF BRACING TO COMPLY WITH CURRENT N.C.C PART 3.4.3 AND AS1894 (RESIDENTIAL TIMBER FRAMED CONSTRUCTION SIMPLIFIED - NON CYCLONIC AREAS)

NOTE TO TRUSS MANUFACTURER:

- ALL ROOF TRUSS DESIGN & DETAILS TO BE SUPPLIED TO LOCAL AUTHORITY.
- PROVIDE LOCATION OF ALL GIRDER TRUSSES TO BUILDER TO ENSURE REINFORCEMENT OF LINETS OVER OPENINGS WHERE SUCH TRUSSES MAY FALL AND CREATE POINT LOAD.

**BUSHFIRE ATTACH LEVEL REQUIREMENTS:**

- COMPLY WITH N.C.C 3.7.4 & AS 3959-2009: CONSTRUCTION IN BUSHFIRE PRONE AREAS
- BAL 19 CONSTRUCTION REQUIREMENTS:
- SUB FLOOR - NO SPECIAL REQUIREMENTS
- FLOORS - NO SPECIAL REQUIREMENTS.
- WALLS - EXTERNAL WALLS - PARTS BELOW 400mm FROM GROUND OR LESS THAN 400mm ABOVE DECKS, CARPORT ROOFS, AWNINGS OR SIMILAR TO BE NON-COMBUSTIBLE MATERIAL OR 6mm FIBRE CEMENT SHEET OR BUSHFIRE RESISTANT TIMBER.
- EXTERNAL WINDOWS - WITHIN 400mm FROM GROUND, MINIMUM 5mm TOLERANCED GLASS OR MIN 4mm GRADE A SAFETY GLASS OR METAL SCREENS AS PER DOORS, WITH METAL OPENING SASHES.
- EXTERNAL DOORS - PROTECT EXTERNALLY, SUCH AS SCREENS WITH MESH MAXIMUM APERTURE OF 2mm MADE OF STEEL/BRONZE OR ALUMINIUM, WITH GAPS NO MORE THAN 3mm FROM METAL FRAMES.
- DOORS - SHALL BE NON-COMBUSTIBLE OR METAL OR 38MM SOLID TIMBER FOR 400mm ABOVE THRESHOLD, OR BUSHFIRE RESISTANT TIMBER FRAMED FOR 400mm ABOVE THE GROUND WITH TIGHT FITTING WEATHER STRIPS AT THE BASE.
- GARAGE DOORS - WITHIN 400mm OF GROUND, TO BE CONSTRUCTED FROM NON-COMBUSTIBLE MATERIAL, SUCH AS METAL, MUST BE FITTED WITH WEATHER STRIPS AND SEALS WITH GAPS NO MORE THAN 3mm, NOT TO HAVE VENTILATION SLOTS.
- ROLLER DOORS TO HAVE GUIDE TRACKS WITH MAXIMUM GAP NO MORE THAN 3mm.
- ROOFS - ROOF COVERINGS TO BE NON-COMBUSTIBLE MATERIAL, ALL ROOFWALL JOINTS TO BE SEALED, ALL OPENINGS OR PENETRATIONS TO BE FITTED WITH NON-COMBUSTIBLE EMERBER GUARDS, ROOF TO BE FULLY SARKED.
- WATER AND GAS SUPPLY PIPES - ALL EXPOSED WATER AND GAS SUPPLY PIPES ABOVE GROUND TO BE METAL.
- EXTERNAL DECKING, STAIR TREADS, LANDINGS, RAMPS, ETC. - FOR TRAFFICABLE SURFACES THAT ARE MORE THAN 300mm FROM GLAZED ELEMENTS, NO SPECIAL REQUIREMENTS.
- FOR LESS THAN 300mm FROM GLAZED ELEMENTS, THEY MUST BE NON-COMBUSTIBLE OR MADE OF FIRE RESISTANT MATERIAL.
- BALLUSTRADES - NO SPECIAL REQUIREMENTS.

- WIND BRACING:
- ALL WALL AND ROOF BRACING TO COMPLY WITH CURRENT N.C.C PART 3.4.3 AND AS1894 (RESIDENTIAL TIMBER FRAMED CONSTRUCTION SIMPLIFIED - NON CYCLONIC AREAS)

NOTE TO TRUSS MANUFACTURER:

- ALL ROOF TRUSS DESIGN & DETAILS TO BE SUPPLIED TO LOCAL AUTHORITY.
- PROVIDE LOCATION OF ALL GIRDER TRUSSES TO BUILDER TO ENSURE REINFORCEMENT OF LINETS OVER OPENINGS WHERE SUCH TRUSSES MAY FALL AND CREATE POINT LOAD.

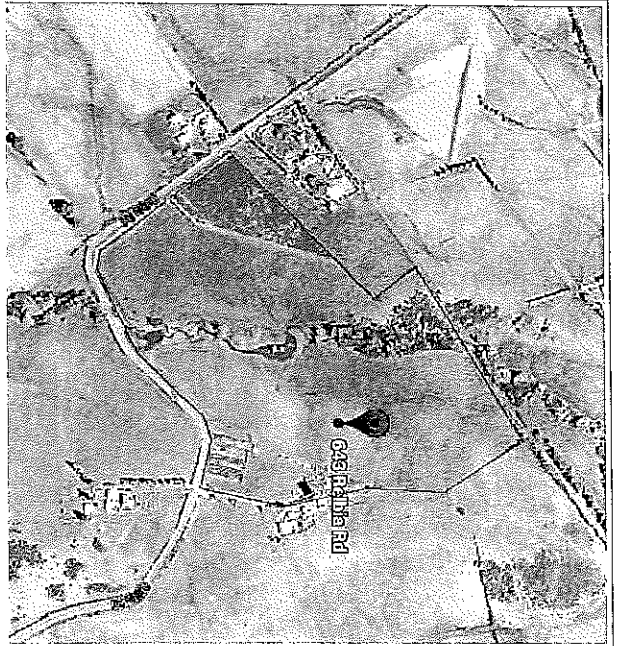
**DO NOT SCALE DRAWINGS. IF IN DOUBT - ASK**

EXHIBITED

LOCAL COUNCIL : LAUNGESTON CITY COUNCIL	DRAWING NO. : LA-14-22
ACCREDITATION COMPLIANCE NUMBER = CC-282 Z	DATE : 22-04-14
TITLE : PROPOSED NEW RESIDENCE	REVISION DATE : 14-11-14
AT 643 RELBIA ROAD RELBIA	SCALE :
FOR MR & MRS SHEPHERDSON	JOB NO. : 1A-14-22
FIELD REFERENCE : 13034/4	DRAWING NO. : P2
	02

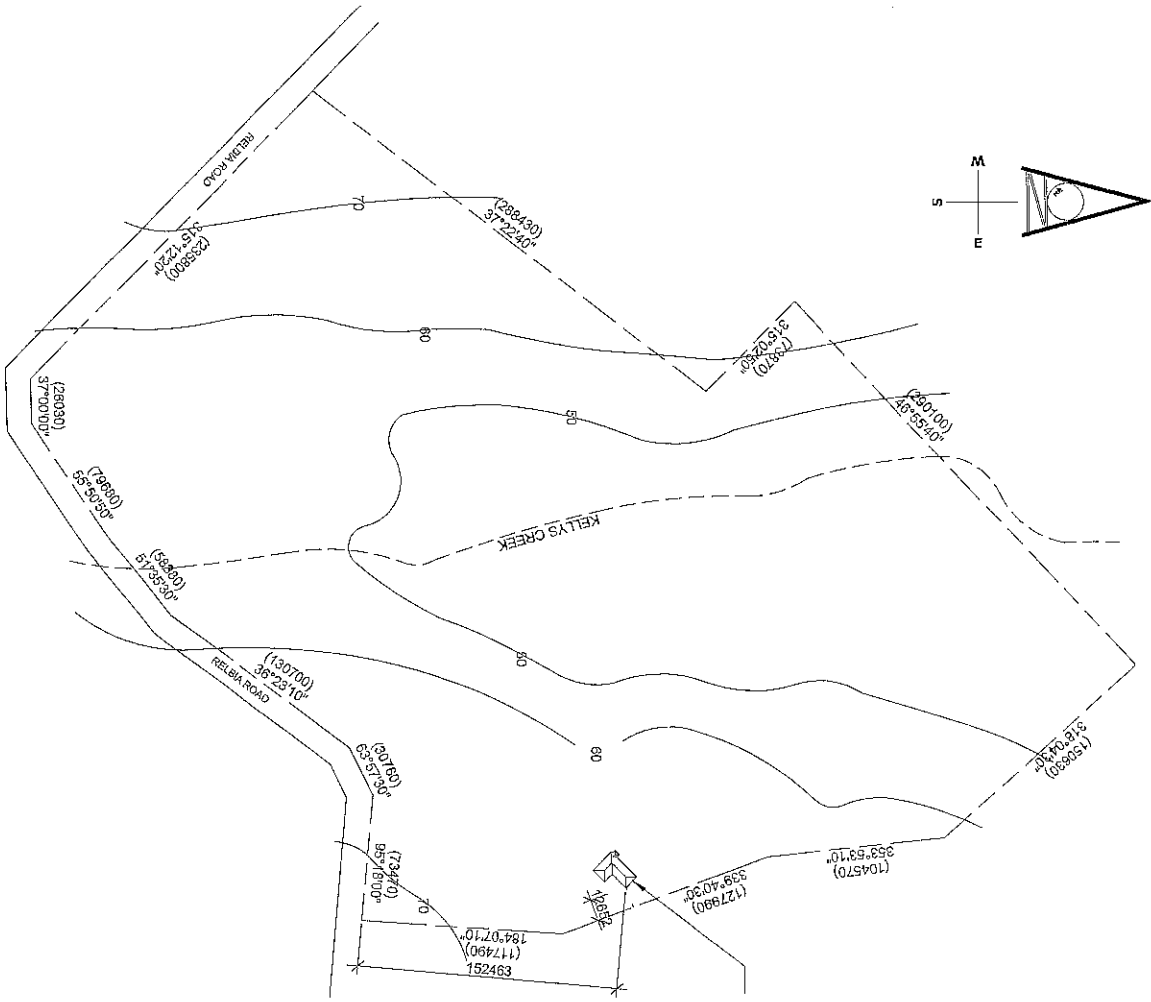
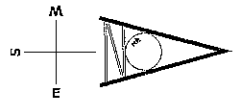
**Lateral**  
ARCHITECTURE

17/2 Inverloch Road  
Inverloch, Victoria 3798  
Ph: 5242 7777  
Fax: 5242 7778  
Email: info@shepherdson.com.au  
www.shepherdson.com.au



**LOCATION PLAN**  
NOT TO SCALE

**NOTE:**  
FINAL POSITION OF NEW BUILDING TO BE CONFIRMED BY REGISTERED SURVEYOR, BEFORE COMMENCEMENT OF WORKS.  
BOUNDARY LINE TO BE CONFIRMED ON SITE BY SURVEYOR, OR AN AUTHORISED PERSON.



