

**PLAN 4**

**PLANNING APPLICATION P17-204**

**15 LAYCOCK STREET, LONGFORD**

**ATTACHMENTS**

- A Application & plans, correspondence with applicant
- B Responses from referral agencies
- C Representation & applicant's response

15 August 2017

Northern Midlands Council  
13 Smith Street,  
LONGFORD TAS 7301

ATTN: Northern Midlands Planning

Dear Planning Team

### **15 Laycock Street, Longford Development Application for Multiple Dwellings**

The following letter has been prepared in support of the application for five (5) multiple dwellings and has been prepared against the relevant provisions of the Northern Midlands Interim Planning Scheme 2013. The letter sets out a detailed assessment of the proposal and is to be read in conjunction with the proposal drawings and Traffic Impact Assessment report. The site conditions at the proposed development location and the surrounding area have been taken into account in the assessment of conformance and informed the opinions expressed throughout.

#### **Proposal**

It is proposed to construct five (5) multiple dwellings on land at 15 Laycock Street, Longford currently comprised of an existing dwelling situated on 2,023m<sup>2</sup>. The dwellings are to be constructed of face brick with a colourbond roof as shown in the DA drawings.

All of the dwellings are to be provided with a single undercover car space (carport or garage) and an additional designated space located adjacent to the respective dwelling. An additional two visitor spaces designated as common property is proposed within the development in order to provide additional car parking. Therefore, there are a total of fifteen (15) car parking spaces within the proposed development, with five (5) of these separately accessible direct from street.

Each dwelling will have its own private open space area, formed using a combination of concreted outdoor areas, lawn and landscaping. Landscaping is also proposed adjacent to the driveway areas in front of the dwellings. Clothesline facilities are proposed within each of the open space areas associated with the dwellings. An external garden shed is also proposed to provide additional storage of gardening equipment.

#### **Subject Site**

The site is contained in CT Vol. 2244 Fol. 54. The total land area occupied by the site is 2,023m<sup>2</sup>. The long axis of the site, which is around 70 metres in length, is orientated to the east.

The existing access strip associated with the property, has a 30 metre wide frontage to Laycock Street with an existing crossover already located at the frontage to the existing dwelling. A new crossover is proposed to access the new dwellings. The rear and western side boundaries of the site abut existing residential dwellings.

#### **Surrounding Development**

The properties which directly adjoin the site include a:

- Single dwelling to the north at 13 Laycock Street,
- Single dwelling to the south at 17 Laycock Street, and
- A dwelling to the east at 8 George Hudson Place.

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### **Description of the Wider Area**

The site is situated in an existing residential zone on the eastern side of Longford. Within the wider surrounding area, including the properties contained in Malcombe Street, Laycock Street and Smith Court there are a number of existing residential properties and this includes a handful that have been developed for multiple dwellings.

### **Closed Residential Zone Intent**

The site is zoned General Residential. Use and development for Multiple Dwellings is allowed within the zone subject to a permit. The intent of the General Residential Zone states:

- 1) *To provide for residential use or development that accommodates a range of dwelling types at suburban densities, where full infrastructure services are available or can be provided.*
- 2) *To provide for compatible non-residential uses that primarily serve the local community.*
- 3) *Non-residential uses are not to be at a level that distorts the primacy of residential uses within the zones, or adversely affect residential amenity through noise, activity outside of business hours traffic generation and movement or other off site impacts.*
- 4) *To encourage residential development that respects the neighbourhood character and provides a high standard of residential amenity.*

**Comment:** Taking account of the description of the site and wider context above, it is considered that the surrounding area is predominantly developed at light to medium density characterised by single dwellings. Multi-unit development is not the prevailing housing style of characteristic of the area or the street and, more significantly, the overall balance between single and multi-unit development will not substantially change as a result of this proposal.

The application complies with the relevant development requirements for the General Residential zone, including site cover, setback and height of buildings in particular.

### **Development Requirements**

Clauses 10.4.1 to 10.4.12 (inclusive) outline the requirements for development within the General Residential zone. These are set out in terms of Performance Criteria (PC), which must be achieved in approving a development application, and Acceptable Solutions (AS), which are one way of ensuring that the respective PCs are met. If the AS is not met, or if there is none available, justification and rationale demonstrating compliance with the corresponding PC is required. These development requirements are addressed as follows:

**Residential density for multiple dwellings (10.4.1):** A total of six dwellings are proposed on the 2,023m<sup>2</sup> title. Combined this achieves an average density per dwelling of 337m<sup>2</sup> which exceeds the per lot 325 m<sup>2</sup> density requirement. The development is as such compatible with the density of the surrounding area.

**Setback from street (10.4.2):** The 4.5 metre requirement under compliance measure is met.

**Site coverage/private open space (10.4.3):** The dwellings do not exceed 50% coverage of the site and provide adequate fenced off private open space that is appropriate to their size. The site also provides pervious surfaces greater than 25% of the total area in line with the planning requirement and provides space for gardening activities. The proposed development additionally provides a grassed/concreted private open space exceeding the 24m<sup>2</sup> requirement adjoining the dining room of each dwelling.

**Sunlight and overshadowing (10.4.4):** The proposed dwellings are orientated to face northwards and thus allow sunlight to enter the lounge, kitchen and dining area. They are also appropriately positioned so as not to shadow neighbouring dwellings and or each other.

**Garages (10.4.5):** The single garage located on the proposed roadside dwelling is not large enough to

dominate the primary frontage.

**Privacy (10.4.6):** The proposed single storey dwellings are to be located on the existing block, which will be surrounded by a six foot fence. This shall provide privacy to residents on the site and to the neighbours.

**Waste storage (10.4.8):** Appropriately sized areas for waste storage has been set aside for both dwellings. These areas are screened from the frontage and from each other.

**Storage (10.4.9):** Each dwelling is to be provided with an external garden shed that meets the 6 m<sup>3</sup> requirement.

**Common property (10.4.10):** At the front of the proposed dwellings the driveway has been designated as being common property.

**Site Services (10.4.12):** Each of the dwellings is provided with a mailbox.

**Infrastructure Available:** Reticulated water and sewerage services are available, and the site has frontage to a road maintained by the Council.

This assessment indicates that the application is appropriately submitted to Northern Midlands Council for approval subject to relevant conditions. Should you have any further queries please do not hesitate to contact the undersigned.

Yours faithfully

**Sam Chugg**

0437 382 792

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# TRAFFIC IMPACT ASSESSMENT

15 Laycock Street,  
Longford

*Prepared By:*

**Sam Chugg**  
Bachelor of Engineering (Civil), MBA

11 Cressy Road, Longford 7301  
Mobile: 0437 382 792  
Email: [srchugg@live.com.au](mailto:srchugg@live.com.au)

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## **Background**

The following report provides a traffic impact assessment for a proposed 5 x 2 bedroom unit development at 15 Laycock Street, Longford.

## **Introduction**

The Northern Midlands Council (NMC) requires that a Traffic Impact Assessment is prepared to comply with the requirements of Code E4 of the Northern Midlands Interim Planning Scheme 2013. Sam Chugg, a qualified civil engineer, has prepared this report to provide an objective description of the impacts and traffic effects of the proposed development.

Preparation of the report has included a site visit and should be read in conjunction with the proposal DA drawings prepared by Scolyer Designs for 15 Laycock Street, Longford.

## **Traffic Impact Assessment (TIA)**

A traffic impact assessment (TIA) is a process of compiling and analysing information on the impacts that a specific development proposal is likely to have on the operation of roads and transport networks. A TIA should not only include general impacts relating to traffic management, but should also consider specific impacts on all road users, including on-road public transport, pedestrians, cyclists and heavy vehicles.

This TIA has been prepared in accordance with the Department of State Growth (DSG – formerly DIER) publication, *A Framework for Undertaking Traffic Impact Assessments, 2007*. It has also been prepared with reference to the Austroads publication, *Guide to Traffic Management, Part 12: Traffic Impacts of Developments, 2009*.

The Northern Midlands Interim Planning Scheme, 2013, sets out the requirements in E4.5 of the Road and Rail Assets Code as follows:

<b>E4.5.1</b> A TIA is required to demonstrate compliance with performance criteria.
<b>E4.5.2</b> A TIA for roads must be undertaken in accordance with Traffic Impact Assessment Guidelines, Department of Infrastructure, Energy and Resources, September 2007.
Australian Guidelines and Australian Standards are to be used as the basis for any required road or junction design.
<b>E4.5.3</b> A TIA must be accompanied by written advice as to the adequacy of the TIA from the: <ul style="list-style-type: none"> <li>a) road authority in respect of a road; and</li> <li>b) rail authority in respect of a railway.</li> </ul>
<b>E4.5.4</b> The Council must consider the written advice of the relevant authority when assessing an application which relies on performance criteria to meet an applicable standard.

## The Site

The site is located at 15 Laycock Street, Longford, as shown below.



Source: Google Maps

**Figure 1 – Site Location**

Currently an existing three bedroom dwelling is located on the site at the front of the block. The development proposal is to construct four units behind and one dwelling adjacent to the existing dwelling.

Access to the existing dwelling will continue to be via the existing driveway, whilst a new double crossover is proposed to be constructed (to LGAT standards) to provide access to the new units.

## Information and Data Sources

Information from the following organisations have guided the preparation of this report:

- Department of State Growth – Crash and traffic data
- Northern Midlands Council – Planning Scheme
- Scolyer Designs

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## **Planning Scheme**

The Northern Midlands Interim Planning Scheme 2013 outlines the traffic, access and parking requirements for developments in the Northern Midlands municipality, and is referred to as the Planning Scheme throughout this report.

## **Existing Conditions**

### *Transport Network*

In considering this assessment, the transport network consists primarily of the property access points (existing and proposed) and Laycock Street.

Laycock Street is approximately 670 metres in length and connects between Pultney Street to the north and Bulwer Street to the south. It has a straight alignment, is approximately 11.5 metres wide and has nature strips and constructed footpaths adjacent to the road.

Laycock Street is a local residential road that primarily provides access for residential properties along its length. A speed limit of 50 km/hr applies to Laycock Street.

Laycock Street looking north and south of the proposed development is shown in Figure 2 and Figure 3 respectively.



Source: Google Street View

**Figure 2 – Looking North**

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Source: Google Street View

**Figure 3 – Looking South**

### Road Safety Performance

Crash data can provide valuable information on the road safety performance of a road network. Existing road safety deficiencies can be highlighted through the examination of crash data, which can assist in determining whether traffic generation from the proposed development may exacerbate any identified issues.

No crashes were reported in crash data from the Department of State Growth for a 5+ year period between 1st January 2010 to 1st November 2016.

As such the crash history during this period does not suggest that there are any specific, existing road safety deficiencies in the vicinity of the subject site.

### Proposed Development

The proposed development involves the construction of 5 units serviced by a new internal driveway to Laycock Street and existing driveway to service the existing dwelling. Parking is proposed for 15 parking spaces, consisting of the following:

- 2 off-street garage spaces (1 for existing dwelling; and 1 for roadside unit)
- 5 off-street carport spaces (1 for existing dwelling; and 1 for each of the 4 internal units)
- 6 off-street parking spaces (1 for each unit; and 1 for the existing dwelling)
- 2 visitor parking space

The proposed driveway to the north of the existing dwelling will extend approximately 60 metres perpendicular from the front boundary, whilst the existing secondary access on the southern boundary of the property is approximately 25 metres long.

## **Traffic Impacts**

### **Traffic Generation**

Traffic generation rates refer the RTA Guide. The RTA Guide states the following traffic generation rates for Smaller units and flats (up to two bedrooms):

- Daily vehicle trips 4-5 per dwelling
- Weekday peak hour vehicle trips 0.4-0.5 per dwelling

Based on these rates, the traffic generation from the subdivision is likely to be in the order of 25 trips per day, and 2.5 trips per hour during peak periods.

### **Planning Scheme Requirements**

Acceptable Solution A2 of E4.6.1 of the Planning Scheme states that:

“For roads with a speed limit of 60km/h or less the use must not generate more than a total of 40 vehicle entry and exit movements per day”.

In this case, the proposed development generates approximately 25 additional vehicle trips per day on Laycock Street and complies with Acceptable Solution A2 of E4.6.1.

### **Access Impacts**

Access to the site is via an existing and a new proposed driveway connecting to Laycock Street. This is shown in Figure 4 and Figure 5.



Source: Google Street View

**Figure 4 – Existing Access (15 Laycock Street)**

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Source: Google Street View

**Figure 5 – Proposed New Access (15 Laycock Street)**

The Acceptable Solution A1 of E4.7.2 of the Planning Scheme states that:

“for roads with a speed limit of 60km/h or less the development must include only one access providing both entry and exit, or two accesses providing separate entry and exit”.

In this case, two accesses are proposed on Laycock Street (50-km/h), and therefore fails to comply with Acceptable Solution A1 of E4.7.2.

The Performance Criteria, P1, of E4.7.2 states:

“For roads with a speed limit of 60km/h or less, the number, location, layout and design of accesses and junctions must maintain an acceptable level of safety for all road users, including pedestrians and cyclists.”

The following is relevant for the proposed development:

- Laycock Street is a low volume residential access road. The forecast additional traffic volume of 25 vehicle trips per day can be readily absorbed into the road

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network without any significant adverse impacts on capacity or efficiency or safety.

- Laycock Street is well serviced with pedestrian infrastructure, with footpaths and nature strips. The relatively low volume of traffic using Laycock Street results in a high level of service for pedestrians.
- The pedestrian path is situated along the opposite side of Laycock Street to the proposed development, suggesting passing foot traffic will be more concentrated on the other side of the road.
- The low volume on Laycock Street, with the wide carriageway provides a high level of service for cyclists in the network.

Based on the road conditions described above the two accesses can be designed to maintain an acceptable level of safety for all road users in a manner that is in alignment with existing access conditions provided on Laycock Street.

The existing access is not proposed to be modified (i.e. widened) during the development, whilst the safety of the design of the new access is comparable to other existing double crossovers on Laycock Street and surrounding area (eg. Malcombe Street).

Based on the above assessment and implementation of design recommendations, the proposed development meets the requirements of Performance Criteria P1 of E4.7.2 of the Planning Scheme.

#### Sight Distance Assessment

Schedule E4.7.4 of the Planning Scheme outlines the sight distance requirements at accesses. This is reproduced in Figure 6.

The Acceptable Solution A1(a) of the Planning Scheme requires Safe Intersection Sight Distance (SISD) to be provided as shown in Table 1.

The Planning Scheme SISD values are based on the measured 85th percentile speed<sup>1</sup> values for the frontage road. Based on the measured 85th percentile speeds at the site (50-km/h), the Planning Scheme requires SISD values of 80 metres. Available SISD exceeds this minimum value in both directions on Laycock Street (refer to Figure 2 and Figure 3), therefore the Acceptable Solution of E4.7.4 of the Planning Scheme is met.

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<sup>1</sup> The 85th percentile speed is the speed not exceeded by 85% of all vehicles.