**SITE PLAN**  
SCALE 1:3000

PROPOSED RESIDENCE

EXTENDED

**Lateral**  
ARCHITECTURE



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Hilbury Trust  
7/1 Lateral Architecture  
104 Invermay Road, 7048  
Invermay, Victoria 3261  
Aust. T: 51 420 740 373  
Telephone: (03) 5325 6500  
lateral@lateralure.com.au

LOCAL COUNCIL : LAUNCESTON CITY COUNCIL	DRAWN: R.Hall
ACREDITATION COMPLIANCE NUMBER = CC-282 Z	CHECKER: M.Hall
TITLE : PROPOSED NEW RESIDENCE AT 643 RELBIA ROAD RELBIA FOR MR & MRS SHEPHERDSON	DATE: 22-04-14
FOI/D REFERENCE : 130344	REVISOR DATE: 14-11-14
	SCALE: 1 : 3000
	JOB No: LA-14-22
	DRAWING NO: P3 03

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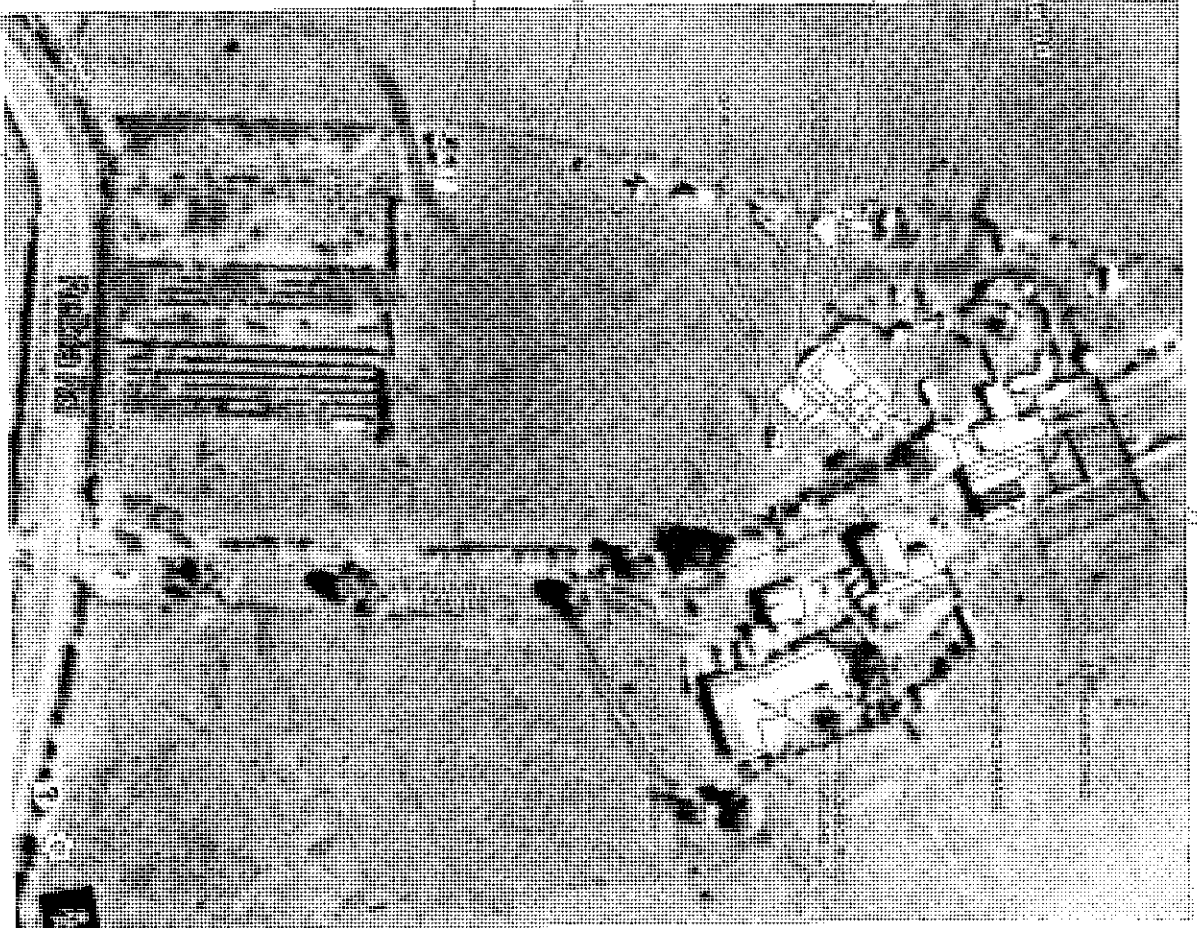
PROPOSED  
RESIDENCE

EXISTING ABSORPTION TRENCH

EXISTING SEPTIC TANK

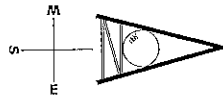
NEW DRIVEWAY TO BE

10,000 L WATER TANK  
FIREFIGHTING PURPOSES



EVERYTHING SHOWN TO BE AN APPROXIMATION. THE CLIENT ACCEPTS RESPONSIBILITY FOR THE ACCURACY OF THE INFORMATION.

**SITE PLAN BLOW - UP**  
SCALE 1:1000



EXHIBIT

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LOCAL COUNCIL : LAINCESTON CITY COUNCIL
ACCOMMODATION COMPLIANCE NUMBER - CC-282 Z
TITLE : PROPOSED NEW RESIDENCE
AT 643 RELBIA ROAD RELBIA
FOR MR & MRS SHEPHERDSON
FOLD REFERENCE : 13024/4




DRAWING NO. <b>P4</b> <b>04</b>	JOB NO. LA-14-22
DATE: 22-04-14	SCALE: 1:1000
REGISTERED DATE: 11-11-14	
CHECKER: R. HALL	
DRAWN: M.L.Hall	
DESIGNER: R.Hall	

**Lateral**  
ARCHITECTURE

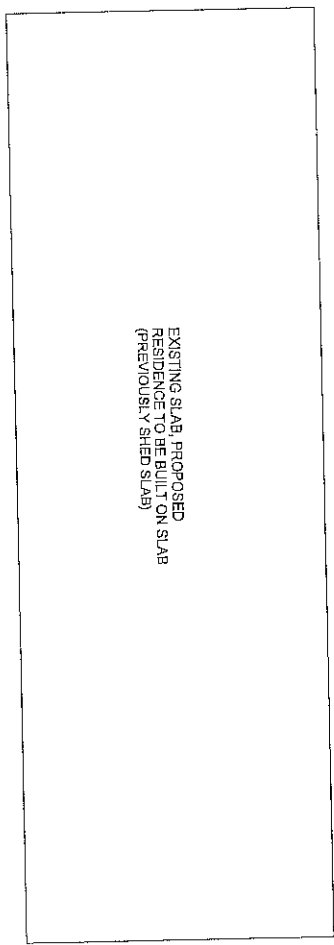
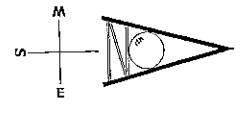
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Hollway Trust  
V/A Lateral Architecture  
104 Invermay Road, 7546  
Aberdeen, WA 6105  
Telephone: (08) 9326 1800  
res@lateralarchitecture.com.au

**Lateral**  
ARCHITECTURE

**LEGEND:**

-  = DENOTES BRICK VENEER WALLS TO BE DEMOLISHED
-  = DENOTES EXISTING STUD WALLS TO BE DEMOLISHED
-  = DENOTES EXISTING WALLS TO REMAIN

1-387



**NOTE:**

REMOVE EXISTING WINDOWS AND STORE AT OWNERS REQUEST.

**EXISTING FLOOR PLAN**

SCALE 1:100

Area	m <sup>2</sup>	sq
Area	30.22	3.25

AREA: m<sup>2</sup> = sq

(EXCLUDING)

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**IMPORTANT: IF DRAWING NOT LODGED OR PROCESSED WITH COUNCIL WITHIN 12 MONTHS OF LAST REVISION DATE, THEN PLEASE RETURN FOR REVIEW BEFORE PROCEEDING.**

LOCAL COUNCIL : LAUNCESTON CITY COUNCIL
ACCREDITATION COMPLIANCE NUMBER - CC-282 Z
TITLE: PROPOSED NEW RESIDENCE
AT 643 RELBIA ROAD RELBIA
FOR MR & MRS SHEPHERDSON
FOI/O REFERENCE: 1303944

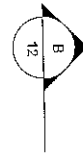
DESIGNER: R.Hall	DRAWN: M.L.Hall	CHECKER: R.HALL	DATE: 22-04-14	REVISED DATE: 14-11-14	SCALE: 1:100	DWG NO: LA-14-22	DRAWING NO. PS	05
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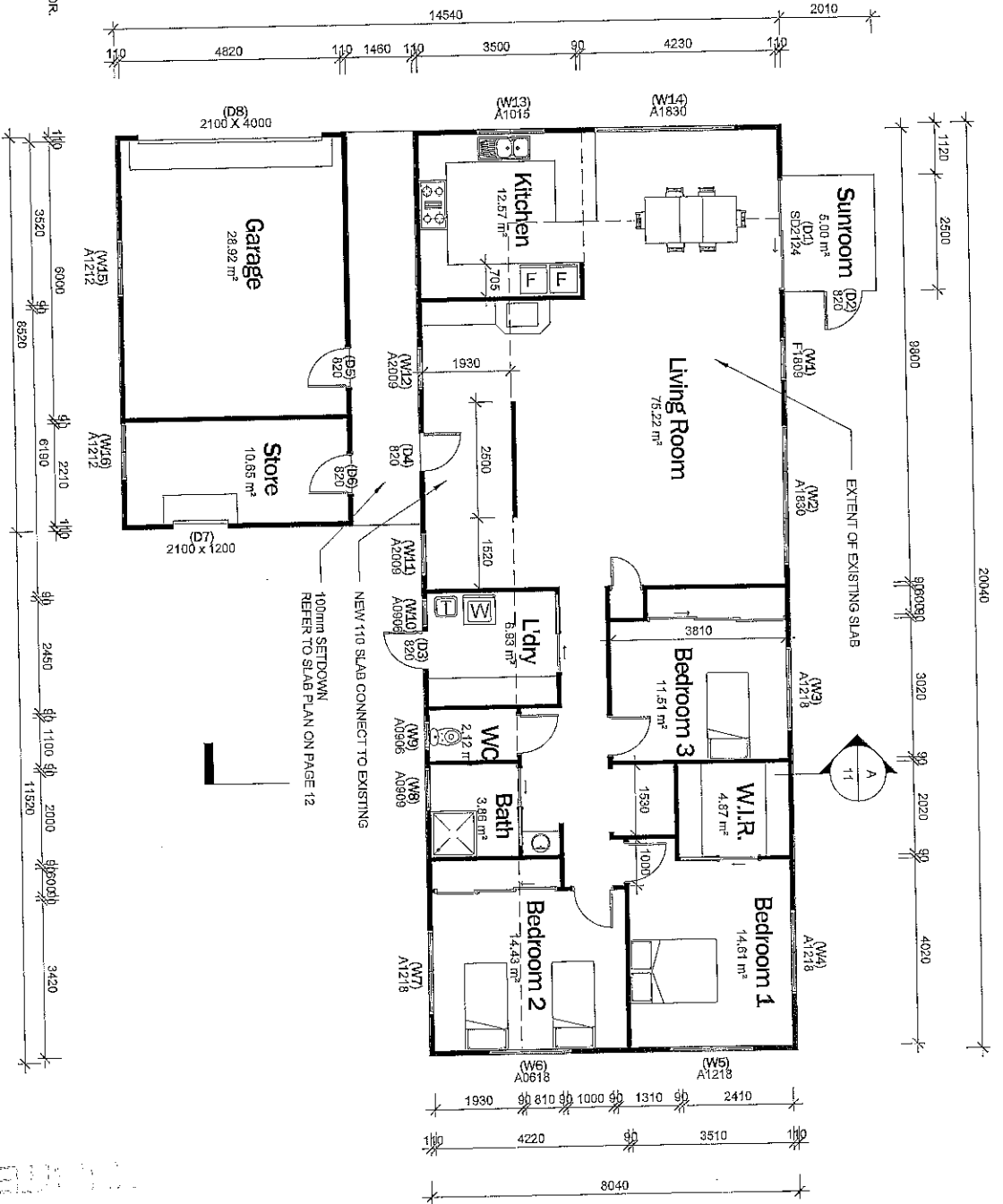
**Lateral**  
ARCHITECTURE

**LEGEND:**

- = DENOTES NEW STUD WALLS TO BE CONSTRUCTED.
- (E) = DENOTES TASTICS ELECTRICAL FANS.
- (S) = DENOTES WIRED IN SMOKE DETECTORS POSITIONS.



1-388



**NOTE:**

**ENERGY ASSESSMENT**  
6.0 STARS.  
NO DOWNLIGHTS ARE IN THE HOUSE.  
ALL WINDOW GLAZING - ALUMINIUM FRAME.  
DOUBLE GLAZED WITH LOW E (LOW-FORT PLUS) GLASS.  
EXTERNAL WALL INSULATION: R2.5 BATTIS OR SIMILAR.  
SUSPENDED FLOOR INSULATIONS: R2.0 POLYESTER BATTIS UNDER FLOOR.  
CEILING INSULATION: R4.0 BATTIS.  
ROOF: COLORBOND WITH RFL & BRADFORD ANTICORN 55 BLANKET.

**MINIMUM REQUIREMENTS:**  
STAIR TO COMPLY WITH LATEST N.C.C. CLAUSE 9.9.1 AND COUNCIL STANDARDS. 190mm MAX. RISE, 250mm RUN, 275mm TREAD, 1000mm BALUSTRADE. ENSURE 2040mm MIN. HEAD CLEARANCE FROM TOE OF TREAD TO OPENING ABOVE ARE TO COMPLY WITH N.C.C. PART 9.9.2.

**WINDOW SCHEDULE NOTE:**  
S1815 = S; SLIDING, 1815x1800mm HIGH, 1615x1510 WIDE.  
A1509 = A; AWNING, 1815x1800mm HIGH, 091910 WIDE.  
D = DOUBLE HUNG, F = FIXED, C = CASEMENT.

**IMPORTANT:** IF DRAWING NOT LOGGED OR PROCESSED WITH COUNCIL WITHIN 12 MONTHS OF LAST REVISION DATE, THEN PLEASE RETURN FOR REVIEW BEFORE PROCEEDING.

GROUND FLOOR	TOTAL AREA	m <sup>2</sup>	sq ft
(EXCLUDING DECK/CARPORT etc.)	291.45	3137	

**DO NOT SCALE DRAWINGS. IF IN DOUBT - ASK**

LOCAL COUNCIL: LANDSEIGN CITY COUNCIL	
ACCREDITATION COMPLIANCE NUMBER - CC-2822	
TITLE: PROPOSED NEW RESIDENCE AT 643 RELBIA ROAD RELBIA FOR MR & MRS SHEPHERDSON	
FOLIO REFERENCE: 1303444	
JOB No:	LA-14-22
SCALE:	1:100
DATE:	22-04-14
REVISION DATE:	14-11-14
CHECKER:	R. HALL
DRAWN:	M. L. HALL
DESIGNER:	R. HALL

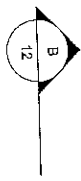
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7/4 lateral Architecture  
1044 Invermay Road, 728  
Adelaide, SA 5061  
Phone: 75 520 740 779  
Telephone: (08) 6326 6810  
lateral@lateralarchitecture.com



**LEGEND:**

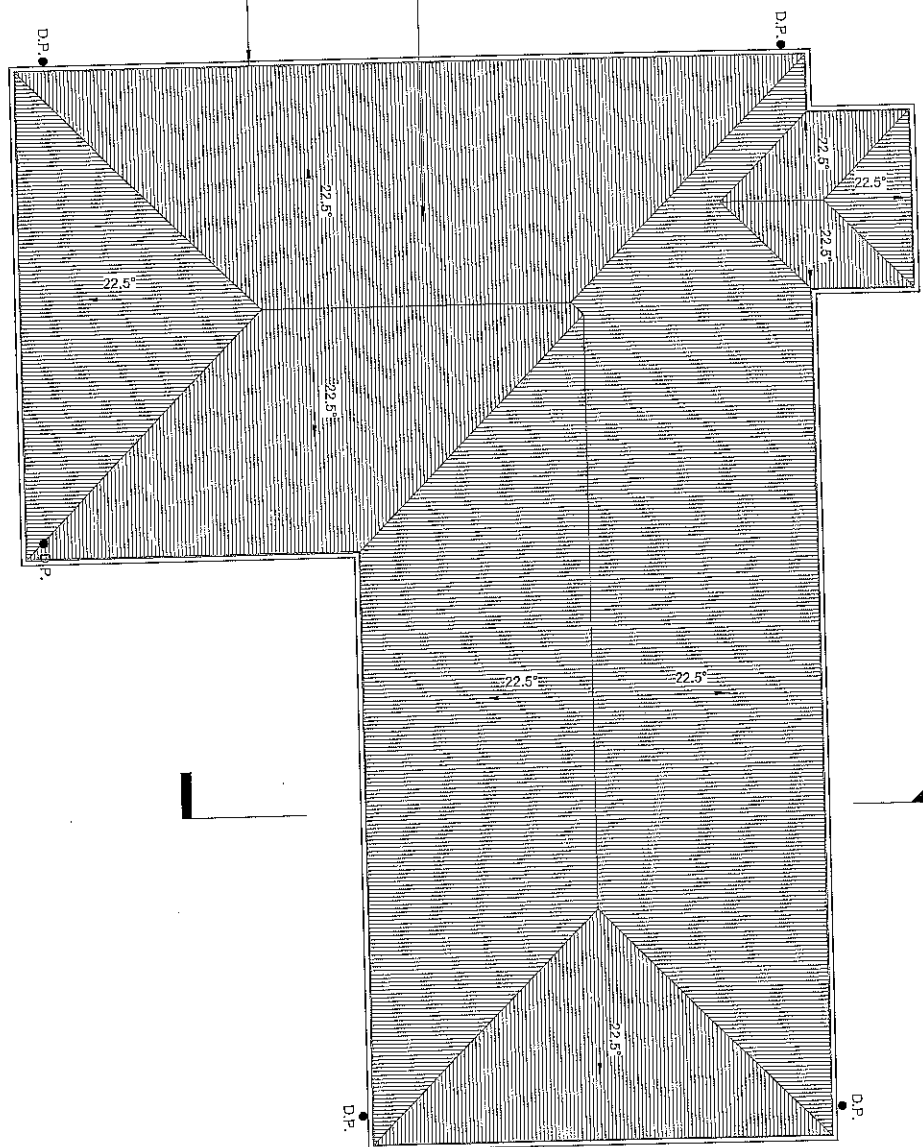
- D.P. ● = DENOTES DOWNPIPES
- S.P. ● = DENOTES SPREADERS



1-389

COLORBOND CUSTOM-GRE ROOF CLADDING,  
COLOUR TO BE SELECTED BY CLIENT.

COLORBOND FASCIA, GUTTERS, BARGES,  
SPREADERS, AND DOWNPIPES TO BE USED,  
COLOUR TO BE SELECTED BY CLIENT.



**NOTE:**

COLORBOND CLADDING FITTED TO ROOF  
AS PER ASI 562 INSTALLATION OF ROOF  
AND WALL CLADDING AND N.C.C. 3.6.1.

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WIND COMBINATION LOADS AND OVERSIGHTS OUTSIDE THE SCOPE OF THIS ENGINEERING SERVICE AND A COMPETENT  
STRUCTURAL ENGINEER SHOULD BE ADVISED THAT THIS PLAN HAS BEEN DRAWN AS A PRELIMINARY DESIGN AND IS NOT  
DISCLAIMER: LATERAL ARCHITECTURE ADVISES THAT THIS PLAN HAS BEEN DRAWN AS A PRELIMINARY DESIGN AND IS NOT  
LOCAL AUTHORITY: LATERAL ARCHITECTURE ADVISES THAT THIS PLAN HAS BEEN DRAWN AS A PRELIMINARY DESIGN AND IS NOT  
OVERSIGHTS IN THE PLAN FOR THE PURPOSE OF SUPPLEMENTARY INFORMATION, NOR FOR MISCONSTRUCTION OR INTERPRETATION.  
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**PROPOSED ROOF PLAN**  
SCALE 1:100

**DO NOT SCALE DRAWINGS. IF IN DOUBT - ASK**

LOCAL COUNCIL : LAUNGESTON CITY COUNCIL ACCREDITATION COMPLIANCE NUMBER - CC-282 Z	DRAWING NO. 07
TITLE PROPOSED NEW RESIDENCE AT 643 RELBIA ROAD RELBIA FOR MR & MRS SHEPHERDSON	SCALE 1:100
FOLIO REFERENCE: 13039/4	DATE 22-04-14
	DESIGNED BY R. HALL
	CHECKED BY R. HALL
	REVISION DATE 14-11-14
	SCALE 1:100
	DWG NO. 1A-14-22

EXHIBIT

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ARCHITECTURE

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1/4 Lateral Road  
MURFRESBURG, Tennessee, 37248  
PHONE: 75 430 740 378  
www.lateralarchitecture.com.au