<p><b>Objective</b></p> <p>To ensure that use and development involving or adjacent to accesses, junctions and level crossings allows sufficient sight distance between vehicles and between vehicles and trains to enable safe movement of traffic.</p>	
<p><b>Acceptable Solutions</b></p>	<p><b>Performance Criteria</b></p>
<p>A1 Sight distances at</p> <p>a) an access or junction must comply with the Safe Intersection Sight Distance shown in Table E4.7.4; and</p> <p>b) rail level crossings must comply with AS1742.7 <i>Manual of uniform traffic control devices - Railway crossings</i>, Standards Association of Australia; or</p> <p>c) if the access is a temporary access, the written consent of the relevant authority has been obtained.</p>	<p>P1 The design, layout and location of an access, junction or rail level crossing must provide adequate sight distances to ensure the safe movement of vehicles.</p>

Source: NMC

Figure 6 – Planning Scheme Sight Distance Requirements

Vehicle Speed km/h	Safe Intersection Sight Distance (SISD) Metres, for speed limit of:	
	60 km/h or less	Greater than 60 km/h
50	80	90
60	105	115
70	130	140
80	165	175
90		210
100		250
110		290

Source: NMC

Table 1 – Planning Scheme SISD Requirements (Table E4.7.4)

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### Pedestrian Impacts

The proposed development is well connected to the surrounding road network's pedestrian infrastructure and includes a footpath on the other side of Laycock Street at the proposed site.

The proposed development will generate some level of pedestrian activity (to/from Longford bus stops, town centre, school, etc). These movements can be accommodated safely and efficiently in the network.

### Road Safety Impacts

No significant road safety impacts are foreseen for the proposed development. This is based on the following:

- The surrounding road transport network is capable of absorbing the relatively small estimated traffic generation of the proposed development (with a peak estimated to be 2.5 vehicles per hour).
- Sight distance at the two accesses exceeds Planning Scheme requirements and therefore provides a safe access environment.
- The crash history of the surrounding road network near the subject site does not indicate that there are any specific road safety issues that are likely to be exacerbated by traffic generated by the proposed development.

## **Summary - Assessment to Rail and Road Code Northern Midlands Interim Planning Scheme**

Assessment in accordance with section E4.0 of the Road and Railway Assets Code indicates:

E4.6.1 A2 The assessed site traffic movements being approximately 25 additional vehicle trips per day will not result in an unacceptable level of service on Laycock Street – Complies.

E4.7.1 The site is more than 50 metres from a railway, and or Category 1 or 2 Road – Complies.

E4.7.2 A1 The development provides two accesses which does not comply with the defined Acceptable Solutions, however, the Performance Criteria of maintaining an acceptable level of safety for all road users is able to be met through appropriate design in a manner consistent with existing conditions along Laycock Street – Complies.

E4.7.3 Not applicable.

E4.7.4 The available sight distances are considered to comply with Table E4.7.4 relative to the approach speeds – Complies.

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## Conclusion

This traffic impact assessment (TIA) investigated the traffic impacts of a proposed 5 dwelling development at 15 Laycock Street, Longford. The development meets the requirements of the Road and Railway Assets Code of the Planning Scheme. This TIA has been conducted following a review of available traffic data and information, Austroads Guidelines, Australian Standards, Planning Scheme and other supplementary traffic data and information.

### Key Conclusions:

The traffic generated by the proposed development, when operating at the maximum output, will be in the order of 2.5 light vehicle movements during peak hour, and will not have any significant adverse impacts on the surrounding road network in terms of traffic efficiency or road safety.

Adequate sight distance is provided at the site access in accordance with the Planning Scheme requirements for the prevailing vehicle speeds.

Therefore, based on the findings of this report, and subject to the recommendations above, the proposed development is supported on traffic grounds.



**August 2017**

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## **E6 Car Parking and Sustainable Transport Code**

### **E6.1 Purpose of Code**

E6.1.1 The purpose of this provision is to:

- (a) ensure that an appropriate level of car parking facilities are provided to service new land use and development having regard to the operations on the land and the nature of the locality; and
- (b) ensure that cycling, walking and public transport are encouraged as a means of transport in urban areas; and
- (c) ensure access for cars and cyclists and delivery of people and goods is safe and adequate; and
- (d) ensure that parking does not adversely impact on the amenity of a locality and achieves high standards of urban design; and
- (e) ensure that the design of car and bicycle parking space and access meet appropriate design standards; and
- (f) provide for the implementation of parking precinct plans.

### **E6.2 Application of Code**

E6.2.1 This code applies to all use and development of land.

### **E6.3 Required Application Information**

E6.3.1 In addition to the normal requirements of development applications and where car parking or sustainable transport facilities are required to be provided, a plan drawn to scale and dimensioned must be provided as part of the application showing:

- (a) all car spaces to be provided on the site (or being relied on as part of the development); and
- (b) access strips and manoeuvring and circulation spaces; and
- (c) all access strips onto the site from roads; and
- (d) details of the existing and proposed surface treatments for all car parking access strips and manoeuvring and circulation spaces; and
- (e) all facilities proposed for cycling or public transport users.

E6.3.2 Council may also require a Traffic Impact Assessment from a suitably qualified person to accompany a development application where it is assessed as having the potential to adversely impact on the traffic circulation, safety or network efficiency in the surrounding area.

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## E6.4 Definition of Terms

- Category 1 – Trunk Road means as defined in *Tasmania State Road Hierarchy (DIER, 2007)*
- Category 2 – Regional Freight Route means as defined in *Tasmania State Road Hierarchy (DIER, 2007)*
- Category 3 – Regional Access Road means as defined in *Tasmania State Road Hierarchy (DIER, 2007)*
- Category 4 – Feeder Road means as defined in *Tasmania State Road Hierarchy (DIER, 2007)*
- Category 5 – Other Road means as defined in *Tasmania State Road Hierarchy (DIER, 2007)*
- Parking Precinct Plan means a strategic plan relating to the parking of cars, bicycles and other vehicles within a defined area which is incorporated into the planning scheme and listed as additional component to this Code. A Parking Precinct Plan must include the following information:
- a) the purpose of the plan; and
  - b) the area to which the plan applies; and
  - c) the parking outcomes to be achieved by the plan; and
  - d) an assessment of car parking demand and supply in the precinct area; and
  - e) the locational, financial, landscape and other actions or requirements necessary to implement the parking precinct plan.

Parking Precinct Plans are contained in Table E6.6 to this code.

## E6.5 Use or Development Exempt from this Code

E6.5.1 There are no exemptions to this code.

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**E6.6 Use Standards**

**E6.6.1 Car Parking Numbers**

**Objective**

To ensure that an appropriate level of car parking is provided to service use.

**Acceptable Solutions**

- A1 The number of car parking spaces must not be less than the requirements of:
  - a) Table E6.1; or
  - b) a parking precinct plan contained in Table E6.6: Precinct Parking Plans (except for dwellings in the General Residential Zone).

**Performance Criteria**

- P1 The number of car parking spaces provided must have regard to:
  - a) the provisions of any relevant location specific car parking plan; and
  - b) the availability of public car parking spaces within reasonable walking distance; and
  - c) any reduction in demand due to sharing of spaces by multiple uses either because of variations in peak demand or by efficiencies gained by consolidation; and
  - d) the availability and frequency of public transport within reasonable walking distance of the site; and
  - e) site constraints such as existing buildings, slope, drainage, vegetation and landscaping; and
  - f) the availability, accessibility and safety of on-road parking, having regard to the nature of the roads, traffic management and other uses in the vicinity; and
  - g) an empirical assessment of the car parking demand; and
  - h) the effect on streetscape, amenity and vehicle, pedestrian and cycle safety and convenience; and
  - i) the recommendations of a traffic impact assessment prepared for the proposal; and
  - j) any heritage values of the site; and
  - k) for residential buildings and multiple dwellings, whether parking is

**Car spaces provided**  
 = 10 internal car spaces + 5 accessible from road  
 = 2 (Unit 1) + 2 (Unit 2) + 2 (Unit 3) + 2 (Unit 4) + 2 (Unit 5) + 3 (existing) + 2 (visitor)

This complies with requirement in Table E6.1

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	adequate to meet the needs of the residents having regard to:  i) the size of the dwelling and the number of bedrooms; and  ii) the pattern of parking in the locality; and  iii) any existing structure on the land.
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**E6.6.2 Bicycle Parking Numbers**

**Objective**

To encourage cycling as a mode of transport within areas subject to urban speed zones by ensuring safe, secure and convenient parking for bicycles.

<b>Acceptable Solutions</b>	<b>Performance Criteria</b>
<p>A1.1 Permanently accessible bicycle parking or storage spaces must be provided either on the site or within 50m of the site in accordance with the requirements of Table E6.1; or</p> <p>A1.2 The number of spaces must be in accordance with a parking precinct plan contained in Table E6.6: Precinct Parking Plans.</p>	<p>P1 Permanently accessible bicycle parking or storage spaces must be provided having regard to the:</p> <p>a) likely number and type of users of the site and their opportunities and likely preference for bicycle travel; and</p> <p>b) location of the site and the distance a cyclist would need to travel to reach the site; and</p> <p>c) availability and accessibility of existing and planned parking facilities for bicycles in the vicinity.</p>

**E6.6.3 Taxi Drop-off and Pickup**

**Objective**

To ensure that taxis can adequately access developments.

<b>Acceptable Solutions</b>	<b>Performance Criteria</b>
<p>A1 One dedicated taxi drop-off and pickup space must be provided for every 50 car spaces required by Table E6.1 or part thereof (except for dwellings in the General Residential Zone).</p>	<p>P1 No performance criteria.</p>

**Bicycle parking**

Each of the dwellings is able to park a bicycle within the garage or carport, without impeding the ability to park a car.

This complies with requirement in Table E6.1

Not applicable.



**E6.6.4 Motorbike Parking Provisions**

Objective	
To ensure that motorbikes are adequately provided for in parking considerations.	
Acceptable Solutions	Performance Criteria
A1 One motorbike parking space must be provided for each 20 car spaces required by Table E6.1 or part thereof.	P1 No performance criteria.

Not applicable.

**E6.7 Development Standards**

**E6.7.1 Construction of Car Parking Spaces and Access Strips**

Objective	
To ensure that car parking spaces and access strips are constructed to an appropriate standard.	
Acceptable Solutions	Performance Criteria
A1 All car parking, access strips manoeuvring and circulation spaces must be: <ul style="list-style-type: none"> <li>a) formed to an adequate level and drained; and</li> <li>b) except for a single dwelling, provided with an impervious all weather seal; and</li> <li>c) except for a single dwelling, line marked or provided with other clear physical means to delineate car spaces.</li> </ul>	P1 All car parking, access strips manoeuvring and circulation spaces must be readily identifiable and constructed to ensure that they are useable in all weather conditions.

**Construction**

Access strips and parking/driveway manoeuvring and circulation spaces are readily identifiable and to be constructed from concrete which is useable in all weather conditions.

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**E6.7.2 Design and Layout of Car Parking**

**Objective**

To ensure that car parking and manoeuvring space are designed and laid out to an appropriate standard.

**Acceptable Solutions**

- A1.1 Where providing for 4 or more spaces, parking areas (other than for dwellings in the General Residential Zone) must be located behind the building line; and
- A1.2 Within the General residential zone, provision for turning must not be located within the front setback for residential buildings or multiple dwellings.

- A2.1 Car parking and manoeuvring space must:
  - a) have a gradient of 10% or less; and
  - b) where providing for more than 4 cars, provide for vehicles to enter and exit the site in a forward direction; and
  - c) have a width of vehicular access no less than prescribed in Table E6.2 and Table E6.3, and
- A2.2 The layout of car spaces and access ways must be designed in accordance with *Australian Standards AS 2890.1 - 2004 Parking Facilities, Part 1: Off Road Car Parking*.

**Performance Criteria**

- P1 The location of car parking and manoeuvring spaces must not be detrimental to the streetscape or the amenity of the surrounding areas, having regard to:
  - a) the layout of the site and the location of existing buildings; and
  - b) views into the site from the road and adjoining public spaces; and
  - c) the ability to access the site and the rear of buildings; and
  - d) the layout of car parking in the vicinity; and
  - e) the level of landscaping proposed for the car parking.
- P2 Car parking and manoeuvring space must:
  - a) be convenient, safe and efficient to use having regard to matters such as slope, dimensions, layout and the expected number and type of vehicles; and
  - b) provide adequate space to turn within the site unless reversing from the site would not adversely affect the safety and convenience of users and passing traffic.

Provision of turning provided to allow vehicles to exit site front ways (refer turning templates).

Car parking is on a relatively flat location, allows exiting in a forward direction. The driveway complies with 4.5m for initial 7m from road carriageway and vehicular access width (3m) in table E6.2 and access/parking widths (varies) in E6.3. Turning templates are in accordance with Aus Standards.

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**E6.7.3 Car Parking Access, Safety and Security**

Objective

To ensure adequate access, safety and security for car parking and for deliveries.

**Acceptable Solutions**

**Performance Criteria**

- A1 Car parking areas with greater than 20 parking spaces must be:
- a) secured and lit so that unauthorised persons cannot enter or;
  - b) visible from buildings on or adjacent to the site during the times when parking occurs.

- P1 Car parking areas with greater than 20 parking spaces must provide for adequate security and safety for users of the site, having regard to the:
- a) levels of activity within the vicinity; and
  - b) opportunities for passive surveillance for users of adjacent building and public spaces adjoining the site.

**E6.7.4 Parking for Persons with a Disability**

Objective

To ensure adequate parking for persons with a disability.

**Acceptable Solutions**

**Performance Criteria**

- A1 All spaces designated for use by persons with a disability must be located closest to the main entry point to the building.

- P1 No performance criteria.

- A2 One of every 20 parking spaces or part thereof must be constructed and designated for use by persons with disabilities in accordance with *Australian Standards AS/NZ 2890.6 2009*.

- P2 No performance criteria.

NOT APPLICABLE

NOT APPLICABLE  
(disabled persons can park appropriately close to the entrance of the dwelling)

Exhibited

**E6.7.6 Loading and Unloading of Vehicles, Drop-off and Pickup****Objective**

To ensure adequate access for people and goods delivery and collection and to prevent loss of amenity and adverse impacts on traffic flows.

**Acceptable Solutions****Performance Criteria**

- A1 For retail, commercial, industrial, service industry or warehouse or storage uses:
- a) at least one loading bay must be provided in accordance with Table E6.4; and
  - b) loading and bus bays and access strips must be designed in accordance with *Australian Standard AS/NZS 2890.3 2002* for the type of vehicles that will use the site.

- P1 For retail, commercial, industrial, service industry or warehouse or storage uses adequate space must be provided for loading and unloading the type of vehicles associated with delivering and collecting people and goods where these are expected on a regular basis.

NOT APPLICABLE





**E6.8 Provisions for Sustainable Transport**

**E6.8.1 Bicycle End of Trip Facilities**

Not used in this planning scheme

**E6.8.2 Bicycle Parking Access, Safety and Security**

Objective	
To ensure that parking and storage facilities for bicycles are safe, secure and convenient.	
Acceptable Solutions	Performance Criteria
<p>A1.1 Bicycle parking spaces for customers and visitors must:</p> <ul style="list-style-type: none"> <li>a) be accessible from a road, footpath or cycle track; and</li> <li>b) include a rail or hoop to lock a bicycle to that meets <i>Australian Standard AS 2890.3 1993</i>; and</li> <li>c) be located within 50m of and visible or signposted from the entrance to the activity they serve; and</li> <li>d) be available and adequately lit in accordance with <i>Australian Standard AS/NZS 1158 2005 Lighting Category C2</i> during the times they will be used; and</li> </ul> <p>A1.2 Parking space for residents' and employees' bicycles must be under cover and capable of being secured by lock or bicycle lock.</p>	<p>P1 Bicycle parking spaces must be safe, secure, convenient and located where they will encourage use.</p>
<p>A2 Bicycle parking spaces must have:</p> <ul style="list-style-type: none"> <li>a) minimum dimensions of:             <ul style="list-style-type: none"> <li>i) 1.7m in length; and</li> <li>ii) 1.2m in height; and</li> <li>iii) 0.7m in width at the handlebars; and</li> </ul> </li> <li>b) unobstructed access with a width of at least 2m and a gradient of no more 5% from a public area where cycling is allowed.</li> </ul>	<p>P2 Bicycle parking spaces and access must be of dimensions that provide for their convenient, safe and efficient use.</p>

Bicycles may be either secured within garage or carport or secured to poles within fenced-off area of the property.