DESIGNER:  
R. Hall

DRAWN:  
M. Hall

CHECKER:  
R. Hall

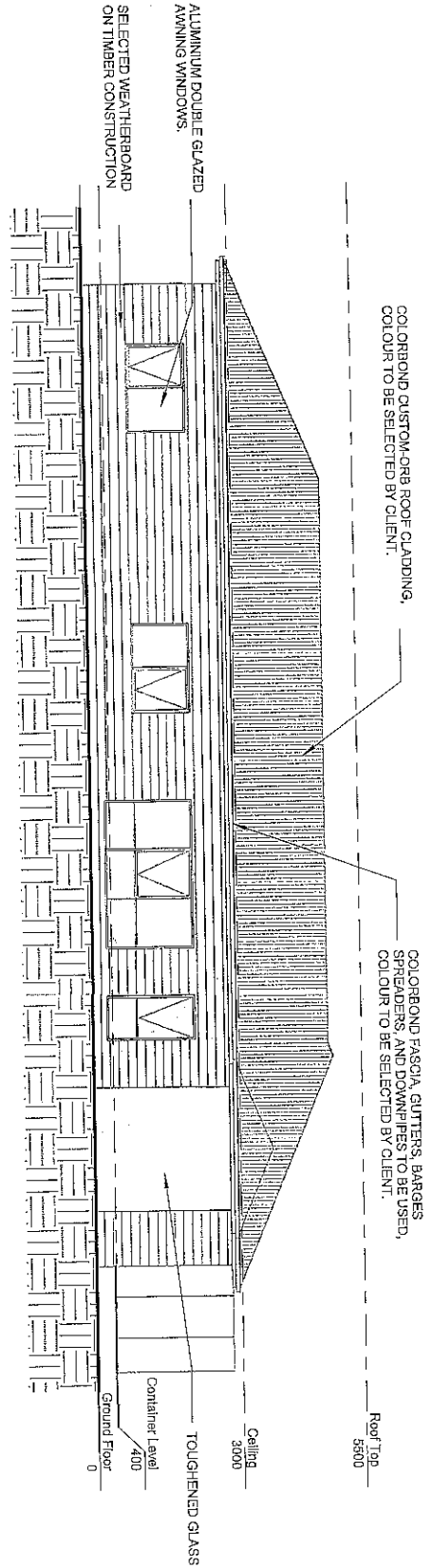
DATE:  
22-04-14

REVISION DATE:  
14-11-14

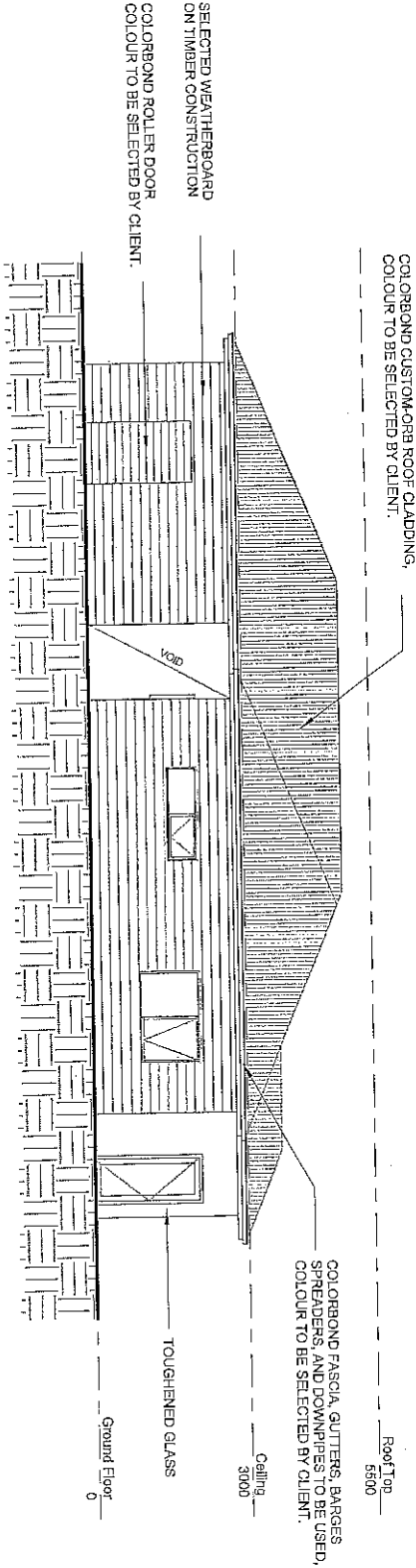
SCALE:  
1:100

DWG NO.:  
1A-14-22

**Lateral**  
ARCHITECTURE



**NORTH ELEVATION**  
SCALE 1:100



**EAST ELEVATION**  
SCALE 1:100

**NOTE:**

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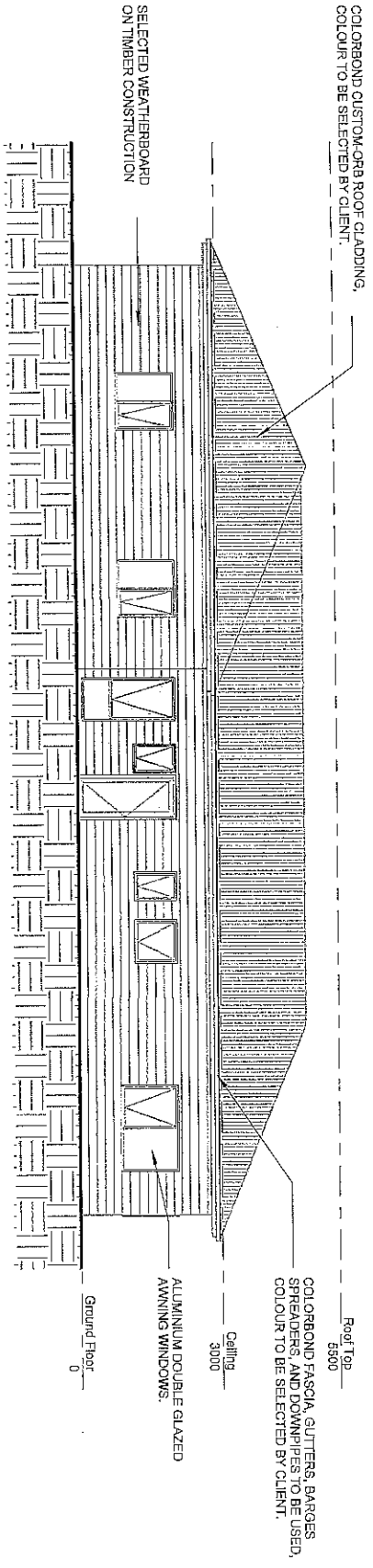
LOCAL COUNCIL : LAINCASTON CITY COUNCIL
ACCREDITATION COMPLIANCE NUMBER - CC-282Z
TITLE : PROPOSED NEW RESIDENCE
AT 643 RELBIA ROAD RELBIA
FOR MR & MRS SHEPHERDSON
FIELD REFERENCE: 130244

DESIGNER: R.Hall	DRAWING NO: LA-14-22
DRAWN: M.L.Hall	JOB NO: 08
CHECKER: R.HALL	DATE: 22-04-14
REVISION DATE: 11-11-14	SCALE: 1:100

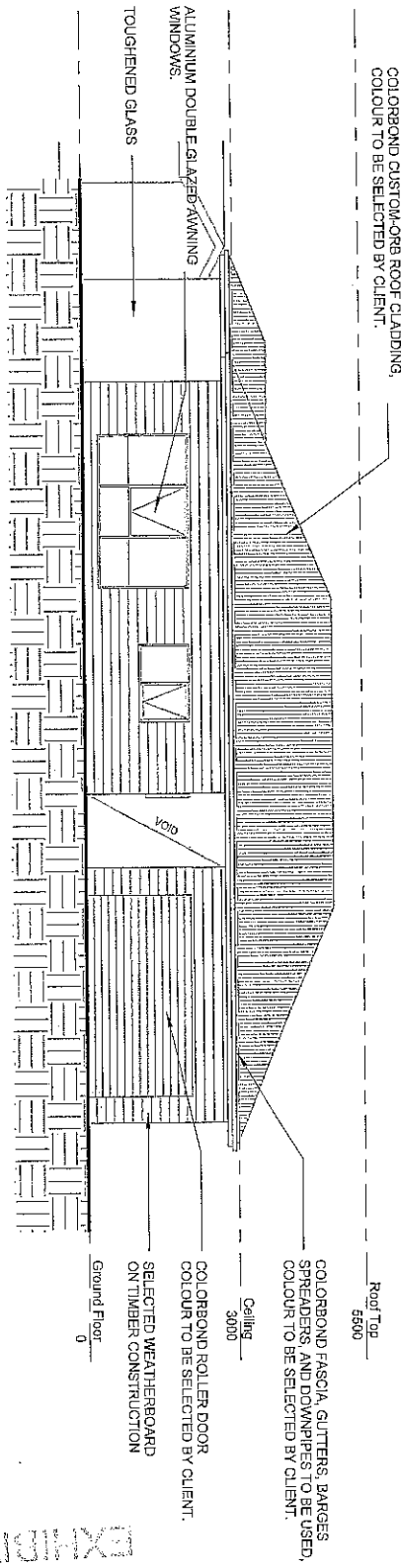
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70/71 The Esplanade  
INVERMAY, Tasmania, 7298  
ABN: 75 420 740 270  
Tel: 08135114200  
lateral@lateralarch.com.au

**Lateral ARCHITECTURE**



**SOUTH ELEVATION**  
SCALE 1:100



**WEST ELEVATION**  
SCALE 1:100

**NOTE:**  
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LOCAL COUNCIL : LANINGSTON CITY COUNCIL
ACREDITATION COMPLIANCE NUMBER - CC-282 Z
<b>TITLE : PROPOSED NEW RESIDENCE</b>
AT 643 HEBLIA ROAD HEBLIA
FOR MR & MRS SHEPHERSON
FOLD REFERENCE: 13084/4

JOB NO: LA-14-22
DRAWING NO: <b>pg</b>
<b>09</b>

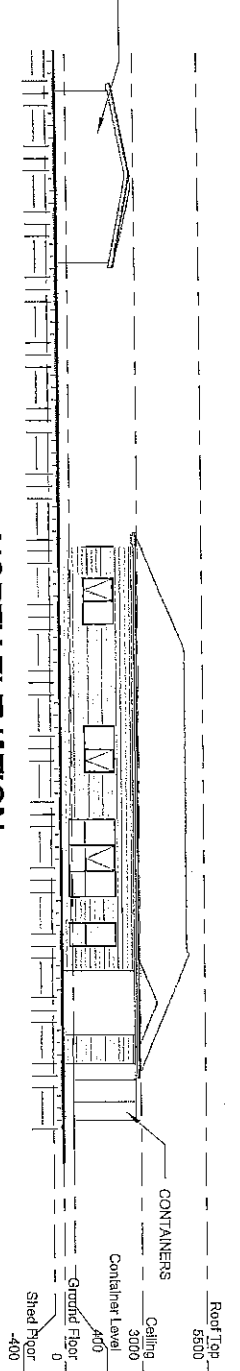
**EXHIBIT**

DATE: 22-04-14
REQUIRED DATE: 11-11-14
SCALE: 1:100
DRAWN: M.L Hall
CHECKER: R. HALL
DESIGNER: R.Hall

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 104a Laverney Road,  
 KINGSLEY, Tasmania 7248  
 Telephone: (03) 6295 6010  
 lateral@lateral.com.au