Bicycle parking space is appropriate for the 5 x 2 bedroom units + existing dwelling.

Exhibited

**E6.8.5 Pedestrian Walkways**

Pedestrians are able to access all dwellings safely. Two roadside dwellings are accessible direct from street and the four internal dwellings are accessed via a shared driveway that exceeds the 3m width requirement along its entire length. There are a total of 10 car spaces accessed using the shared driveway, which in accordance to Table E6.5 does not require a separate pedestrian access.

<p><b>Objective</b></p> <p>To ensure pedestrian safety is considered in development</p>	
<p><b>Acceptable Solution</b></p>	<p><b>Performance Criteria</b></p>
<p>A1 Pedestrian access must be provided for in accordance with Table E6.5.</p>	<p>P1 Safe pedestrian access must be provided within car park and between the entrances to buildings and the road.</p>

Exhibited

**Table E6.1: Parking Space Requirements**

Use	Parking Requirement	
	Vehicle	Bicycle
<b>Bulky goods sales</b>	1 space per employee + 1 space per 100m <sup>2</sup> net floor area	1 space per 500m <sup>2</sup> net floor area
<b>Business and professional services:</b>		
bank, office, real estate agency, travel agent	1 space per employee + 1 space per 50m <sup>2</sup> net floor area	1 space + 1 space per 500m <sup>2</sup> net floor area
funeral parlour	1 space per employee + 1 visitor space + 1 space per 4 chapel seats	1 space per 50 chapel seats
call centre	1 space per employee	1 space per 5 employees
doctors' surgery, clinic, consulting room, veterinary surgery	4 spaces per professional practitioner	No requirement set
<b>Emergency services</b>		
<b>Community meeting and entertainment:</b>		
fire/ambulance station	1 space per employee	No requirement set
art gallery, church, conference centre, dancing school, exhibition centre, library, cinema, theatre, function centre, hall, indoor recreation, gymnasium, cemetery, crematorium	1 space per 20m <sup>2</sup> of public area or 1 space per 4 seats whichever is greater	1 space per 50m <sup>2</sup> net floor area or 1 space per 40 seats whichever is greater
<b>Corrective institution</b>	1 space per 2 employees + 1 space per 5 inmates	1 space per 10 employees
<b>Crematoria and cemetery</b>	1 space per employee + 1 visitor space + 1 space per 4 chapel seats	1 space per 50 chapel seats

Exhibited

<b>Domestic animal breeding, boarding or training</b>	1 space per staff member + 2 visitor spaces	No requirement set
<b>Educational and occasional care</b>	1 space per employee + 1 space per 6 tertiary or training students	1 space per 5 staff and students
<b>Equipment and machinery sales and hire</b>	1 space per 50m <sup>2</sup> net floor area	No requirement
<b>Extractive industry</b>	1 space per 2 employees	1 space per 10 employees
<b>Food services</b> (restaurant, cafe, take-away)	1 space per 15m <sup>2</sup> net floor area + 6 queuing spaces for drive-through	1 space per 75m <sup>2</sup> net floor area
<b>Retail and hire</b> (amusement centre, betting agency, department store, market, supermarket, video shop)	1 space per 30m <sup>2</sup> net floor area	1 space per 100m <sup>2</sup> net floor area
<b>Hospital services</b>	1 space per 4 beds + 1 space per doctor + 1 space per 2 employees	1 space per 10 beds
<b>Hotel industry</b> (hotel, bottle shop, tavern)	1 space per 20m <sup>2</sup> of net public area + 1 space per bedroom + 6 spaces for drive-in bottle shop	1 space per 100m <sup>2</sup> net floor area
<b>Manufacturing and processing</b> (boat-building, brick, cement works, furniture, glass, metal, wood and textile making)	1 space per 200m <sup>2</sup> net floor area or 2 spaces per 3 employees (whichever is greater)	1 space per 5 employees
<b>Minor utilities</b>	No requirements set	No requirements set
<b>Motor racing facility</b>	1 space per 5 visitor seats	No requirement set
<b>Natural and cultural values management</b>	No requirements set	No requirements set
<b>Passive recreation</b>	No requirements set	No requirements set

Exhibited

<b>Recycling and waste disposal</b> (scrap, car wrecking yard, refuse disposal/transfer station)	1 space per 500 m <sup>2</sup> of the site + 1 space per employee	1 space per 5 employees
<b>Research and development</b>	1 space per 100m <sup>2</sup> or 2 spaces per 3 staff whichever is greater	1 space per 5 employees
<b>Residential:</b>		1 space per unit or 1 spaces per 5 bedrooms in other forms of accommodation
If a 1 bedroom or studio dwelling in the General Residential Zone (including all rooms capable of being used as a bedroom)	1 space per dwelling	
If a 2 or more bedroom dwelling in the General Residential Zone (including all rooms capable of being used as a bedroom)	2 spaces per dwelling	
visitor parking for multiple dwellings in the General Residential Zone	1 dedicated space per 4 dwellings (rounded up to the nearest whole number); or  If on an internal lot or located at the head of a cul-de-sac, 1 dedicated space per 3 dwellings (rounded up to the nearest whole number)	
Residential use in any other zone or any other residential use in the General Residential Zone	1 space per bedroom or 2 spaces per 3 bedrooms + 1 visitor space for every 5 dwellings	
<b>Resource development:</b>		
Aquaculture	2 spaces per 3 employees	1 spaces per 5 employees
Other	No requirement	No requirement

Exhibited

Resource processing <b>(abattoir, fish/cheese/milk processing)</b>	2 spaces per 3 employees	1 space per 5 employees
<b>Service industry</b> (car wash, commercial laundry, repair garage, panel beater)	1 space per 80m <sup>2</sup> or 2 spaces per 3 employees whichever is greater	1 space per 5 employees
<b>Sports and Recreation:</b>		
golf course	4 spaces per golf hole	1 space per tennis court, 1 space per 100m <sup>2</sup> of pool + 1 space per 30 spectator places
bowling green,	6 spaces per bowling rink	
tennis courts (if not associated with a single dwelling)	4 spaces per tennis court + 1 space per 5 spectator places	
swimming pool (if not associated with a single dwelling)	1 space per 20m <sup>2</sup> of pool + 1 space per 5 spectator places	
marina	2 spaces for 3 berths	
race course, firing range and other outdoor recreation	No requirements	No requirements
Sportsground, showground	1 space per 5 spectator places and a drop-off and pickup area	1 space per 50 spectator places
<b>Storage</b> (vehicle/caravan/boat store, cold store, wood yard / fuel depot, warehouse)	1 space per 200m <sup>2</sup> of the site or 1 space per 2 employees; whichever is greater	No requirement
<b>Tourist operation</b> (winery, theme/wildlife park, zoo)	1 space per 200 m <sup>2</sup> gross floor area or 1 space per 500m <sup>2</sup> of the site; whichever is greater	1 space per 1000m <sup>2</sup> gross floor area or 1 space per 2500m <sup>2</sup> of the site
<b>Transport depot and distribution</b>	10% of the site to be set aside for car spaces and access strips (excluding driveways)	1 space per 5 employees
<b>Utilities</b>	No requirement set	No requirement set

Exhibited

<b>Vehicle fuel sales and servicing</b>	4 spaces per service bay	1 space per 5 employees
<b>Visitor accommodation</b> (bed and breakfast, camping, caravan park, unit/cabin, backpacker hostel, motel, serviced apartments)	1 space per unit or 1 space per 4 beds whichever is greater	1 space per 10 beds

**Notes:**

1. The number of parking spaces required is to be calculated on the basis of the area of each new or expanded use on the application site and/or the additional number of people capable<sup>4</sup> of using the site, where indicated.
2. Each space is to be individually accessible (not jockey or tandem), except for single dwellings and multiple dwellings where spaces are tandem for individual units only.
3. Fractions of a space are to be rounded to the nearest whole number, so that a full number of spaces is provided for any fraction of a quota of floor area or number of employees.
4. Where a proposal contains multiple use classes, the car parking requirements must be calculated as the sum of the requirements for each individual use component.

**Table E6.2: Access Widths for Vehicles**

Number of parking spaces served	Access width (see note 1)	Passing bay (2.0m wide by 5.0m long plus entry and exit tapers) (see note 2)
1 to 5	3.0m	Every 30m
6 to 20	4.5m* for initial 7m from road carriageway and 3.0m thereafter	Every 30m
21 and over	5.5m	Not applicable

**\*Note 1**

1. Carriageways must have an internal radius of at least 4.0 metres at changes of direction or intersections or be wider than 4.2 metres.

**Note 2**

1. Passing bay area is additional to the required carriageway width.
2. For one-way operation the minimum access width is 3 metres and there is no passing bay requirement.

Exhibited

<sup>4</sup>By statute where appropriate

**Table E6.3: Width of Access and Manoeuvring Space adjacent to Parking Spaces**

Angle of Car Spaces to Access Strip	Access Strips Widths	Car Park Widths	Car parking length
Parallel	3.6m	2.3m	6.7m
45 degrees	3.5m	2.6m	5.4m
60 degrees	4.9m	2.6m	5.4m
90 degrees	6.4m	2.6m	5.4m
	5.8m	2.8m	5.4m
	5.2m	3.0m	5.4m
	4.8m	3.2m	5.4m

**Notes:**

1. A building may project into a parking space provided it is at least 2.1 metres above the parking surface level.
2. If entry to the car space is from a road then the width of the access strips may include the road

**Table E6.4: Loading bays**

Floor area of the Building	Minimum Loading Bay Dimensions	
2600m <sup>2</sup> or less in a single occupation	Required Area	27.4m <sup>2</sup>
	Required Length	7.6m
	Required Width	3.6m
	Required Height Clearance	4.0m
For every additional 1,800m <sup>2</sup> or part thereof of building floor area	An additional 18m <sup>2</sup> of area	

**Table E6.5: Pedestrian Access**

Number of Parking Spaces Required	Pedestrian Facility
1-10	No separate access required (i.e. pedestrians may share the driveway). [Note (a) applies].

Exhibited



11 or more	A 1m wide footpath separated from the driveway and parking aisles except at crossing points. [Notes (a) and (b) apply].
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**Notes**

- a) In parking areas containing spaces allocated for disabled persons, a footpath having a minimum width of 1.5m and a gradient not exceeding 1 in 14 is required from those spaces to the principal building.
- b) Separation is deemed to be achieved by:
  - i) a horizontal distance of 2.5m between the edge of the driveway and the footpath; or
  - ii) protective devices such as bollards, guard rails or planters between the driveway and the footpath; and
  - iii) signs and line marking at points where pedestrians are intended to cross driveways or parking aisles.

**Table E6.6 - Parking Precinct Plans**

Not used in this Scheme

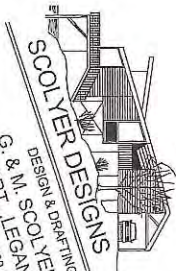
Exhibited

1-725

# MULTIPLE DWELLINGS FOR S. CHUGG 15 LAYCOCK STREET LONGFORD, TAS. 7301

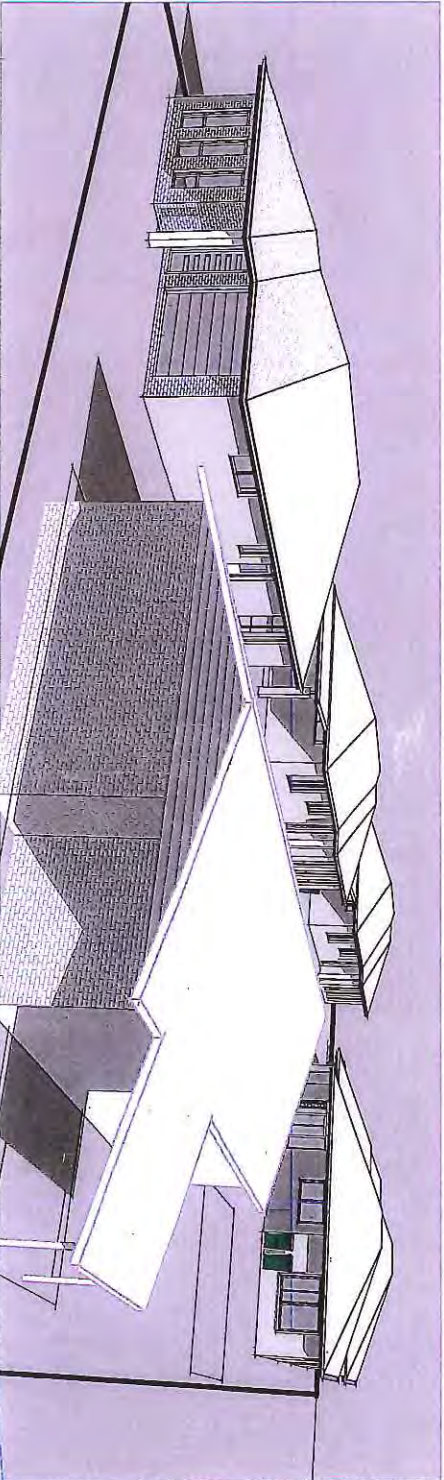
Sheet Number	Sheet Name
A00	COVER SHEET
A01	SITE PLAN - EXISTING
A02	SITE PLAN - PROPOSED
A03	LANDSCAPE PLAN
A04	AREA PLAN
A05	DRAINAGE PLAN
A06	VEHICLE TURNING PATHS - 1
A07	VEHICLE TURNING PATHS - 2
A08	VILLA 2 FLOOR PLAN
A09	VILLA 2 ELEVATIONS
A10	VILLAS 3/4 FLOOR PLAN
A11	VILLAS 3/4 ELEVATIONS
A12	VILLA 5 FLOOR PLAN
A13	VILLA 5 ELEVATIONS

Land Title Reference: SP13992/4  
 PID: 6743315  
 Soil Classification: TBA  
 Wind Classification: TBA  
 Climate Zone: 7  
 Planning Scheme Zone: 10.0 General Residential  
 BALL Level: N/A



**SCOLYER DESIGNS**  
 DESIGN & DRAFTING  
 G. & M. SCOLYER  
 1 AVRAHAM COURT, L1 EGNANA  
 Phone: (08) 6530 8144  
 Mobile: (08) 408 550 8144  
 Email: info@scolyer.com.au  
 scolyer@scolyer.com.au  
 Fax: 43 578 695 595  
 CC Ref: 563