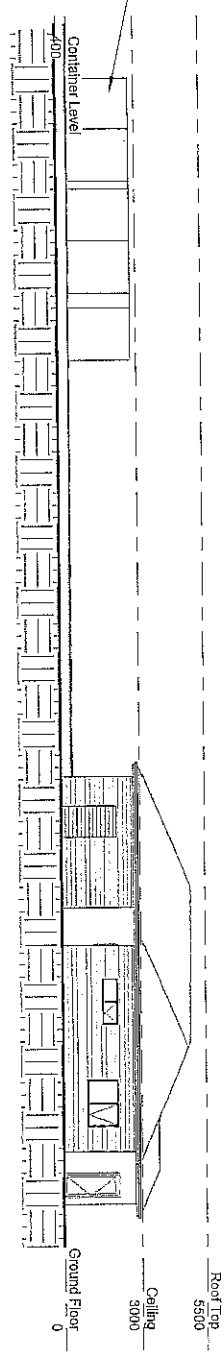
**Lateral ARCHITECTURE**

EXISTING SHED TO BE RELOCATED TO THIS LOCATION



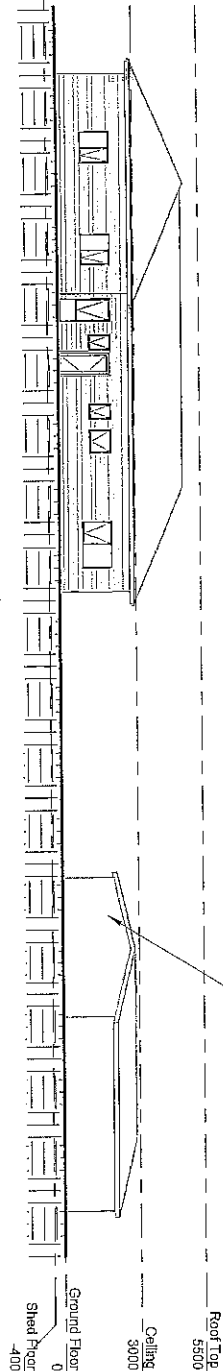
**NORTH ELEVATION**  
SCALE 1:200

CONTAINERS



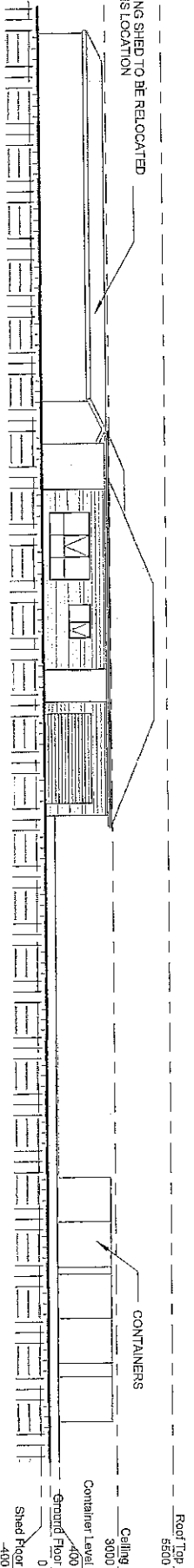
**EAST ELEVATION**  
SCALE 1:200

EXISTING SHED TO BE RELOCATED TO THIS LOCATION



**SOUTH ELEVATION**  
SCALE 1:200

EXISTING SHED TO BE RELOCATED TO THIS LOCATION



**WEST ELEVATION**  
SCALE 1:200

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EXHIBIT

LEGAL COUNCIL : LAURIBERTON CITY COUNCIL
ACCRETION COMPLIANCE NUMBER = OC-292Z
TITLE : PROPOSED NEW RESIDENCE AT 643 RELBIA ROAD RELBIA FOR MR & MRS SHEPHERDSON
FOLD REFERENCE : 13034/4

DESIGNER: R.Hall	DRAWING NO: LA-14-22
DRAWN: A.Morris	JOB No: P10
CHECKER: M.L.Hall	SCALE: 1:200
DATE: 22-04-14	REVISION DATE: 11-11-14

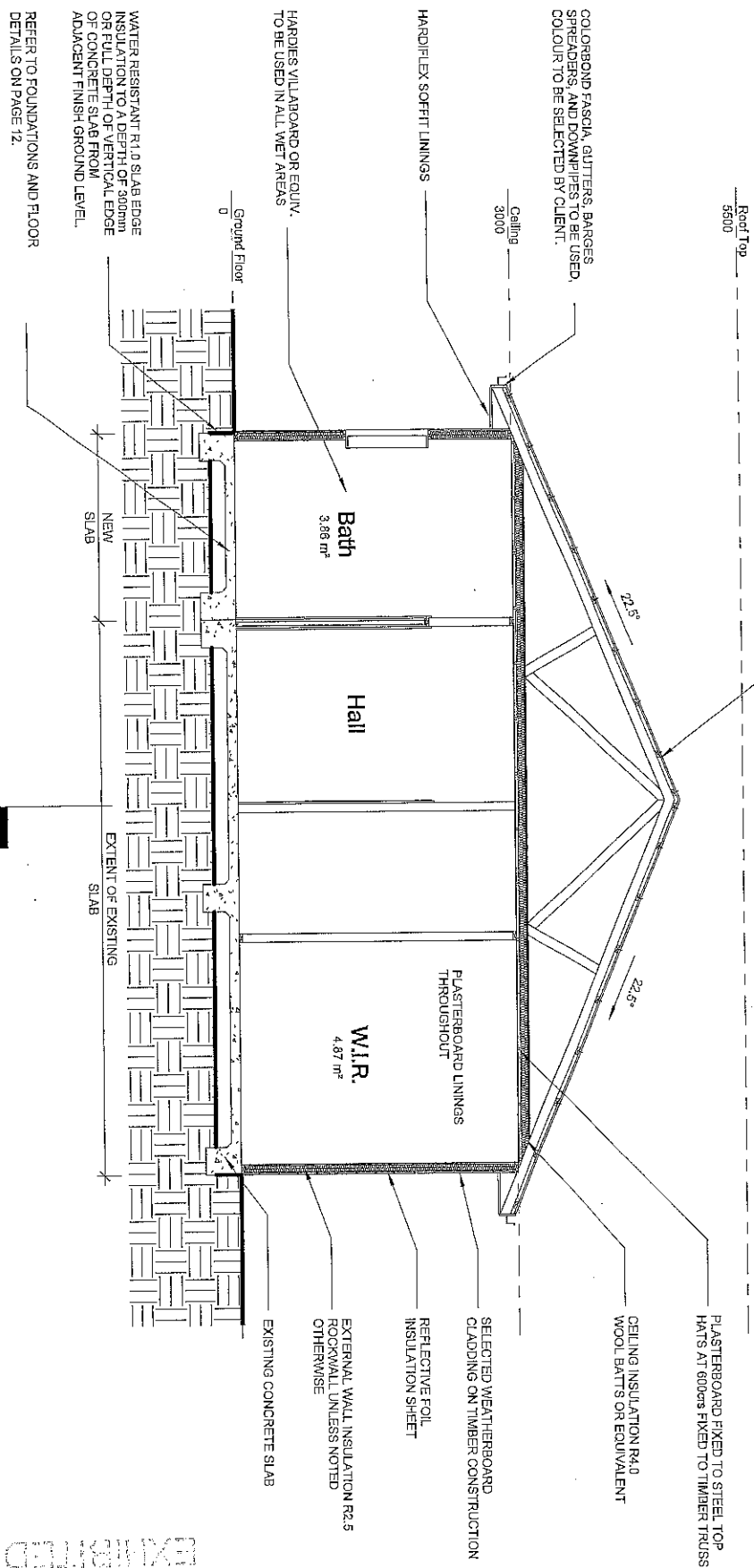
**Lateral ARCHITECTURE**

Headrow, Trunk  
 7/4 lateral architecture  
 10/4 Invermay Road,  
 INVERMAY, Tasmania, 7248  
 Telephone (03) 6255 6610  
 lateral@lateral.com.au

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- PROVIDE LOCATION OF ALL GIRDER TRUSSES TO BUILDER TO ENSURE REINFORCEMENT OF LINTELS OVER OPENINGS WHERE SUCH TRUSSES MAY FALL AND GREAT POINT LOAD.

COLORBOND ROOF CLADDING ON TIMBER BATTENS 75 X 38 HV AT 900mm ON PINE TRUSSES AT 600mm TO BE LINED WITH PLASTERBOARD



**SECTION A**  
SCALE 1:50

**NOTE:**

- COLORBOND CLADDING FITTED TO ROOF AS PER AS1962 INSTALLATION OF ROOF AND WALL CLADDING AND N.C.C. 3.5.1.
- WATER RESISTANT PLASTERBOARD OR HARDIES VILBOARD TO BE USED IN ALL WET AREAS. FLOOR TO BE TREATED WITH APPROPRIATE WATERPROOFING PRODUCTS TO ENSURE COMPLIANCE WITH N.C.C. PART 3.5.1 AND AUSTRALIAN STANDARD AS 3740.
- "PRYDA" METAL VENTS (230 x 79) TO BE USED ALL AROUND SUB-FLOOR. MAXIMUM SPACING 1048mm, 600mm FROM EACH END, TO COMPLY WITH N.C.C. CLAUSE 3.4.1.

REFER TO FOUNDATIONS AND FLOOR DETAILS ON PAGE 12

ENGINEERING DISCIPLINE: STRUCTURAL ENGINEERS CERTIFICATE NO. 1234567890. THE ENGINEER HAS REVIEWED THE DRAWING AND IS Satisfied THAT THE STRUCTURE WILL BE SAFE AND SOUND. THE ENGINEER'S REVIEW IS LIMITED TO THE STRUCTURAL ASPECTS OF THE DRAWING AND DOES NOT COVER THE ARCHITECTURAL OR FINISHES ASPECTS. THE ENGINEER'S REVIEW IS LIMITED TO THE STRUCTURAL ASPECTS OF THE DRAWING AND DOES NOT COVER THE ARCHITECTURAL OR FINISHES ASPECTS. THE ENGINEER'S REVIEW IS LIMITED TO THE STRUCTURAL ASPECTS OF THE DRAWING AND DOES NOT COVER THE ARCHITECTURAL OR FINISHES ASPECTS.

**DO NOT SCALE DRAWINGS. IF IN DOUBT - ASK**

LOCAL COUNCIL : LANCASTON CITY COUNCIL
TITLE : PROPOSED NEW RESIDENCE
AT 643 RELBIA ROAD RELBIA
FOR MR & MRS SHEPHERDSON
FOLIO REFERENCE : 13034/4

JOB NO	LA-14-22
DRAWING NO	P11
SCALE	1:50
DATE	22-04-14
CHECKED	M.L.Hall
DRAWN	A.Morris
DATE	11-11-14

DESIGNER	R.Hall
DATE	22-04-14
REVISION DATE	11-11-14
SCALE	1:50
JOB NO	LA-14-22
DRAWING NO	P11
SCALE	1:50
DATE	22-04-14
CHECKED	M.L.Hall
DRAWN	A.Morris
DATE	11-11-14

EXHIBIT

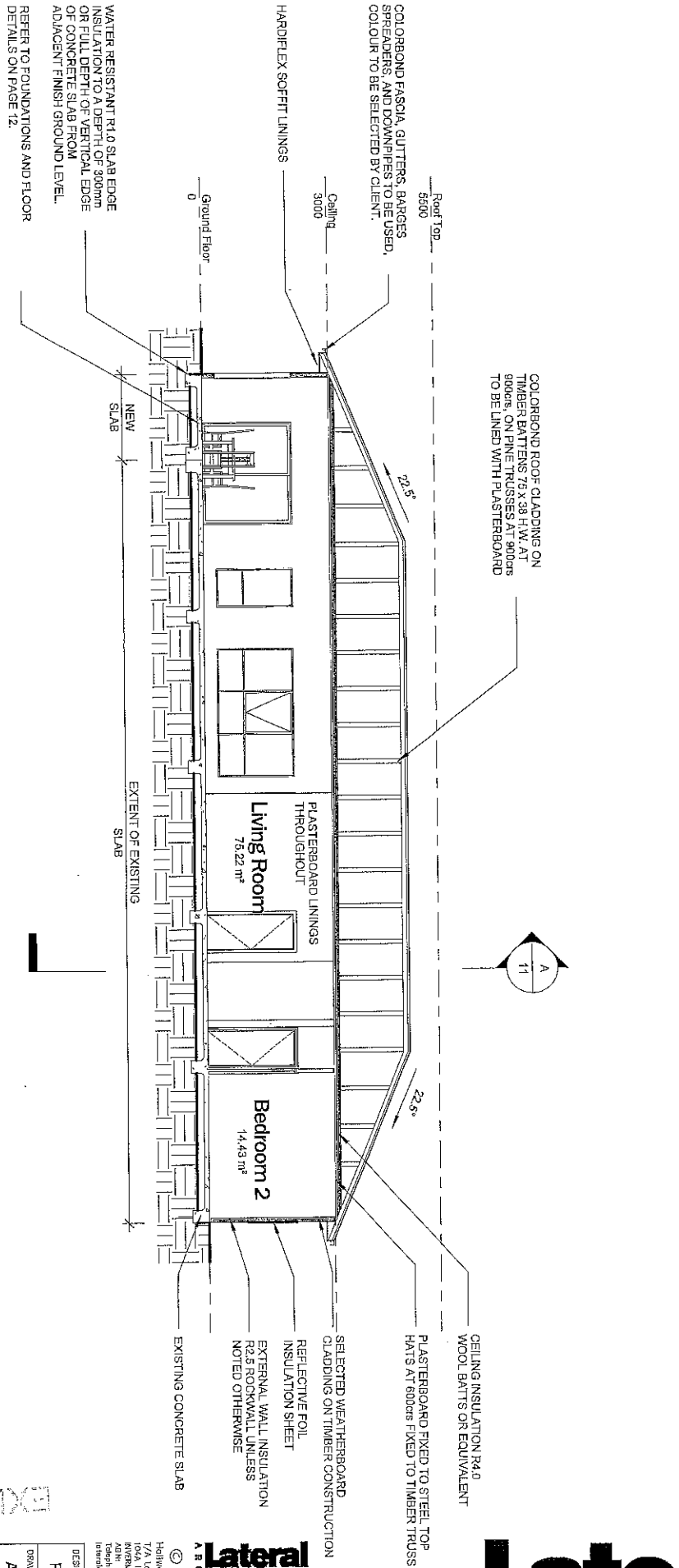
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104 Waverley Road,  
Waverley, Victoria 3174  
Telephone: (03) 8345 6810  
lateral@lateral.com.au

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**NOTE:**

- COLORBOND CLADDING FITTED TO ROOF AS PER AS1682  
 INSTALLATION OF ROOF AND WALL CLADDING AND N.C.C. 3.5.1.  
 - WATER RESISTANT PLASTERBOARD OR HARDIES VILLABOARD TO BE USED IN ALL WET AREAS. FLOOR TO BE TREATED WITH APPROPRIATE WATERPROOFING PRODUCTS TO ENSURE COMPLIANCE WITH N.C.C. PART 3.8.1 AND AUSTRALIAN STANDARD AS 3740.  
 - PRYDA® METAL VENTS (230 x 75) TO BE USED ALL AROUND SUB-FLOOR. MAXIMUM SPACING 1048mm, 900mm FROM EACH END. TO COMPLY WITH N.C.C. CLAUSE 3.4.1.

**SECTION B**  
 SCALE 1:100

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LOCAL COUNCIL : LANINGSTON CITY COUNCIL	DRAWING NO.
ACREDITATION COMPLIANCE NUMBER - CC-2822	LA-14-22
TITLE: PROPOSED NEW RESIDENCE	DATE
AT 643 RELBIA ROAD RELBIA	22-04-14
FOR MR & MRS SHEPHERDSON	REVISION DATE
FOLIO REFERENCE: 13034/4	11-11-14
	SCALE
	1:100
	JOB NO.
	12

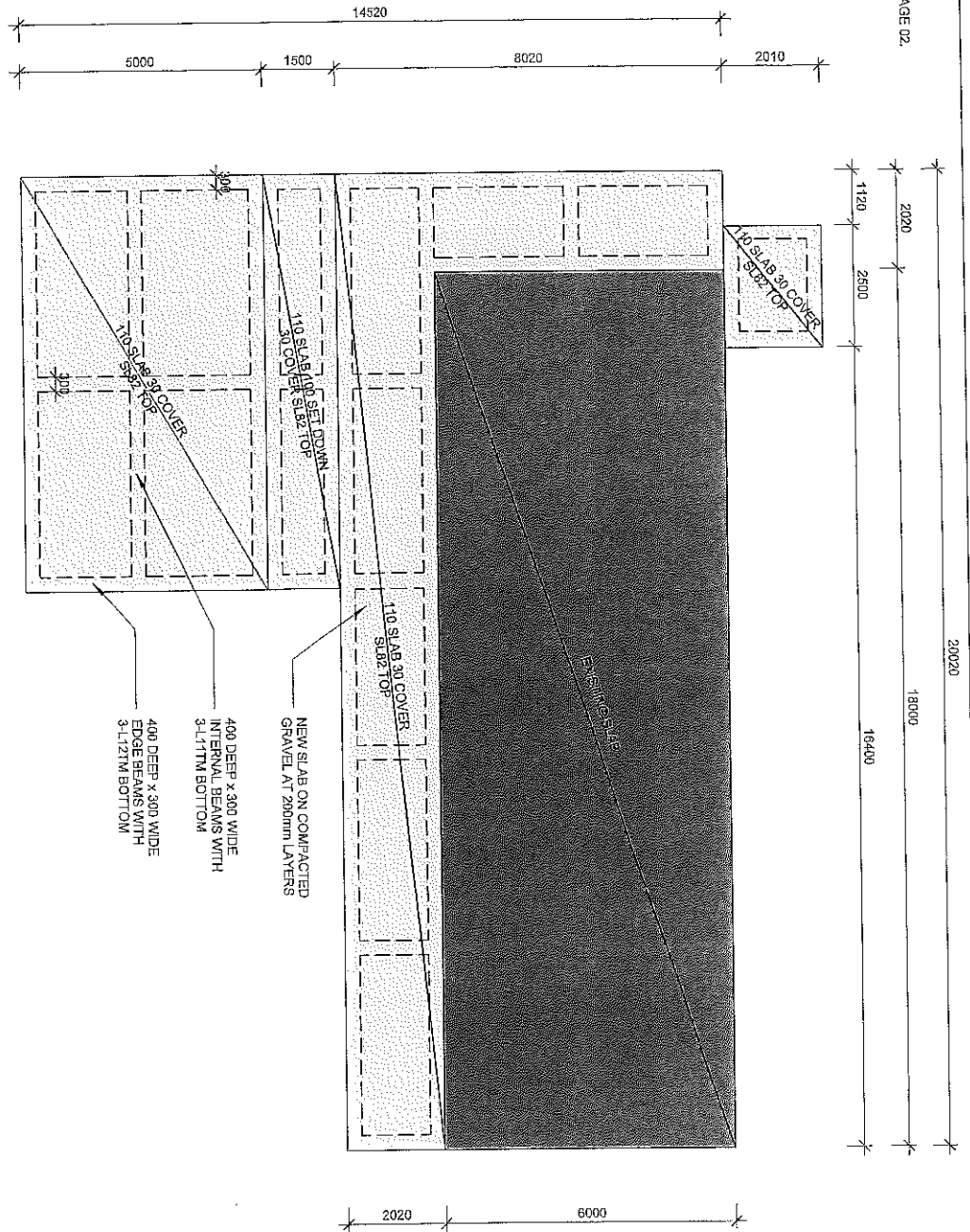
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 7/A Lateral Architecture  
 10/4 Invermay Road,  
 KIRKBY, Gordon, 2248  
 Telephone: (02) 5282 8010  
 lateral@lateral.com.au

DESIGNER: R.Hall  
 DRAWN: A.Morris  
 CHECKERS: M.L.Hall  
 DATE: 22-04-14  
 REVISED DATE: 11-11-14  
 SCALE: 1:100  
 JOB NO: LA-14-22

NOTE:  
REFER TO CONSTRUCTION NOTES ON PAGE 02.



1-395

**SLAB AND FOOTING NOTE**

SLAB AND FOOTINGS DESIGNED IN ACCORDANCE WITH AS 2870, TO "H" SOIL CLASSIFICATION AND WIND CLASSIFICATION TO AS 4056 WIND RATING: N2. V<sub>W</sub> 33m/s SHOULD SOIL TEST OR WIND CLASSIFICATION BE HIGHER THAN NOTED. REFER ENGINEER FOR DESIGN.