**AMENDED**



DEVELOPMENT APPLICATION

NOTE: COMPUTER IMPRESSION ONLY

Exhibited

P1

AUGUST 2017



DEVELOPMENT APPLICATION

Site Plan - Existing  
SCALE 1:250

Exhibited

- GENERAL NOTES:**
- CHECK & VERIFY ALL DIMENSIONS & LEVELS ON SITE
  - WRITTEN DIMENSIONS TO TAKE PREFERENCE OVER SCALED, DO NOT SCALE
  - ALL WORK TO BE STRICTLY IN ACCORDANCE WITH THE B.C.A., ALL S.A.A. CODES & LOCAL AUTHORITY BY-LAWS.
  - ALL DIMENSIONS INDICATED ARE FRAME TO FRAME AND DO NOT ALLOW FOR WALL LININGS.
  - ALL PLUMBING WORKS TO BE STRICTLY IN ACCORDANCE WITH A.S. 3500 & APPROVED BY COUNCIL INSPECTOR.
  - BUILDER/PLUMBER TO ENSURE ADEQUATE FALL TO SITE CONNECTION POINTS IN ACCORDANCE WITH A.S. 3500 FOR STORMWATER AND SEWERS BEFORE CONSTRUCTION COMMENCES.
  - ALL WINDOWS AND GLAZING TO COMPLY WITH AS 1289
  - CHECK ON SITE FOR ALL EXISTING UNDERGROUND SERVICES PRIOR TO COMMENCEMENT OF WORKS, TAKE ALL NECESSARY PRECAUTIONS & RE-CONNECT WHERE REQUIRED.
  - ALL GROUNDWORK, FITMENTS, APPLICATIONS, PLUMBING FITTINGS & MATERIALS SHALL BE AS SCHEDULED BY OWNER.
  - ALL SURFACE DRAINAGE - FINISHED GROUND TO FALL AWAY FROM BUILDING TO A MINIMUM DISTANCE OF 1000mm AT 1:20 AND TO A POINT WHERE FLOODING WILL NOT OCCUR.
  - FINISHED FLOOR LEVELS TO BE A MINIMUM 150mm ABOVE FINISHED GROUND LEVEL.
  - STORMWATER TO BE PAVED SURFACES
  - FINISHING OF WATER UNDER SUSPENDED FLOORS

AMENDED



**SCOLLER DESIGNS**  
 residential architecture  
**G. & M. SCOLLER**  
 7 AVRAMAM CRT. 1, LILGANA  
 phone 08 930 1 915  
 mobile 0408 550 814  
 gscoller@scollerdesigns.com.au  
 GSTIN: 48 578 629 926  
 ABRN: 48 578 629 926  
 CC Ref: 563

Rev No	Description	Date
B	AMENDMENTS	21.08.17
A	DEVELOPMENT APPLICATION	09.08.17

Client: S. CHUGG

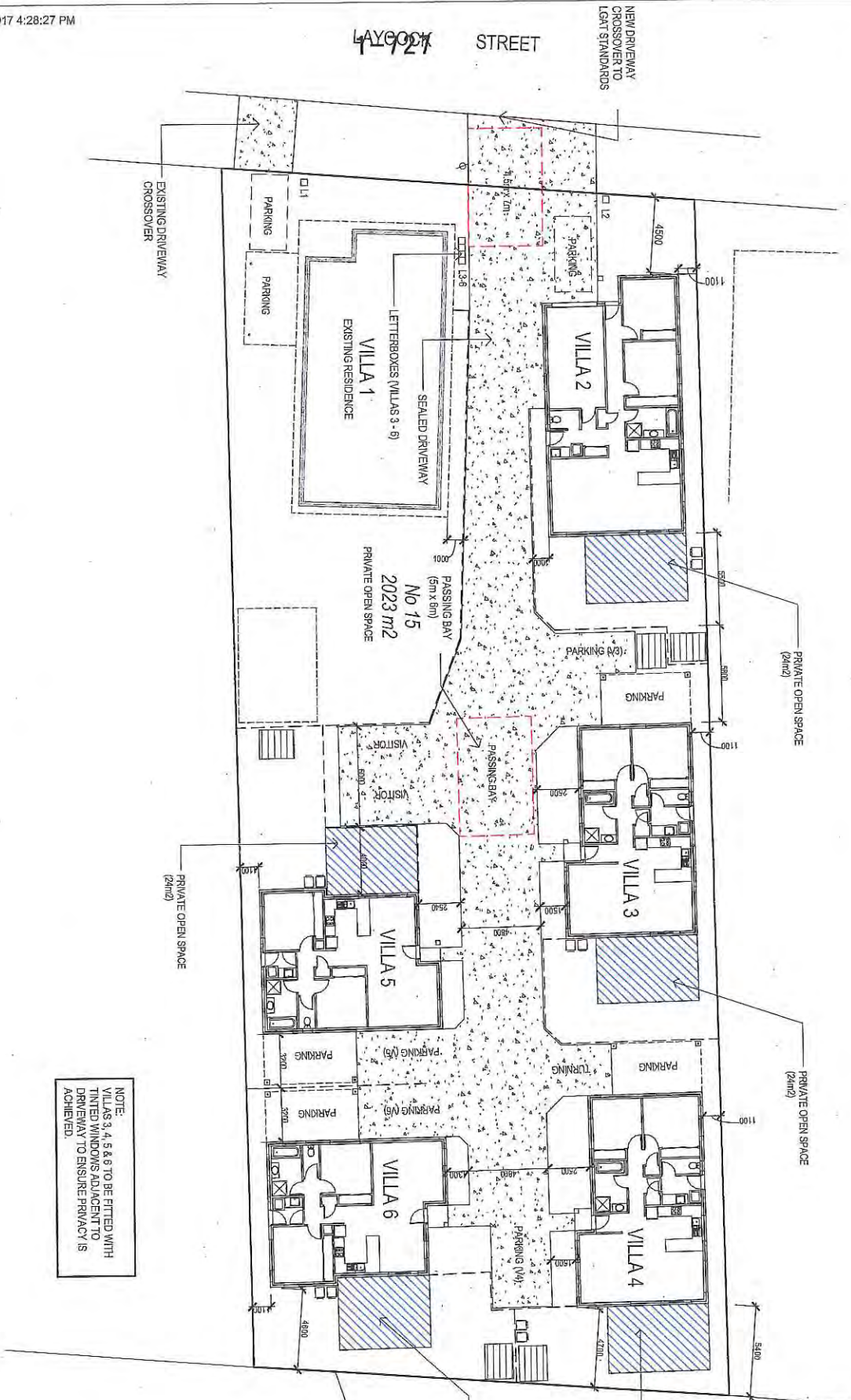
Project:  
 MULTIPLE DWELLINGS  
 15 LAYCOCK STREET  
 LONGFORD, TAS, 7301

Title:		Rev. No:	
SITE PLAN - EXISTING		B	
Date:	Project No:	Date:	Project No:
AUG. '17			
Scale:	Sheet:	Scale:	Sheet:
As Indicated	2 OF 16	As Indicated	2 OF 16
		No: A01	



# DEVELOPMENT APPLICATION

Site Plan - Proposed  
SCALE 1:250



NOTE:  
VILLAS 3, 4, 5 & 6 TO BE FITTED WITH  
TINTED WINDOWS ADJACENT TO  
DRIVEWAY TO ENSURE PRIVACY IS  
ACHIEVED.

Amended

IMPERVIOUS SURFACE COMPLIANCE  
LOT AREA = 2023m<sup>2</sup>  
VILLA 1 AREA = 164m<sup>2</sup>  
VILLA 2 AREA = 109m<sup>2</sup>  
VILLAS 3-4 AREA (105x2) = 210m<sup>2</sup>  
VILLAS 5-6 AREA = (107x2) = 222m<sup>2</sup>  
TOTAL = 608 m<sup>2</sup>  
DRIVEWAY = 48m<sup>2</sup> P.O.S. = 120m<sup>2</sup>  
TOTAL = 1304m<sup>2</sup>  
2023 - 1304 = 719m<sup>2</sup>  
WHICH IS 35.5% AREA FREE FROM  
IMPERVIOUS SURFACE

AMENDED



**SCOLLYER DESIGNS**  
DESIGN & DRAFTING  
G & M. S. SCOLLYER  
7 ANVRAM CRT. T. 1. EGGAN  
mobile 0408 530 1155  
p 0408 530 1154  
gscollyer@scollyerdesigns.com.au  
ABN: 45 578 639 995  
CC Tel 595

Rev. No.	Description	Date
A	DEVELOPMENT APPLICATION	09/08/17
B	AMENDMENTS	21/08/17

Client: **S. CHUGG**  
Project: MULTIPLE DWELLINGS  
15 LAYCOCK STREET  
LONGFORD, TAS. 7301

Title: <b>SITE PLAN - PROPOSED</b>	
Date: <b>AUG. '17</b>	Project No: <b>B</b>
Scale: <b>1:250</b>	Sheet: <b>3 OF 16</b>
No: <b>A02</b>	



1-728 LAYCOCK STREET

# DEVELOPMENT APPLICATION

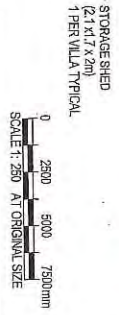
Landscape Plan  
SCALE 1:250



Exhibited

PLANTING NOTES:  
 P - DRIVETTES SELECTED SPECIES  
 150mm MAIN TOPSOIL, PINE BARK MULCH TO GARDEN AREAS  
 150mm MAIN TOPSOIL, GRASS & FERTILISE TO LAWN AREAS  
 SELECTED PEBBLES / GRAVEL WHERE SHOWN

AMENDED



**SCOLLER DESIGNS**  
 REGISTERED ARCHITECTS  
**G & M SCOLLER**  
 7 AVRAM CIRCLE  
 GARDENS  
 PHONE (08) 8530 1455  
 MOBILE (08) 8530 8114  
 EMAIL gscollerv@scollers.com.au  
 AUST. REG. NO. 578 638 695  
 CC REG. 593

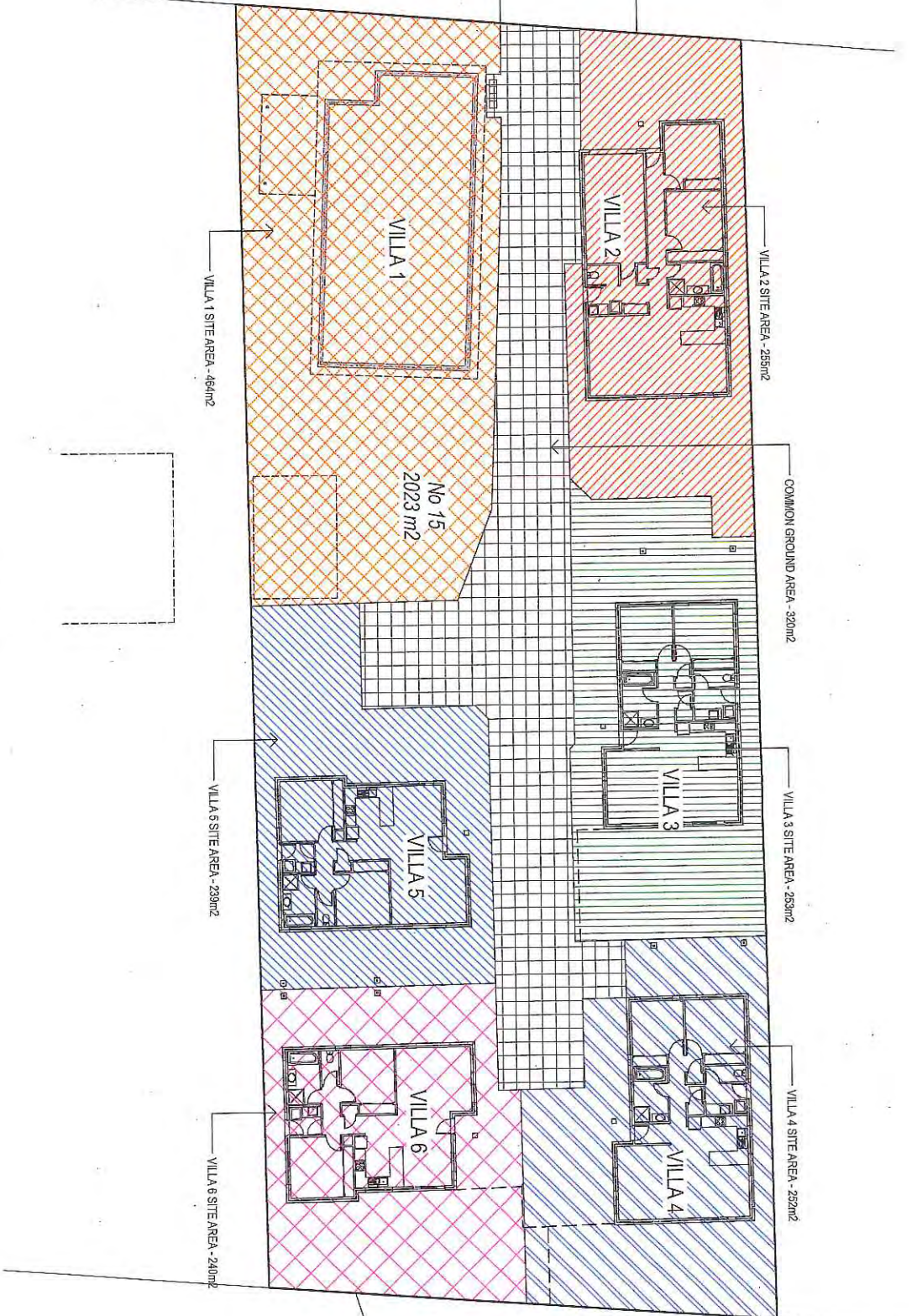
Client:	S. CHUGG
Project:	MULTIPLE DWELLINGS 15 LAYCOCK STREET LONGFORD, TAS. 7301
Title:	LANDSCAPE PLAN
Date:	AUG. '17
Scale:	1:250
Sheet:	4 OF 16
Rev. No.:	B
Project No.:	
Rev. No.:	
Sheet:	A03



1-729 COCK STREET

Plot Date: 21/08/2017 4:28:29 PM

# DEVELOPMENT APPLICATION



Area Plan  
SCALE 1 : 250

Exhibited

AMENDED

0 2500 5000 7500mm  
SCALE 1:250 AT ORIGINAL SIZE

**SCOLLYER DESIGNS**  
DESIGN & DEVELOPING  
**G. & M. S. COLLIER**  
DESIGNERS  
7 AVRAHAM COURT, LEGANA  
PHONE (08) 9530 1485  
PHONE 0408 550 814  
geo@scollyer.com.au  
scollyer@scollyer.com.au  
PABX: 48 578 539 535  
CC Ref: 553

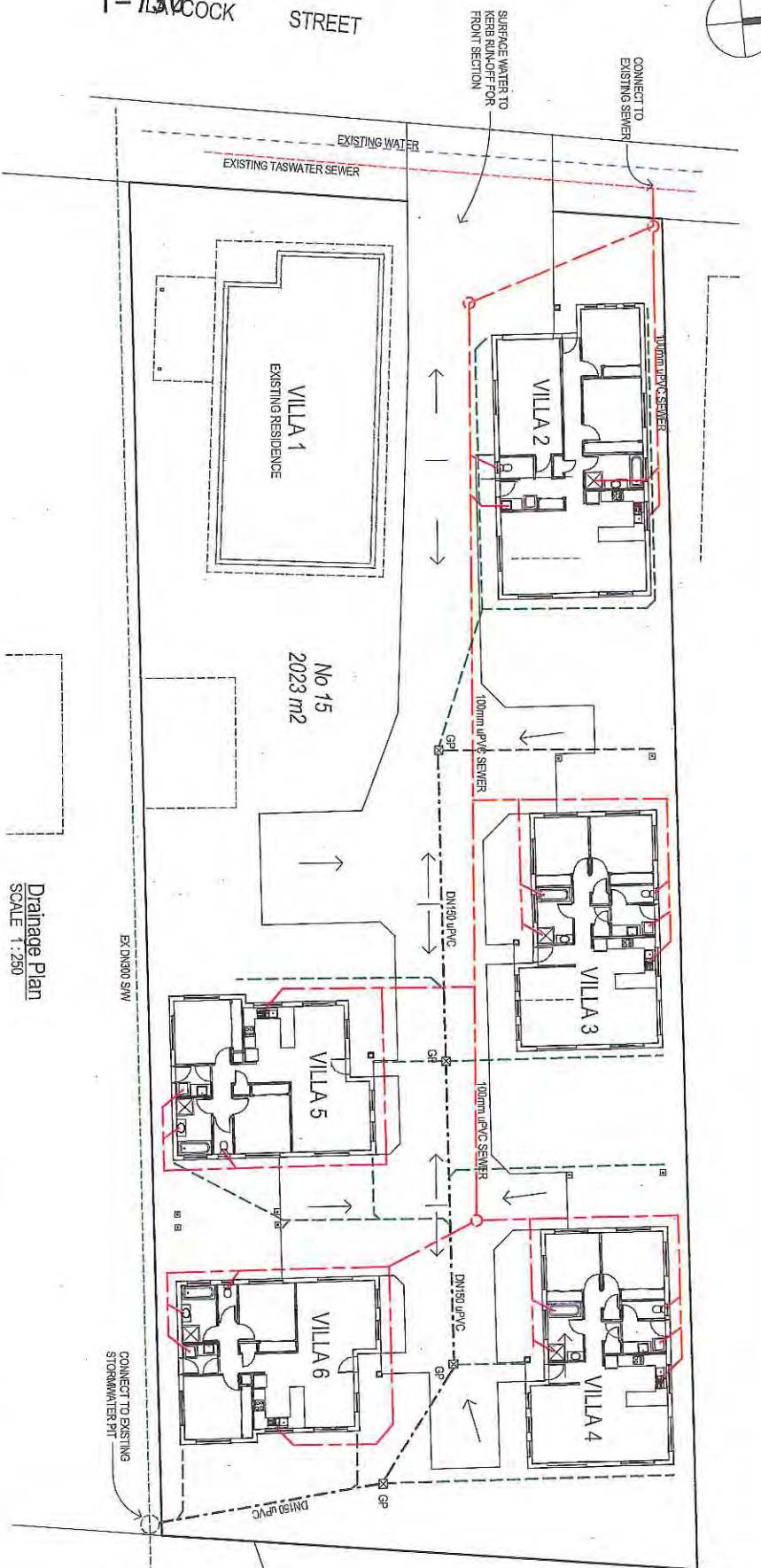
Rev No.	Description	Date
B	AMENDMENTS	21.08.17
A	DEVELOPMENT APPLICATION	09.08.17

Client: **S. CHUGG**  
Project: **MULTIPLE DWELLINGS  
15 LAYCOCK STREET  
LONGFORD, TAS. 7301**

Title: AREA PLAN		Rev No: B	
Date: AUG. 17	Project No:	Date:	Rev No:
Scale: 1 : 250	Sheet: 5 OF 16	No: A04	



1-730 COCK STREET



Drainage Plan  
SCALE 1:250

**PLUMBING NOTES:**  
 - ALL DRAINAGE AND RIG SOUNDS IS PROVISIONAL ONLY AND IS SUBJECT TO ANY CHANGE IN THE REQUIREMENTS OF THE LOCAL AUTHORITIES.  
 - ALL DRAINAGE TO BE COMPLY WITH THE REQUIREMENTS OF AS/NZS 3500 PARTS 1, 2, 3 & 4 & THE TISSIMAN PLUMBING CODE AND MUST BE CARRIED OUT BY A LICENSED TRADESMAN.  
 - BUILDER TO ENSURE ADEQUATE FALL TO SITE CONNECTION POINTS IN ACCORDANCE WITH AS 3500 FOR STORMWATER AND SEWER BEFORE CONSTRUCTION COMMENCES.  
 - ALL RUNOFF FROM PAVED & DRIVEWAY AREAS TO BE RETAINED WITHIN THE SITE BOUNDARIES AND DRAINED TO AN APPROVED STORMWATER SYSTEM.

**LEGEND**

- 1. TROUGH (TR)
- 2. SINK (S)
- 3. BATH (BT)
- 4. BASIN (B)
- 5. SHOWER (SH)
- 6. WC (WC)
- 7. DISHWASHER (DW)

- SEWER PIPE = 100mm DIA. UPVC @ 1:50 FALL
- OVERFLOW RELIEF GULLY (OR)
- 150mm WASTE GULLY (WG)
- INSPECTION OPENING (IO)
- MANHOLE (MH)
- GRAVED PIT - 300x300 (GP)
- DOWNPIPE (DP)
- STORMWATER = 90mm DIA. UPVC @ 1:100 FALL

**NOTE:**  
 DESIGN & CALCULATIONS FOR POTENTIAL ON-SITE STORMWATER DETENTION TO BE UNDERTAKEN DURING DETAILED DESIGN

**LEGEND**

- DN80 UPVC SW
- DN150 UPVC SW
- DN300 EXISTING SW
- DN100 UPVC SEWER



AMENDED

**SCOLLER DESIGNS**  
 DESIGN & DRAWING  
**G. & M. SCOLLER**  
 7 AVRAM CRT. 1, LEGANA  
 MOBILE 0465 550 814  
 PHONE 0465 550 814  
 GSCOLLER@ASAPDESIGN.COM.AU  
 A.B.N. 45 578 633 563  
 C.C. TEL 593

Rev No.	Description	Date
B	AMENDMENTS	21.08.17
A	DEVELOPMENT APPLICATION	03.08.17

Client: **S. CHVGG**  
 Project: **MULTIPLE DWELLINGS 15 LAYCOCK STREET LONGFORD, TAS. 7301**

Date	Project No.	Rev. No.
AUG. 17		B

Title: **DRAINAGE PLAN**

Scale	Sheet	No.
As Indicated	6 OF 16	A05

DEVELOPMENT APPLICATION



1-731

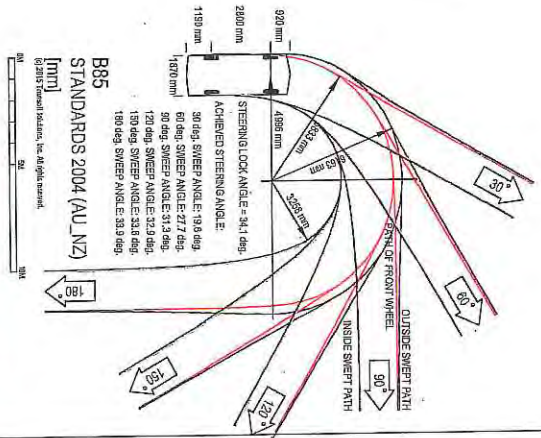
LAYCOCK STREET


DEVELOPMENT APPLICATION



Site Plan - Proposed  
SCALE 1:250

Exhibited





**SCOLYER DESIGNS**  
D Designer & Drafting  
**G. & M. SCOLYER**  
7 INGRAM CRT. T. LEEHANN  
phone 0405 650 814  
mobile 0405 650 814  
mcscolyer@scolyerdesigns.com.au  
ARN: 45 578 698 595  
CC Ref: 683

Rev. No.	Description	Date
B	AMENDMENTS	21.08.17
A	DEVELOPMENT APPLICATION	09.08.17

Client: **S. CHUGG**

Project: **MULTIPLE DWELLINGS  
15 LAYCOCK STREET  
LONGFORD, TAS. 7301**

Title: **VEHICLE TURNING PATHS (1)**

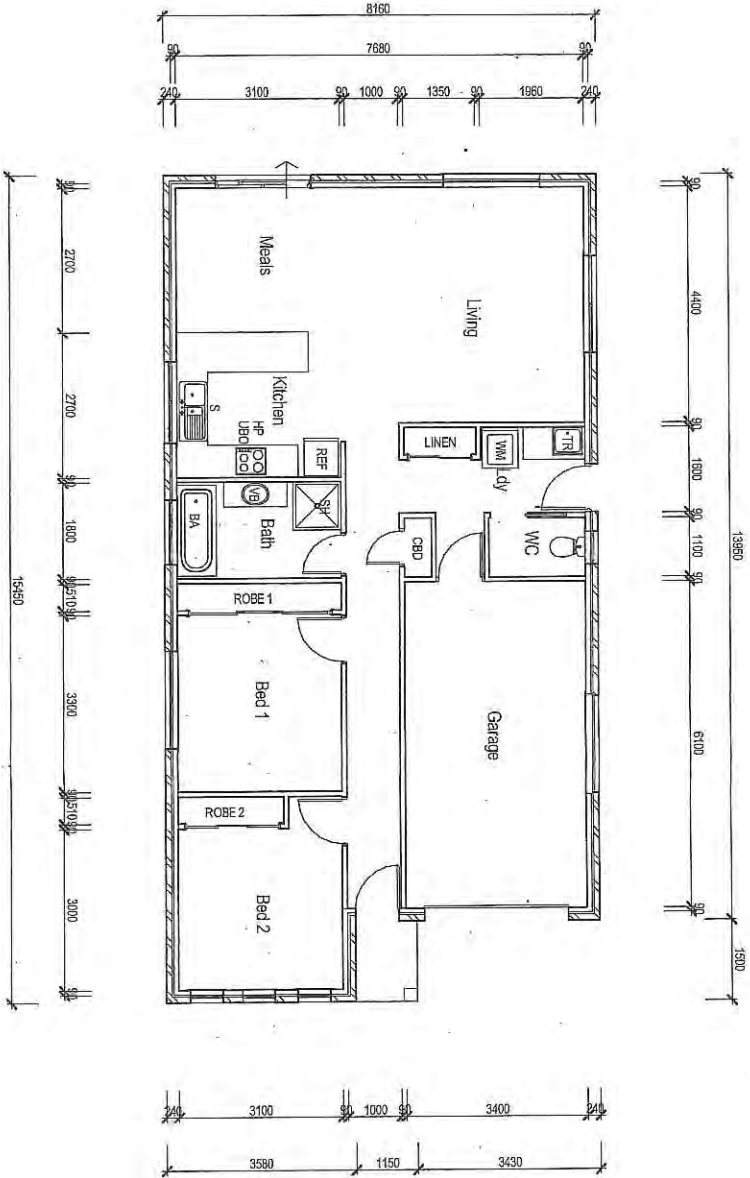
Date	Project No.	Rev. No.
AUG '17		B

Scale: 1:250 Sheet: 7 OF 16 No: **A06**





1-733



DEVELOPMENT APPLICATION

Area Schedule (Gross Building)		
Name	Area	Area (sq)
	119,20 m <sup>2</sup>	12,83
Area		12,83



VILLA 2

**SCOLYER DESIGNS**  
design & drafters  
**G. & M. SCOLYER**  
7 LAVRAM CRT., TIEGANA  
VIC 3089  
phone (03) 9330 7195  
mobile 048 550 8141  
fronville 048 550 8141  
gscolyer@scolyerdesign.com.au  
ABN: 46 516 638 505  
CO Reg 1565

**AMENDED**

0 1000 2000 3000mm  
SCALE 1: 100 AT ORIGINAL SIZE

Rev No.	Description	Date
B	AMENDMENTS	21.08.17
A	DEVELOPMENT APPLICATION	09.08.17

Client: **S. CHUGG**

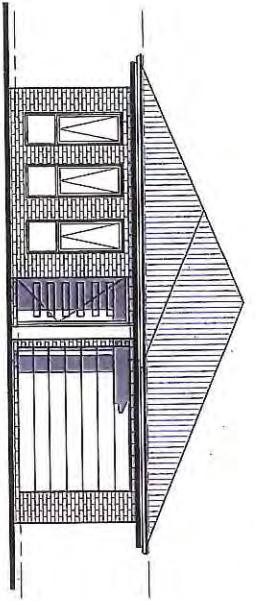
Project: **MULTIPLE DWELLINGS  
15 LAYCOCK ST  
LONGFORD, TAS. 7301**

Date:	Project No.:	Rev No.:
AUG. '17		B

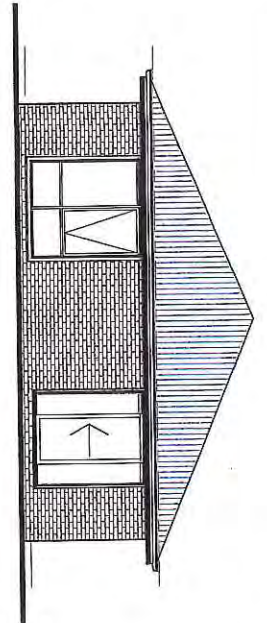
Title: **FLOOR PLAN**

Scale:	Sheet:	No.:
1 : 100	9 OF 16	<b>A08</b>

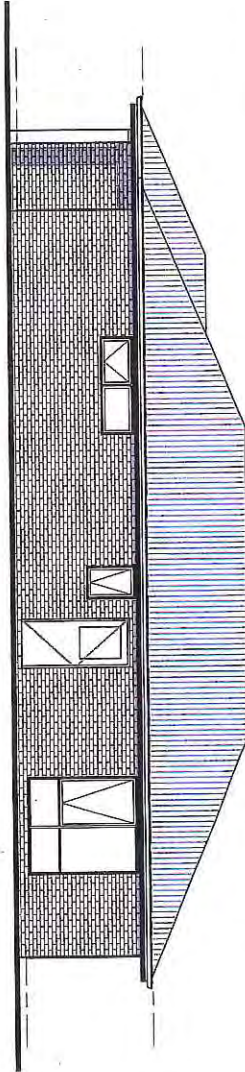
DEVELOPMENT APPLICATION



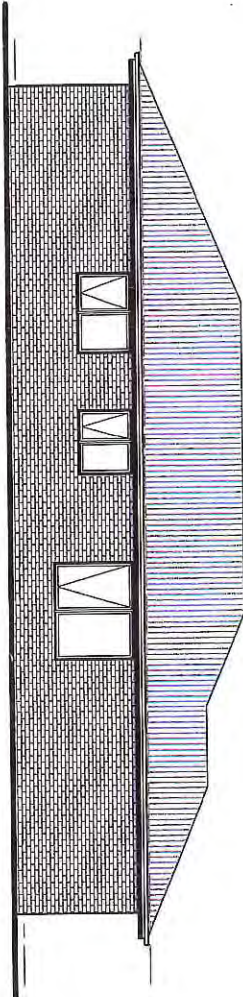
West Elevation  
SCALE 1:100



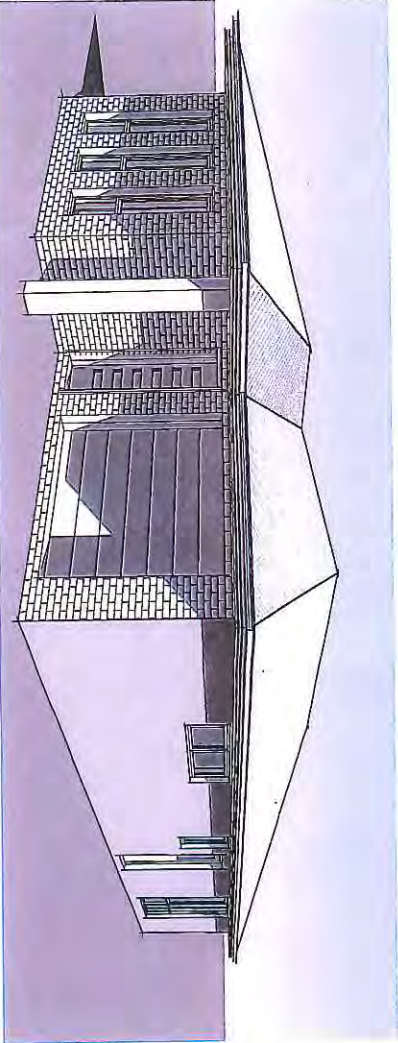
East Elevation  
SCALE 1:100



South Elevation  
SCALE 1:100



North Elevation  
SCALE 1:100



WINDOWS  
PROPOSED ALUMINIUM WINDOW FRAMES TO SELECTED COLOUR  
AS PER ARCHITECT'S SPECIFICATIONS  
MAP BEVELS AND TRIMS  
BRICK ON EDGE EXTERNAL SILLS  
DOUBLE GLAZED THROUGHOUT  
ALL FIXINGS AND FLASHINGS TO MANUFACTURERS' WRITTEN  
RECOMMENDATIONS  
GLAZING AS PER AS 1288 & BCA 36 & 312.2 FOR DETAILS  
VENTILATION TO BCA 3.1.5