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**SLAB SETOUT PLAN**  
SCALE 1:100

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LOCAL COUNCIL : LANGSESSON CITY COUNCIL	ACREDITATION COMPLIANCE NUMBER - CC-282 Z
TITLE : PROPOSED NEW RESIDENCE AT 643 REIBA ROAD REIBA FOR MR & MRS SHEPHERDSON	
FOLD REFERENCE: 130344	
JOB No:	LA-4-22
SCALE:	1:100
REVISED DATE:	11-11-14
DATE:	22-04-14
CHECKER:	ML Hall
DESIGNER:	R Hall
DRAWN:	AMorris

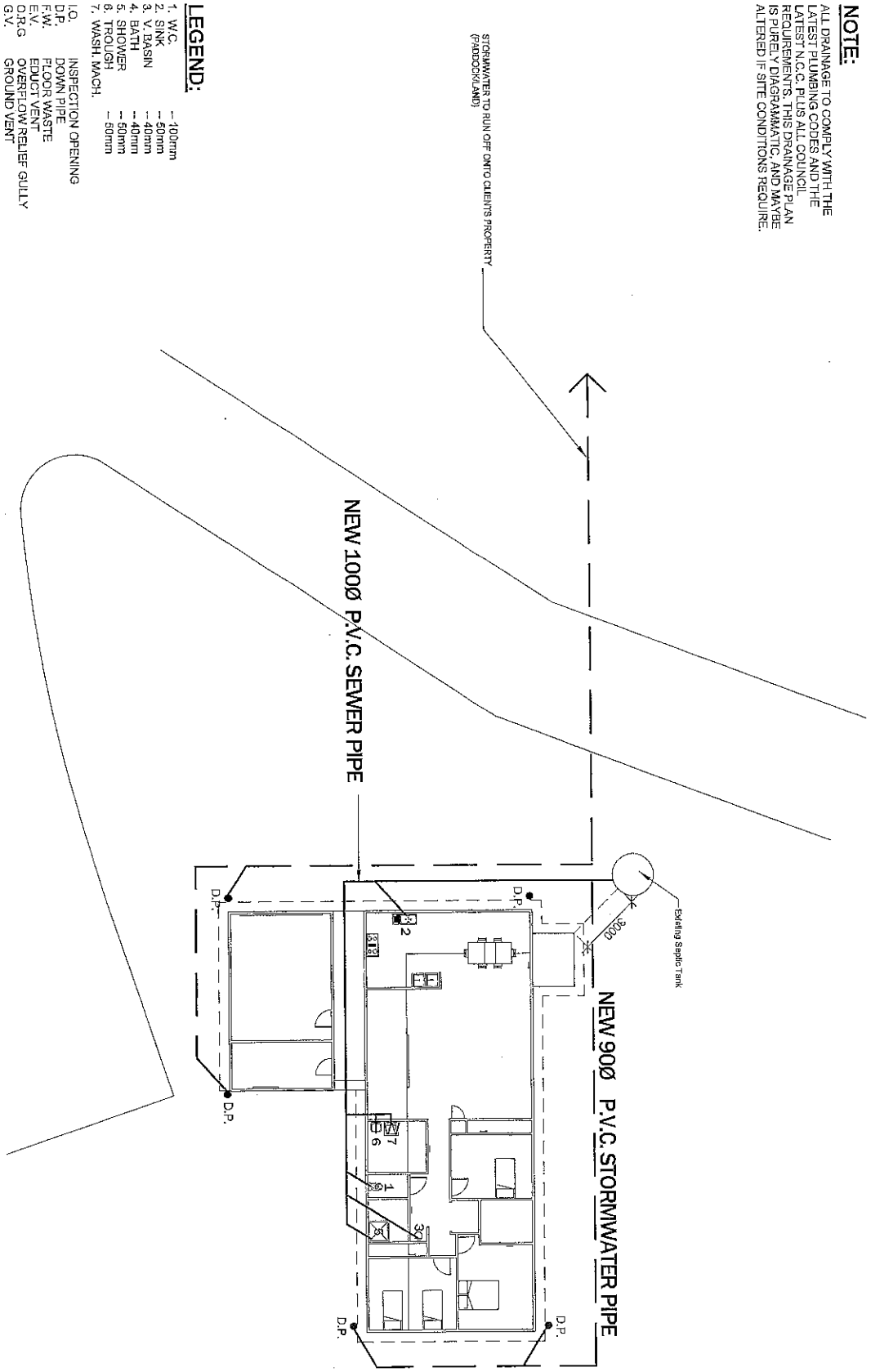
EX-101



**Lateral**  
ARCHITECTURE

**NOTE:**  
 ALL DRAINAGE TO COMPLY WITH THE LATEST PLUMBING CODES AND THE LATEST N.C.C. PLUS ALL COUNCIL REQUIREMENTS. THIS DRAINAGE PLAN IS PURELY DIAGRAMMATIC AND MAYBE ALTERED IF SITE CONDITIONS REQUIRE.

1-396



**DRAINAGE PLAN**  
 SCALE 1:100

**NOTE:**  
 CONFIRM ON SITE THE CORRECT LOCATION OF EXISTING SEWER AND STORMWATER PIPES, PRIOR TO SETOUT OF DRAINAGE WORK.  
 CHECK CONDITION OF ALL EXISTING SEWER AND STORMWATER PIPES, REPLACING ANY THAT ARE NOT SATISFACTORY, BEFORE CONNECTING NEW PIPES INTO EXISTING. ALL DRAINAGE TO COMPLY TO COUNCIL REQUIREMENTS.  
 PLACE A SLOTTED SUB-SOIL DRAIN BEHIND GROUND FLOOR WALL, WHERE NECESSARY, CONNECT TO EXISTING STORM WATER PIPE.

**LEGEND:**

- 1. W.C. --- 100mm
- 2. SINK --- 50mm
- 3. V. BASIN --- 40mm
- 4. BATH --- 40mm
- 5. SHOWER --- 50mm
- 6. TROUGH --- 50mm
- 7. WASH, WASH:
- I.O. INSPECTION OPENING
- D.P. DOWN PIPE
- F.W. FLOOR WASTE
- E.V. EJECT VENT
- O.R.G. OVERFLOW/RELIEF GULLY
- G.V. GROUND VENT

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LOCAL COUNCIL : LAINCASTON CITY COUNCIL	TITLE
ACCOMMODATION COMPLIANCE NUMBER - CC-282Z	PROPOSED NEW RESIDENCE
	AT 643 HELBIA ROAD HELBIA
	FOR MR & MRS SHEPHERDSON
FOLD REFERENCE: 13034/4	

DATE:	22-04-14
REVISD DATE:	11-11-14
SCALE:	1:200
JOB No:	LA-14-22
DRAWING NO:	014
	14

**Lateral**  
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 1004 Invermay Road, 3726  
 Hillside, 35 430 940 372  
 Telephone: (03) 6348 8610  
 lateral@architecture.com.au

DESIGNER:  
 R.Hall

DRAWN:  
 A.Morris

CHECKER:  
 M.L.Hall



# BUILDING FABRIC - N.C.C V2 3.12.1 CLIMATE ZONE 7

Installation MUST include an energy reflecting insulation outer than of aerogel or perlite such as columns, studs, sponges, joists, framing and the like where the insulation must be applied the member MUST form a continuous barrier with ceiling, walls, bulkheads, floors.

ROOF CONSTRUCTION	MINIMUM R-VALUE REQUIRED	EQUALISATION FACTOR	TOTAL R-VALUE OF BUILDING MATERIALS	TYPE OF INSULATION MATERIAL TO BE ADDED	R-VALUE ADDED INSULATION	TOTAL R-VALUE
WEATHERBOARD CLADDING ON TIMBER CONSTRUCTION LINED IN PLASTER WITH SARKING	4.6	N/A	0.53	X1 BROADFORD GOLD 75mm	2.1	4.88
WEATHERBOARD CLADDING ON TIMBER CONSTRUCTION LINED IN PLASTER WITH SARKING	2.8	X	0.7	X1 BROADFORD GOLD 90mm	2.1	2.8
FLOOR CONSTRUCTION	1.5	X	.5	SLAB EDGE 1.0 INSULATION	1.0	1.5
1.0 CONCRETE SLAB ON GROUND WITH SARKING AND SLAB EDGE INSULATION						

## EXTERNAL GLAZING - N.C.C V2 3.12.2

**BCA VOLUME TWO GLAZING CALCULATOR (First Issued with BCA 2013)**

Building Identification: 643 Relbia Road, Relbia  
 Storey: Four  
 Floor: Corridor  
 Area: 23.2m<sup>2</sup>  
 Glazing Area: 1.9% of Sfd  
 Area of glazing: 23.2m<sup>2</sup> (19% of area of storey)

Climate zone: 7  
 Constants: 5.66, 0.28  
 Allowances: 5.5, 80.2

Glazing element	Orientation	Size			Performance		SHADING		CALCULATION DATA		CALCULATION OUTCOMES - GRIFFIN (if not valid)	
		Height (m)	Width (m)	Area (m <sup>2</sup> )	U-Value (W/m <sup>2</sup> K)	SHGC (0.0-1.0)	SHADING P	SHADING H	SHADING PH	SHADING Ea	SHADING H <sub>max</sub>	SHADING H <sub>min</sub>
1 W1 (W1)	N	1.80	0.50	0.90	0.56	0.42	0.56	1.62	0.15	30% of 93%	0.8	83% of 50%
2 W2 (W2)	N	1.80	3.00	5.40	0.56	0.42	0.56	5.40	0.49	10% of 93%	2.3	95% of 50%
3 W3 (W3)	N	1.20	1.80	2.16	0.56	0.42	0.56	2.16	0.20	65% of 93%	1.1	65% of 50%
4 W4 (W4)	N	1.20	1.80	2.16	0.56	0.42	0.56	2.16	0.20	65% of 93%	1.1	65% of 50%
5 W5 (W5)	E	1.20	1.80	2.16	0.56	0.42	0.56	2.16	0.20	65% of 93%	1.1	65% of 50%
6 W6 (W6)	E	1.20	1.80	2.16	0.56	0.42	0.56	2.16	0.20	65% of 93%	1.1	65% of 50%
7 W7 (W7)	S	1.20	1.80	2.16	0.56	0.42	0.56	2.16	0.20	65% of 93%	1.1	65% of 50%
8 W8 (W8)	S	0.50	0.90	0.45	0.56	0.42	0.56	0.45	0.15	30% of 93%	0.8	83% of 50%
9 W9 (W9)	S	0.50	0.90	0.45	0.56	0.42	0.56	0.45	0.15	30% of 93%	0.8	83% of 50%
10 W10 (W10)	S	2.00	0.90	1.80	0.56	0.42	0.56	1.80	0.18	30% of 93%	0.6	92% of 50%
11 W11 (W11)	W	1.80	1.50	2.70	0.56	0.42	0.56	1.50	0.44	13% of 93%	3.5	92% of 50%
12 W12 (W12)	W	1.80	1.50	2.70	0.56	0.42	0.56	1.50	0.44	13% of 93%	3.5	92% of 50%
13 W13 (W13)	W	1.20	1.20	1.44	0.56	0.42	0.56	1.44	0.13	50% of 93%	0.5	92% of 50%
14 W14 (W14)	W	1.20	1.20	1.44	0.56	0.42	0.56	1.44	0.13	50% of 93%	0.5	92% of 50%
15 W15 (W15)	W	2.10	2.10	4.41	0.56	0.42	0.56	2.10	0.80	17% of 93%	3.9	93% of 50%
16 W16 (W16)	W	2.10	2.10	4.41	0.56	0.42	0.56	2.10	0.80	17% of 93%	3.9	93% of 50%
17 W17 (W17)	W	3.00	2.00	6.00	0.56	0.42	0.56	2.00	1.21	14% of 93%	3.1	93% of 50%
18 W18 (W18)	W	3.00	2.00	6.00	0.56	0.42	0.56	2.00	1.21	14% of 93%	3.1	93% of 50%
19 W19 (W19)	E	3.00	2.00	6.00	0.56	0.42	0.56	2.00	1.21	14% of 93%	3.1	93% of 50%
20 W20 (W20)	E	3.00	2.00	6.00	0.56	0.42	0.56	2.00	1.21	14% of 93%	3.1	93% of 50%