ROOFING  
SELECTED FIBRO CLAY FACE BRICKS  
PAVED JOINTS, STRETCHER BOND  
ALL MORTAR COLOUR TO BE NATURAL GREY CEMENT SAND & LIME  
REFER ENGINEER DRAWINGS FOR LOCATION OF ANCHORAGE JOINTS  
ALL LABOUR TO COMPLY WITH PART 3.2 OF THE BCA

ROOF FRAMING  
ROOF ORG (L/D) BAY OR SIMILAR APPROVED SHEET ROOFING  
COLORBOND COLOUR TO BE SELECTION OVER R9 X31 R9 HD WALK  
BATTERS IN SLOPES AND APPROVED FLASHINGS PER MANUFACTURERS'  
RECOMMENDATIONS

FASCIAS  
COLORBOND PRESSED METAL FASCIA & GUTTER  
EITHER 600 OR 900mm WIDE  
INSTALL IN ACCORDANCE WITH THE MANUFACTURERS'  
INSTRUCTIONS. COLOUR TO BE SELECTED BY OWNER

EAVES & SOFFITS  
EITHER METAL OR 45mm OR 45mm ON PLANKS  
EITHER METAL OR 45mm ON PLANKS  
EAVES SHEETING  
LINE ALL SOFFITS WITH HARDWEX SHEETING

AMENDED 21/8/17



**SCOTLYER DESIGNS**  
Design & Drafting  
G. & M. SCOTLYER  
7 LAVANNA CRT., J. LEGANNA  
phone (08) 6390 1155  
phone (08) 6408 990 814  
mobile 0800 990 814  
gscotlyer@scotlyerdesigns.com.au  
RBN: 45 578 639 935  
CC Per 1983

Rev No.	Description	Date
B	AMENDMENTS	21.08.17
A	DEVELOPMENT APPLICATION	09.08.17

Client:  
**S. CHUGG**

Project:  
**MUL TIPLE DWELLINGS  
15 LAYCOCK ST  
LONGFORD, TAS. 7301**

Date:	Project No:	Rev No:
AUG. '17		B

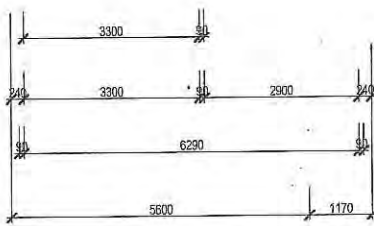
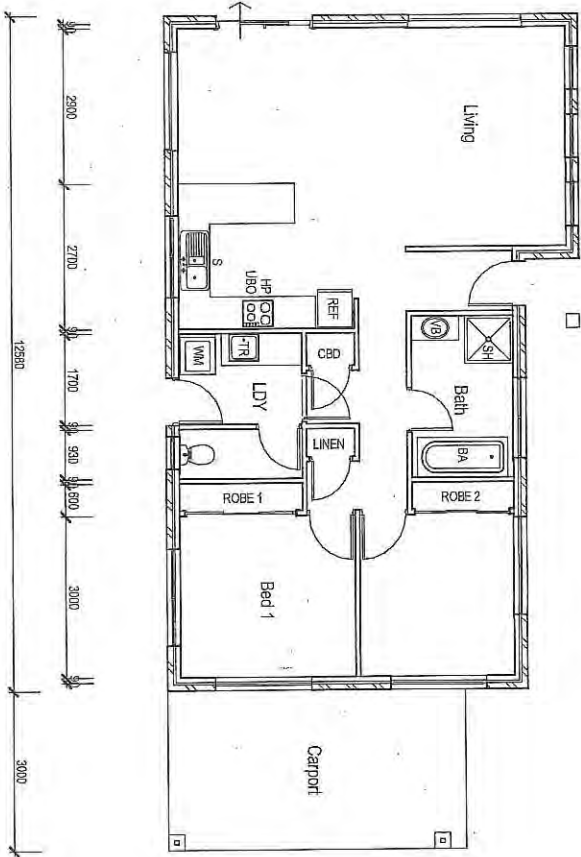
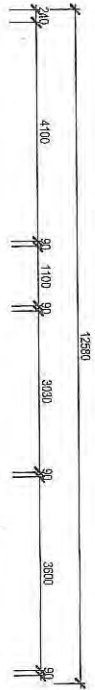
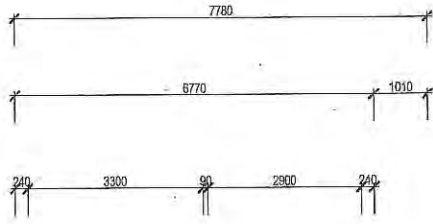
Scale: 1:100 Sheet: 10 OF 16 No: A09



VILLA 2

ELEVATIONS

1-735



Ground Floor Plan  
SCALE 1 : 100

Area Schedule (Gross Building)		
Name	Area	Area (sq)
Living	85.88 m <sup>2</sup>	9.24
Carport	16.80 m <sup>2</sup>	1.81
	102.68 m <sup>2</sup>	11.05

Revised

DEVELOPMENT APPLICATION

VILLAS 3/4



AMENDED

**SCOLYER DESIGNS**  
REGISTERED ARCHITECTS  
**G. & M. SCOLYER**  
1 AVRAHAM COURT, LEGANA  
PHONE (08) 908 659 5194  
MOBILE 0408 659 5194  
scolyer@scolyer.com.au  
ASNT 45 516 635 6368  
CC Ref: 563

Rev No.	Description	Date
B	AMENDMENTS	21.08.17
A	DEVELOPMENT APPLICATION	09.08.17

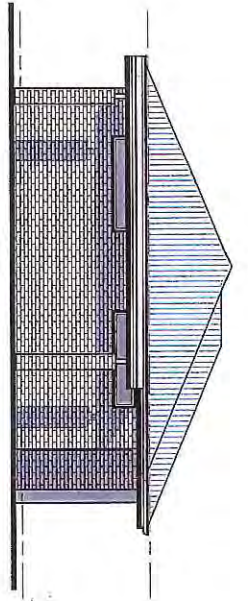
Client: S. CHUGG

Project: VILLA DEVELOPMENT  
15 LAYCOCK ST  
LONGFORD, TAS. 7301

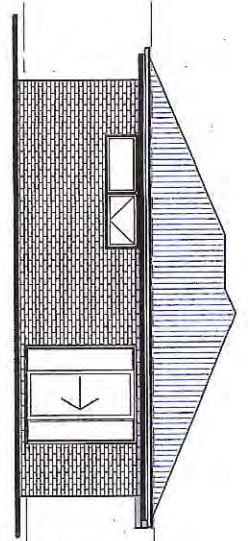
Date:	Project No:	Rev No:
AUG. '17		B

Title: FLOOR PLAN

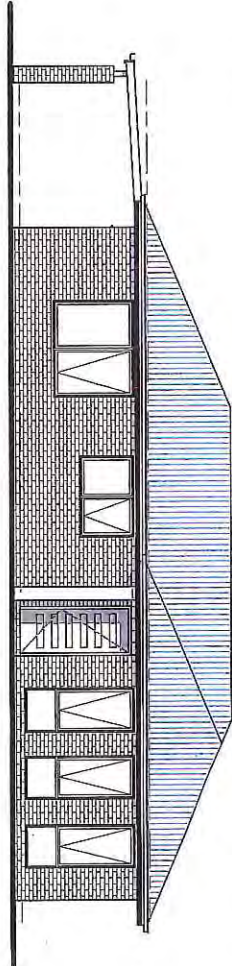
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1 : 100	11 OF 16	A10



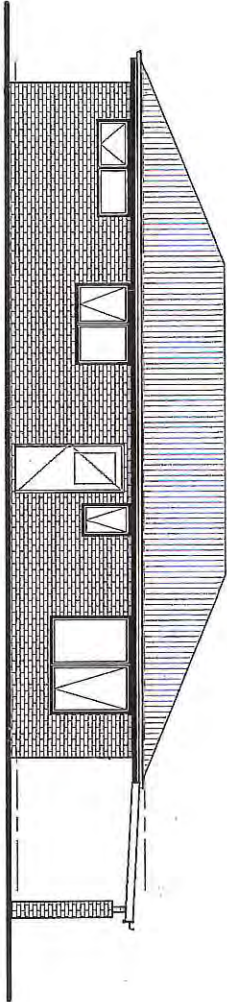
West Elevation  
SCALE 1:100



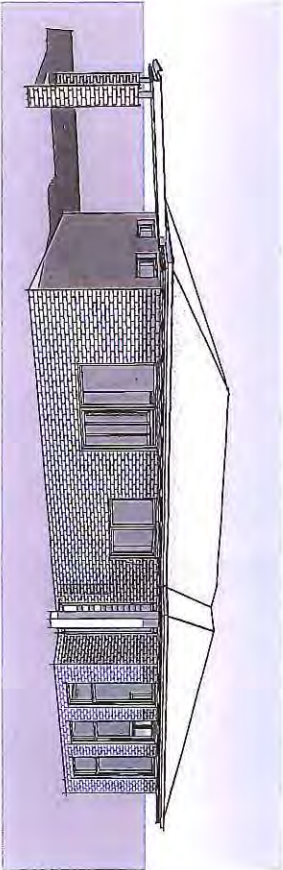
East Elevation  
SCALE 1:100



South Elevation  
SCALE 1:100



North Elevation  
SCALE 1:100



**WINDOWS**  
 POWDER COATED ALUMINIUM WINDOW FRAMES TO SELECTED COLOUR  
 FINISHES, INCLUDING ANODISED ALUMINIUM, BRASS, COPPER, STAINLESS  
 STEEL, BLACK, WHITE, GREY, BROWN, RED, BLUE, GREEN, YELLOW,  
 PURPLE, PINK, SILVER, GOLD, AND OTHERS.  
 JOINTS, STITCHER-BOND.  
 ALL WORK TO BE IN ACCORDANCE WITH THE MANUFACTURERS  
 INSTRUCTIONS. COLOUR TO BE SELECTED BY OWNER.  
**GLAZING**  
 DOUBLE GLAZED THROUGHOUT  
 ALL GLAZING TO BE SUPPLIED BY MANUFACTURERS WRITTEN  
 TO CONFIRM THE LOCATION OF ARTICULATION JOINTS.  
 GLAZING AS PER AS 288 & BCA 3.8 & 3.12 FOR DETAILS.  
 VENTILATION TO BCA 3.4.3

**ROOFING**  
 SELECTED RED CLAY FACE BRICKS.  
 RAKED JOINTS, STITCHER-BOND.  
 ALL WORK TO BE IN ACCORDANCE WITH THE MANUFACTURERS  
 INSTRUCTIONS. COLOUR TO BE SELECTED BY OWNER.  
**ROOF FRAMING**  
 CUSTOMISED (1/2 BATT) OR SIMILAR APPROVED SHEET ROOFING.  
 COLOUR AND COLOUR TO BE SELECTED BY OWNER.  
 BATTING TO BE APPROVED BY THE MANUFACTURERS.  
 ALL WORK TO BE IN ACCORDANCE WITH THE MANUFACTURERS  
 INSTRUCTIONS. COLOUR TO BE SELECTED BY OWNER.

**EAVES & GUTTERS**  
 EAVES TO BE 450mm OR AS NOTED ON PLANS.  
 FRAMES FOR LEAF GAVES AND LINER WITH HARDWARE  
 FINISHES TO BE SELECTED BY OWNER.  
 LINE ALL SOFFITS WITH HARDWARE SHEETING.

**AMENDED**

0 1000 2000 3000mm  
 SCALE 1:100 AT ORIGINAL SIZE

**SCOLYER DESIGNS**  
 DESIGN & DRAWING  
**G & M. SCOLYER**  
 1 LAVANNA CRT. LILGANA  
 phone (08) 639 1188  
 mobile 0406 559 814  
 gscolyer@scolyerdesigns.com.au  
 APT. 45 STB 639 808  
 CC Rd 583

Rev No.	Description	Date
B	AMENDMENTS	21.08.17
A	DEVELOPMENT APPLICATION	09.08.17

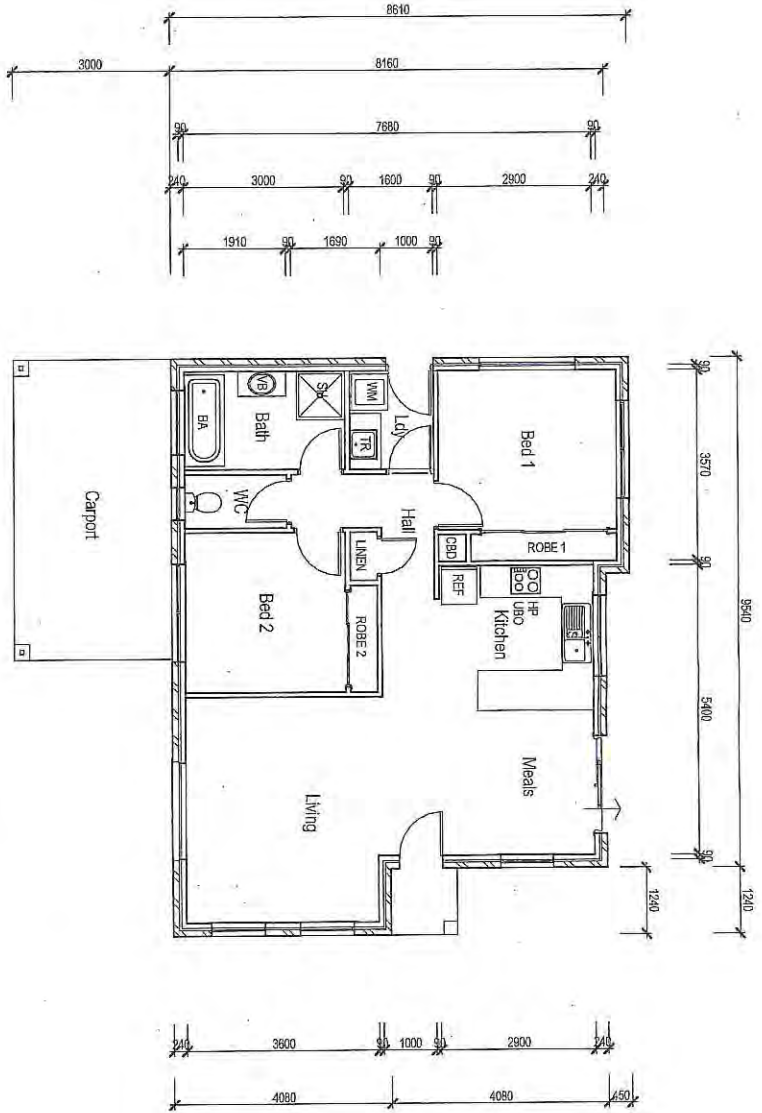
**Client:**  
 S. CHUUG  
**Project:**  
 VILLA DEVELOPMENT  
 15 LAYCOCK ST  
 LONGFORD, TAS. 7301