**IMPORTANT NOTICE AND DISCLAIMER IN RESPECT OF THE GLAZING CALCULATOR**  
 The Glazing Calculator has been developed by the Council of Building Surveyors. It is provided "as is" and without any warranty or representation of accuracy of any kind, including that it is fit for any purpose or of insalubrious quality or functions as intended or to be used.  
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## ENERGY EFFICIENCY NOTES

- N.C.C. Part 3.12.3 - Building Sealing
- Roof joints (3.12.3.2)
- Roof lights must be sealed when serving a habitable room using an impermeable ceiling diffuser installed at the ceiling or internal lining level
- External windows and doors (3.12.3.3)
- A seal to restrict air infiltration must be fitted to each edge of an external door, operable window and other such opening when serving a habitable room.
- Exhaust fans (3.12.3.4)
- An exhaust fan must be fitted with a sealing device such as a self-closing damper, filter or the like when serving a habitable room.
- N.C.C. Part 3.12.5 - Services
- Artificial Lighting (3.12.5.5)
- The lamp power density or illumination power density of artificial lighting, excluding heaters that emit light, must not exceed the allowance of 5 W/m<sup>2</sup> internally and 4 W/m<sup>2</sup> on verandas or balconies.
- Outdoor lighting must comply with N.C.C. 3.12.5.5 E

## LIGHTING CALCULATOR - N.C.C V2 3.12.5.5

**LIGHTING CALCULATOR FOR USE WITH N.C.C VOLUME ONE AND 3.12.5.5 VOLUME TWO (First Issued with NCC 2014)**

Building Identification: 643 Relbia Road, Relbia  
 Proposed floor: 4th Floor  
 Area: 23.2m<sup>2</sup>  
 Number of rooms: 12  
 Climate zone: 7

Constants: 1.2, 1.2  
 Allowances: 5.5, 80.2

ID	Room Name	Area (m <sup>2</sup> )	Volume (m <sup>3</sup> )	Room Type	Room Use	Room Category	Room Purpose	Room Use	Room Category	Room Purpose	Room Use	Room Category	Room Purpose
1	Bedroom	11.4m <sup>2</sup>	41.0m <sup>3</sup>	Bedroom	Bedroom	Bedroom	Bedroom	Bedroom	Bedroom	Bedroom	Bedroom	Bedroom	Bedroom
2	Bathroom	4.1m <sup>2</sup>	14.8m <sup>3</sup>	Bathroom	Bathroom	Bathroom	Bathroom	Bathroom	Bathroom	Bathroom	Bathroom	Bathroom	Bathroom
3	Living Room	24.2m <sup>2</sup>	87.1m <sup>3</sup>	Living Room	Living Room	Living Room	Living Room	Living Room	Living Room	Living Room	Living Room	Living Room	Living Room
4	Dining Room	11.4m <sup>2</sup>	41.0m <sup>3</sup>	Dining Room	Dining Room	Dining Room	Dining Room	Dining Room	Dining Room	Dining Room	Dining Room	Dining Room	Dining Room
5	Kitchen	11.4m <sup>2</sup>	41.0m <sup>3</sup>	Kitchen	Kitchen	Kitchen	Kitchen	Kitchen	Kitchen	Kitchen	Kitchen	Kitchen	Kitchen
6	Laundry	4.1m <sup>2</sup>	14.8m <sup>3</sup>	Laundry	Laundry	Laundry	Laundry	Laundry	Laundry	Laundry	Laundry	Laundry	Laundry
7	W.C.	4.1m <sup>2</sup>	14.8m <sup>3</sup>	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.
8	W.C.	4.1m <sup>2</sup>	14.8m <sup>3</sup>	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.
9	W.C.	4.1m <sup>2</sup>	14.8m <sup>3</sup>	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.
10	W.C.	4.1m <sup>2</sup>	14.8m <sup>3</sup>	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.
11	W.C.	4.1m <sup>2</sup>	14.8m <sup>3</sup>	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.
12	W.C.	4.1m <sup>2</sup>	14.8m <sup>3</sup>	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.	W.C.

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**DO NOT SCALE DRAWINGS. IF IN DOUBT - ASK**

LOCAL COUNCIL: LAUNCESTON CITY COUNCIL  
 ACCREDITATION COMPLIANCE NUMBER: CC-282 Z  
 TITLE: PROPOSED NEW RESIDENCE AT 643 RELBIA ROAD RELBIA FOR MR & MRS SHEPHERDSON  
 FOLD REFERENCE: 1302344

DRAWING NO: P15  
 SCALE: 1:5

**Lateral ARCHITECTURE**

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 Lateral Architect  
 7/4 Legend Architecture  
 Newcastle, Tennessee, 7246  
 AIN: 75 430 740 378  
 Telephone: (601) 632 8910  
 Email: info@lateralarchitect.com.au

DESIGNER: R.Hall  
 DRAWN: A.Morris  
 CHECKED: M.Hall  
 DATE: 22-04-14  
 REVISION DATE: 11-11-14  
 SCALE:

**Exhibited**

# **FJA Solutions**

**Planning Report**

**To**

**Support an Application for the Erection of a Dwelling**

**643 Relbia Road**

**RELBIA**

**For**

**Mr and Mrs Shepherdson**

**Oct 2014**

**PROPOSAL**

It is proposed to carry out the following development:

- erect a dwelling,
- a shed
- four shipping containers, and
- Alter the access point
- Demolish illegal structures

on this 17ha site accessed off Relbia Road.

The dwelling will be single level with a pitched roof. It will be formed on the slab of the current machinery shed. The shed will be relocated to the north of the current small sheds.

The house will have three bedrooms, two/three public rooms and the usual amenity areas.

The current access point to the site (a blind hill) will be modified to come off Relbia Road on the outside of a corner where sightline distance is better.

**BACKGROUND**

The applicant purchased this site in March 2014. At the time of purchase the improvements on site were:

- A caravan
- A large annex
- A large machinery shed and open car port
- Three small domestic sized sheds
- Open structure
- Working septic system
- An overgrown vineyard

Enquiries to Northern Midlands Council revealed that none of the buildings/structures (with the exception of the machinery shed) on site had planning or building permissions. As a result the applicant commissioned actions to rectify this situation. In the meantime Council has served a series of Planning and Building notices on the applicant relative to the illegal structures.

Many of these structures had been on site for 20 years – erected under the Evandale Council. This application will seek to rectify the situation on this site.

**SITE/TITLE**

The site is legally known as:

<i>Property Address</i>	'KELLY'S CREEK VINEYARD' - 643 RELBIA RD RELBIA TAS 7258
<i>Property ID</i>	<a href="#">View Details</a>
<i>Title Reference</i>	13034/4

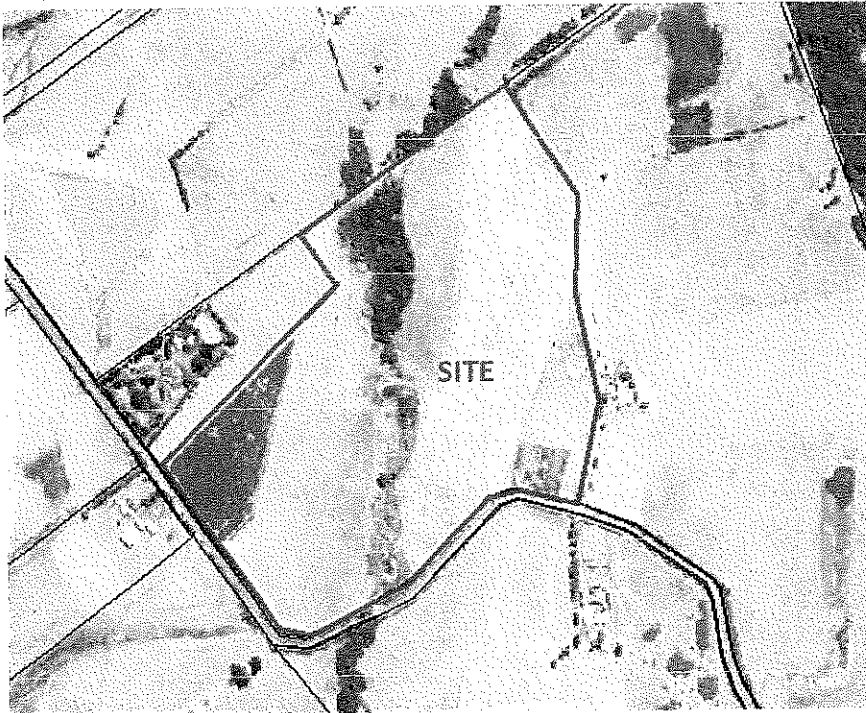


Figure 1 – Site Plan

**USE OF SITE AND SURROUNDING USE OF LAND**

The land is used for grazing – there is a deep creek feature running north-south bisecting the site. A disused vineyard is located in the western sector of the site. A section of replanted vegetation is located in the eastern sector facing Relbia Road.

The buildings on site are identified below:

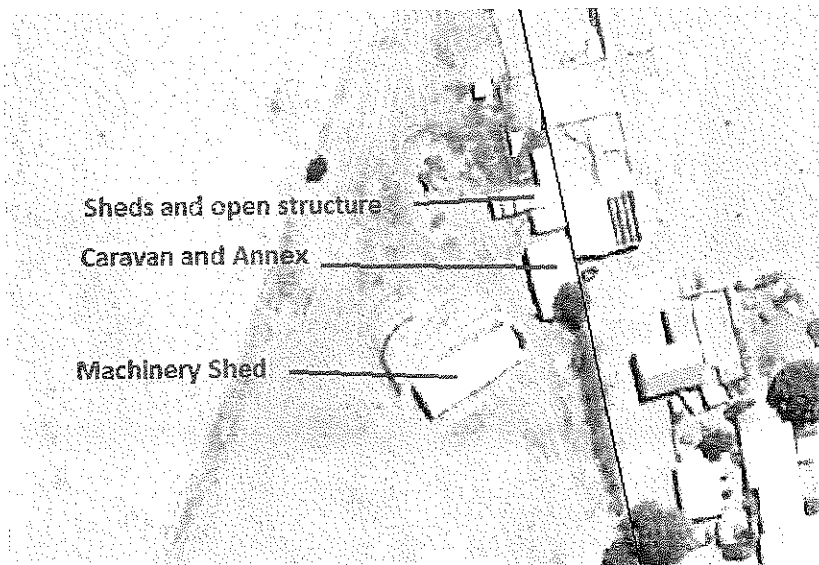


Figure 2 – Buildings on Site