ELEVATIONS			
Date:	Project No:	Rev No:	Sheet No:
AUG. 17		B	
Scale:			
1:100	12 OF 16		A11



1-737

Plot Date: 21/08/2017 1:05:47 PM



Ground Floor Plan  
SCALE 1:100

Area Schedule (Gross Building)		
Name	Area	Area (sq)
Living	84.73 m <sup>2</sup>	9.12
Carport	16.80 m <sup>2</sup>	1.81
	101.53 m <sup>2</sup>	10.93



DEVELOPMENT APPLICATION

VILLA 5



AMENDED

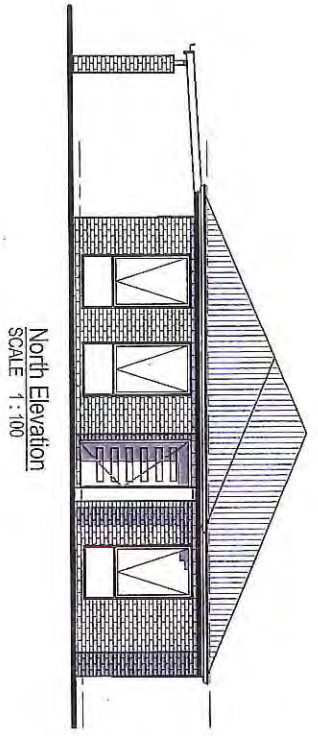
**SCOLTIER DESIGNS**  
 DESIGN & ARCHITECTURE  
**G. & M. SCOLTIER**  
 G. & M. I. EGAMA  
 7 LAURAM CIRCLE  
 PHONE (08) 8320 1185  
 MOBILE 0408 690 814  
 EMAIL gscoltier@scoltier.com.au  
 ABN: 45 673 639 205  
 CC Ref: 593

Rev. No.	Description	Date
B	AMENDMENTS	21.08.17
A	DEVELOPMENT APPLICATION	09.08.17

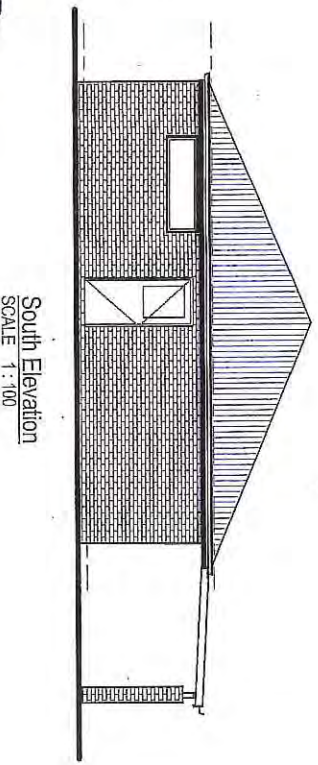
Client: **S. CHUGG**  
 Project: **VILLA DEVELOPMENT  
 15 LAYCOCK ST  
 LONGFORD, TAS, 7301**

Title: **FLOOR PLAN**

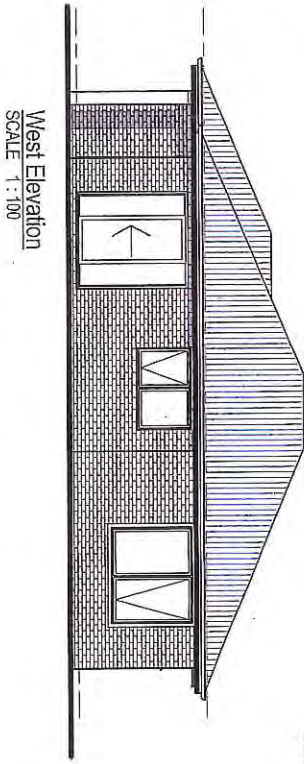
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Scale: 1:100	Sheet: 13 OF 16	No: A12



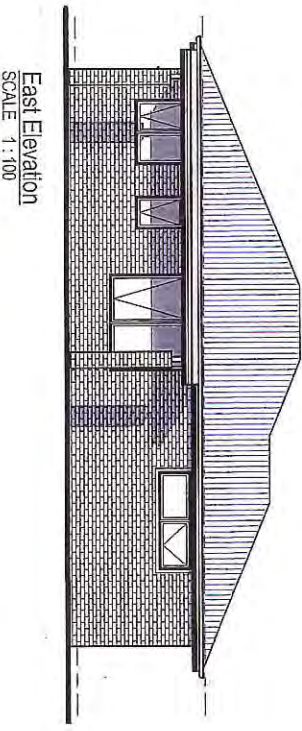
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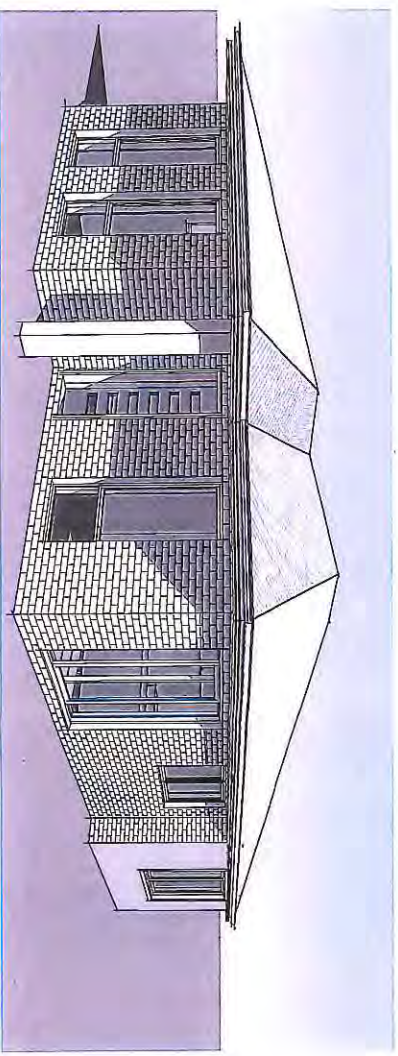
South Elevation  
SCALE 1 : 100



West Elevation  
SCALE 1 : 100



East Elevation  
SCALE 1 : 100



DEVELOPMENT APPLICATION

**WINDOWS**  
POWERCOATED ALUMINIUM WINDOW FRAMES TO SELECTED COLOUR  
WITH GLAZING PATTERNS.  
MSP REPELS AND TRIMS.  
BRICK ON EDGE EXTERNAL SILLS.  
DOUBLE GLAZED THROUGHOUT  
ALL FRAMES AND FIXINGS TO MANUFACTURERS WRITTEN  
SPECIFICATIONS.  
GLAZING AS PER AS 1288 & BCA 3.12.2 FOR DETAILS.  
VENTILATION TO BCA 3.15

**ROOFING**  
SELECTED FIRE RATED CLAY FACE BRICKS  
RANGED JOINTS, STRETCHER BOND.  
ALL WORKING COLOUR TO BE NATURAL GREY CEMENT SAND & LIME.  
REFER ENGINEER DRAWINGS FOR LOCATION OF ARTICULATION JOINTS.  
ALL MASONRY TO COMPLY WITH PART 3.3 OF THE BCA.

**ROOF FRAMING**  
CUSTOM ORG @ 42 BHU OR SIMILAR APPROVED SHEET ROOFING.  
COLORBOND COLOUR TO SELECTED OVERYS & BRIBBAND NAILING  
PATTERN IN 900 GMS AND APPROVED REFERENCED ROOF TRUSSES,  
RECOMMENDATIONS.

**FASCIA**  
COLORBOND PRESERVED METAL FASCIA & GUTTER  
INSTALLATION TO COMPLY WITH THE MANUFACTURERS  
INSTRUCTIONS COLOUR TO BE SELECTED BY OWNER

**EAVES & SOFFITS**  
OVERHANG ROOF 450MM OR AS NOTED ON PLANS.  
EAVES SHEETING TO BE 1100MM WIDE WITH 1100MM LxLxL.  
LINE ALL SOFFITS WITH HARDIPI EX SHEETING.

AMENDED



**SCOLLER DESIGNS**  
DESIGN & DRAWINGS  
G. & M. SCOLLER  
1 ANRANMAN CRT. 1 LEGANA  
phone (08) 6330 1185  
mobile 0406 6590 814  
mobile 0406 6590 814  
gscollerv@scollerdesigns.com.au  
ASBX: 46 578 639 636  
CC Ptd 535

Rev No.	Description	Date
B	AMENDMENTS	21.08.17
A	DEVELOPMENT APPLICATION	05.08.17

Client: **S. CHUGG**  
Project: **VILLA DEVELOPMENT  
15 LAYCOCK ST  
LONGFORD, TAS. 7301**

Date	Project No.	Rev No.
AUG '17		B

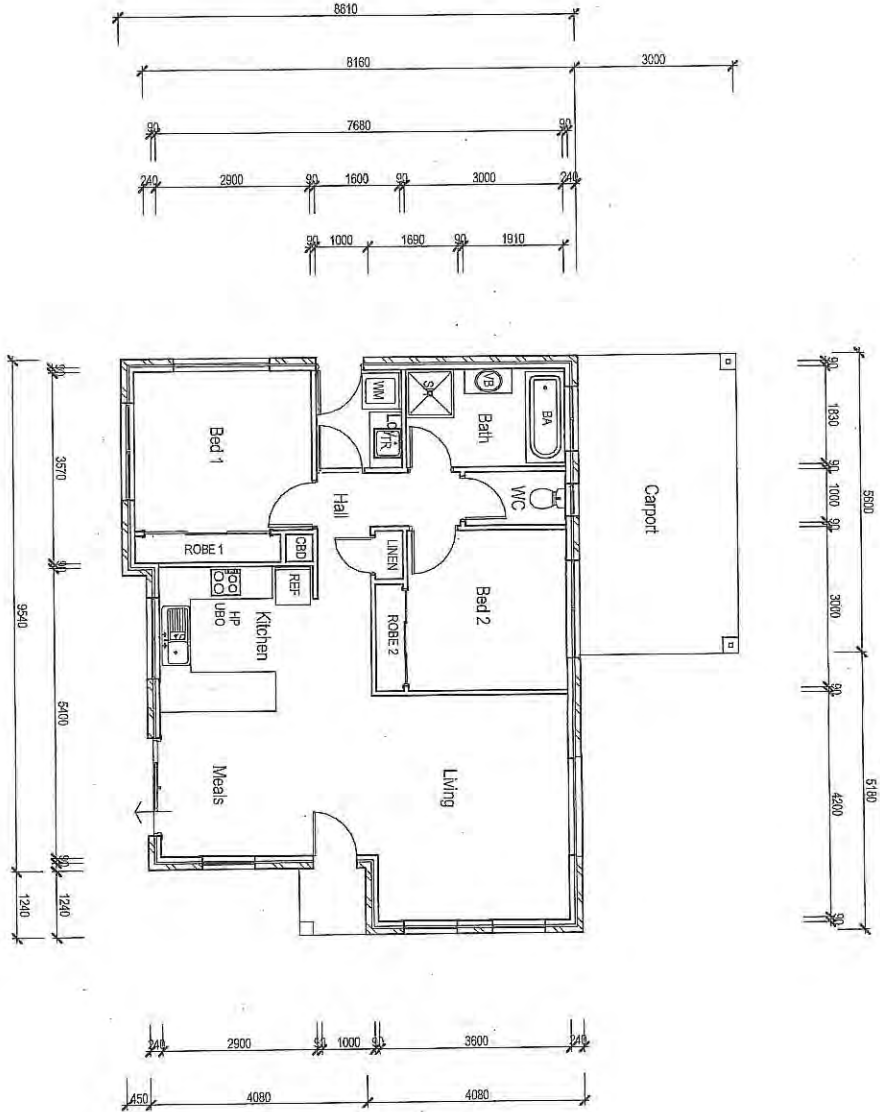
Title: **ELEVATIONS**

Scale	Sheet	No.
1 : 100	14 OF 16	A13

VILLA 5



DEVELOPMENT APPLICATION



Area Schedule (Gross Building)		
Name	Area	Area (sq)
Living	81.89 m <sup>2</sup>	8.79
Carport	16.80 m <sup>2</sup>	1.81
	98.49 m <sup>2</sup>	10.60

VILLA 6



**AMENDED**

**SCOLYER DESIGNS**  
design & drafting  
**G. & M. SCOLYER**  
7 AIRAVAM COURT - 11, LEGANA  
LONGFORD, TAS. 7301  
phone: (01) 6330 1198  
mobile: 0410 559 814  
m.scolyer@scolyerdesigns.com.au  
g.scolyer@scolyerdesigns.com.au  
FAX: 48 578 639 856  
CC Fax: 553

Date:	21/08/17
Rev No.:	A
Description:	DEVELOPMENT APPLICATION
Date:	09/08/17

**Client:**  
S. CHUUG

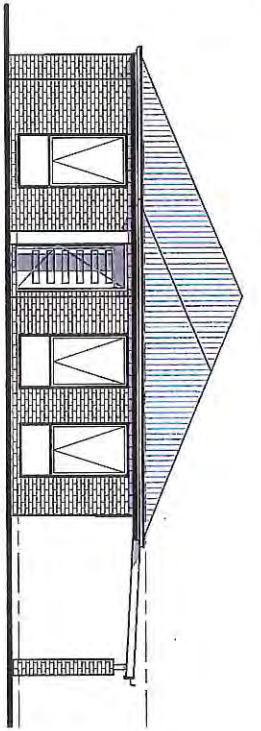
**Project:**  
VILLA DEVELOPMENT  
15 LAYCOCK ST  
LONGFORD, TAS. 7301

**Title:**  
FLOOR PLAN

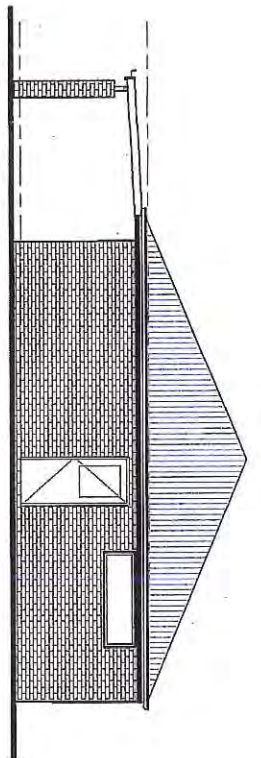
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AUG. '17		B
Sheet:	Sheet:	No.:
1 : 100	15 OF 16	A14



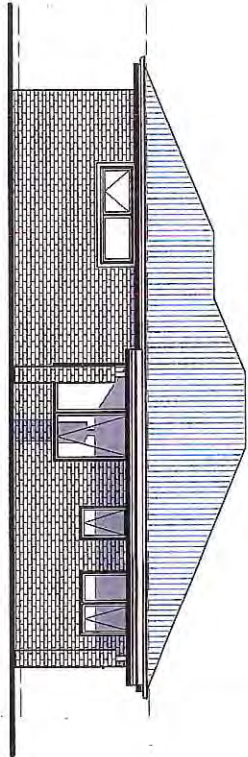




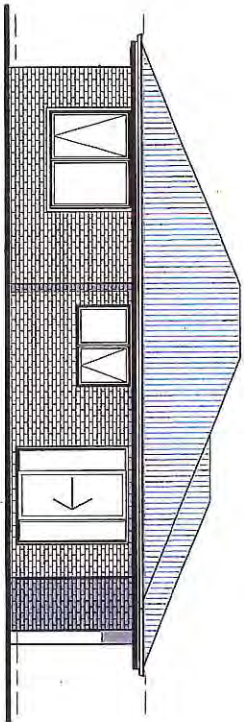
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SCALE 1 : 100



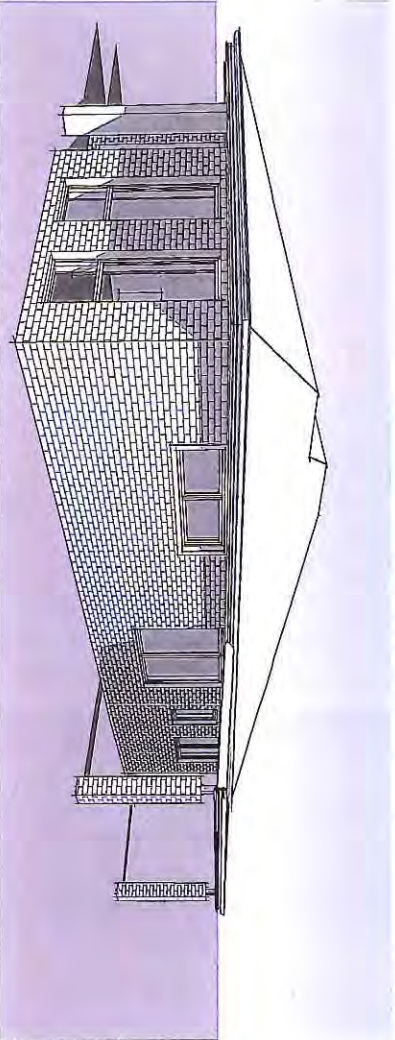
South Elevation  
SCALE 1 : 100



West Elevation  
SCALE 1 : 100



East Elevation  
SCALE 1 : 100



DEVELOPMENT APPLICATION

**WINDOWS**  
 POWDERCOATED ALUMINIUM WINDOW FRAMES TO SELECTED COLOUR  
 SLIDING SASHES, LOCKS & SCREENS.  
 BRICKWORK TO MATCH EXISTING BUILDING.  
 BRICKWORK TO MATCH EXISTING BUILDING.  
 BRICKWORK TO MATCH EXISTING BUILDING.  
 DOUBLE GLAZED THROUGHOUT  
 ALL FININGS AND FLASHING TO MANUFACTURERS WRITTEN  
 RECOMMENDATIONS.  
 ALL MANUFACTURERS TO BE APPROVED BY THE MANUFACTURERS  
 VERIFICATION TO BSA 332

**ROOF FINISHES**  
 SELECTED FROM ONE OF THE ABOVE.  
 ALL WORKMANSHIP TO BE IN ACCORDANCE WITH THE MANUFACTURERS  
 REFER ENGINEER PERMISSIONS FOR LOCATION OF ARTICULATION JOINTS.  
 ALL MASONRY TO COMPLY WITH PART 33 OF THE BCA.

**ROOF FRAMING**  
 CONSTRUCTION OF 42 RIB OR SIMILAR APPROVED SHEET ROOFING.  
 COLOURING COLOUR TO BE SELECTED OVER 75 x 38 RIB HANGING  
 BATTENS AT 900 CRS AND APPROVED PRESHEATED ROOF FUSERS,  
 INSTALLED STRICTLY IN ACCORDANCE WITH THE MANUFACTURERS  
 RECOMMENDATIONS.

**FLASHING**  
 COLOURING PRESHEATED METAL FLASHING & GUTTER  
 COLOURING FLASHINGS  
 ALL WORKMANSHIP TO BE IN ACCORDANCE WITH THE MANUFACTURERS  
 INSTRUCTIONS. COLOUR TO BE SELECTED BY OWNER.

**EAVES & SCAFFOLDS**  
 OVERHANG ROOFS AS SHOWN OR AS NOTED ON PLANS.  
 FRAME FOR LEVEL EAVES AND LINE WITH HARDWEX  
 ALL SCAFFOLDS TO BE IN ACCORDANCE WITH THE MANUFACTURERS  
 LINEAL SPECS WITH HARDWEX SHEETING.

AMENDED



**SCOLYER DESIGNS**  
 DESIGN & DRAWING  
**G. & M. SCOLYER**  
 7 AVRAM CRT. 1, LEGANA  
 A.S. 1530 1195  
 phone (08) 938 550 814  
 mobile 0438 550 814  
 gscolyer@scolyer.com.au  
 ABN: 45 576 638 596  
 CC fee 1553

Rev No.	Description	Date
B	AMENDMENTS	21.08.17
A	DEVELOPMENT APPLICATION	09.08.17

**Client:**  
 S. CHUGG

**Project:**  
 VILLA DEVELOPMENT  
 15 LAYCOCK ST  
 LONGFORD, TAS. 7301

ELEVATIONS			
Date:	Project No.:	Rev. No.:	
AUG. 17		B	
Scale:	Sheet:	No. OF	No.
1 : 100	16 OF 16		A15

VILLA 6

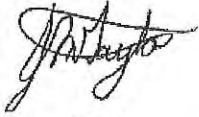


## Submission to Planning Authority Notice

Council Planning Permit No.	P17-204	Council notice date	29/08/2017
<b>TasWater details</b>			
TasWater Reference No.	TWDA 2017/01381-NMC	Date of response	05/09/2017
TasWater Contact	Phil Papps	Phone No.	(03) 6237 8246
<b>Response issued to</b>			
Council name	NORTHERN MIDLANDS COUNCIL		
Contact details	Planning@nmc.tas.gov.au		
<b>Development details</b>			
Address	15 LAYCOCK ST, LONGFORD	Property ID (PID)	6733176
Description of development	Multiple Dwellings x 6 (one existing)		
<b>Schedule of drawings/documents</b>			
	Prepared by	Drawing/document No.	Revision No.
	Scolyer Designs	Site Plan / A02 Sht 3	B
			Date of Issue
			21/08/2017
<b>Conditions</b>			
Pursuant to the <i>Water and Sewerage Industry Act 2008 (TAS)</i> Section 56P(1) TasWater imposes the following conditions on the permit for this application:			
<b>CONNECTIONS, METERING &amp; BACKFLOW</b>			
1. A suitably sized water supply with metered connections / sewerage system and connections to each dwelling unit of the development must be designed and constructed to TasWater's satisfaction and be in accordance with any other conditions in this permit.			
2. Any removal/supply and installation of water meters and/or the removal of redundant and/or installation of new and modified property service connections must be carried out by TasWater at the developer's cost.			
<b>DEVELOPMENT ASSESSMENT FEES</b>			
3. The applicant or landowner as the case may be, must pay a development assessment fee of \$343.55 to TasWater, as approved by the Economic Regulator and the fee will be indexed, until the date it is paid to TasWater. The payment is required within 30 days of the issue of an invoice by TasWater.			
<b>Advice</b>			
<b>General</b>			
For information on TasWater development standards, please visit <a href="http://www.taswater.com.au/Development/Development-Standards">http://www.taswater.com.au/Development/Development-Standards</a>			
For application forms please visit <a href="http://www.taswater.com.au/Development/Forms">http://www.taswater.com.au/Development/Forms</a>			
The developer is responsible for arranging to locate existing TasWater infrastructure and clearly showing it on any drawings. Existing TasWater infrastructure may be located by TasWater (call 136 992) on site at the developer's cost, alternatively a surveyor and/or a private contractor may be engaged at the developers cost to locate the infrastructure.			
<b>Declaration</b>			
The drawings/documents and conditions stated above constitute TasWater's Submission to Planning			

Authority Notice.

**Authorised by**

A handwritten signature in black ink, appearing to read "J. Taylor".

**Jason Taylor**

Development Assessment Manager

**TasWater Contact Details**

Phone	13 6992	Email	development@taswater.com.au
Mail	GPO Box 1393 Hobart TAS 7001	Web	www.taswater.com.au

## ATTACHMENT C

Northern Midlands Council  
([planning@nmc.tas.gov.au](mailto:planning@nmc.tas.gov.au))

Dear Sir/Madam  
Re Ref No P17-204  
Site 15 Laycock street Longford

I wish to object to the proposed Multiple dwelling (5 new dwellings)

I own and reside at the neighbouring property, 13 Laycock Street Longford.

The bases for my objection are as follows:

1. **FLOODING:** During the seasons of Autumn, Winter and Spring, all properties 11, 13 and 15 Laycock are subject to significant groundwater flooding, which often renders parts of our land un-usable. I have spent considerable time, effort and money, installing soakage drains, tanks and gardens to reduce the problem; however there is no adequate slope off the land to properly drain the site. I am aware that when N.M.Council allows construction of residential homes, there is a requirement to build floor level 300mm above existing ground level. If this were to happen, it would further exacerbate the drainage problem and have a severe detrimental effect on my property and my ability to enjoy or use my yard. I could not identify any flood mitigation processes noted in this application.
2. **PARKING:** The scheme requires 2 spaces per residential unit plus an additional visitor parking allocation of 1 space for every 3 units. This application falls far short of this requirement, therefore does not comply.
3. **TURNING CIRCLES:** From the plans submitted, it appears that the required turning circles as per standards seem not to comply.
4. **TRAFFIC MOVEMENTS:** I strenuously dispute the findings of the traffic report in stating that there would be only 25 traffic movements per day. This is less than one movement per vehicle/per day. I personally enter and leave my home between 2-3 times per day, and if this is a basic guide, the stated anticipated traffic flow projections are severely understated, and I would suggest that they have been provided simply to assist the application, and are not a true representation of the actual traffic flow.
5. **SHADOWING:** I feel that with the intended dwellings being built so close to my boundary, and given that they will be at least 300mm above normal ground level, I fear that my home will be severely impacted by over shadowing, and this will lead to further loss of amenity to my home. I saw no "shadowing plans" in this application, and I am of the view that this is a considered omission to further assist the application.
6. The intended constructions are on the southern side of my home. This area along the southern side of my home is very wet and damp during the cooler and wetter months of the year. As my home is an 1800 construction it is built on the ground. This area does not receive adequate sun light to dry the area out and I feel the intended constructions will increase the issue of dampness both indoors and out for my home.
7. When I purchased my home, one of the attractions was the feeling of space, as both adjoining properties were on large allotments of land, allowing an element of privacy for all. At that time neither neighbour had any intention of sub-dividing or further developing their land (I asked both property owners prior to purchase) this development will severely impact on my ability to enjoy my property for the purpose that I acquired it.
8. **ALLOTMENT SIZE:** At an average land allocation of approx 330 sq/m, this is not the type of development that is charastic of the majority of other developments in the township of Longford. I think this proposal is detrimental to the amenity of the Longford Township.

9. LOSS OF AMENITY: With minimal boundary setbacks on my boundary, my hobbies and interests will have a negative effect on the occupants of the proposed development. This will undoubtedly lead to complaints and possibly conflict and will again erode my ability to use my home for my own enjoyment.
10. I will like the opportunity to address this issue in person at a schedule meeting; however I have a pre-planned trip oversea where I am leaving on September 12<sup>th</sup> and returning 13<sup>th</sup> October 2017. I request that all schedule meeting be held after that date in October for my ability to attend any meeting.

I ask that Council consider my concerns and refuse the Application

Kind Regards

Maggie Wilson  
13 Laycock Street Longford 7301  
[Maggiewilson58@gmail.com](mailto:Maggiewilson58@gmail.com)  
0467556307

15 Laycock Street, Longford - Applicant response to representation received to Planning Application P17-204 - Multiple dwellings x 5 & new access (vary side [N & S] setbacks & widow/access separation)

**Objection #1 - Flooding**

During the seasons of Autumn, Winter and Spring, all properties 11, 13 and 15 Laycock are subject to significant groundwater flooding which often renders parts of our land un-usable. I have spent considerable time, effort and money, installing soakage drains, tanks and gardens to reduce the problem; however there is no adequate slope off the land to properly drain the site. I am aware that when N.M.Council allows construction of residential homes, there is a requirement to build floor level 300mm above existing ground level. If this were to happen, it would further exacerbate the drainage problem and have a severe detrimental on my property and my ability to enjoy or use my yard. I could not identify any flood mitigation processes noted in this application.

**Applicant Response #1:**

1.1 - Stormwater management has been a significant consideration of the development design process, as evidence in the drainage plan layout provided in the submission.

1.2 - Council's Engineering Department were proactively engaged in relation to stormwater management and the feedback received was included in the drainage plan.

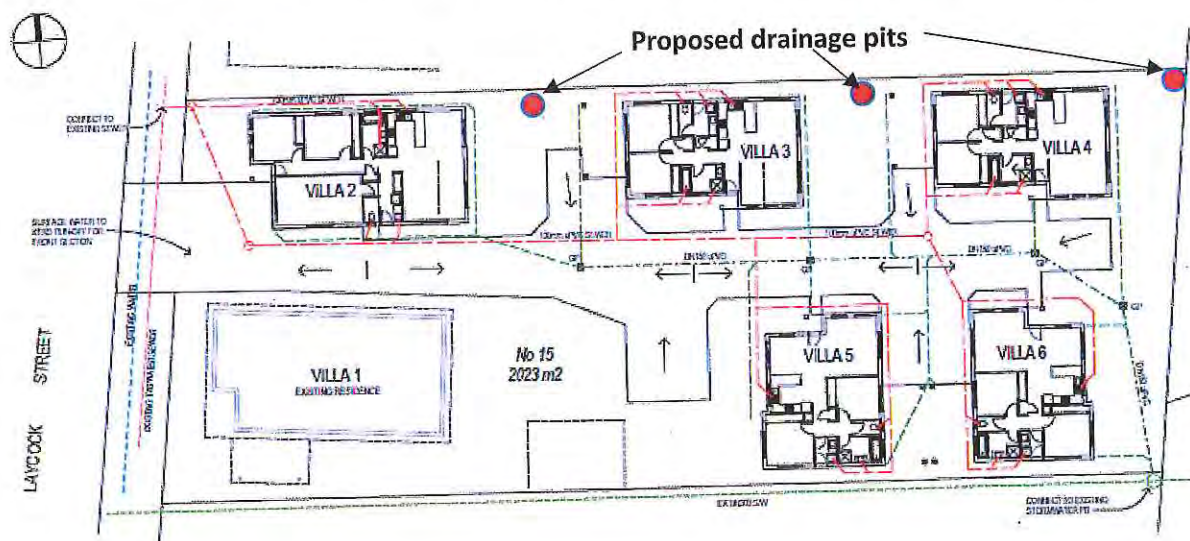
1.3 - Water collected from the roofs and driveway of the proposed development will be gravity fed via pipeline to Council's existing stormwater main.

1.4 - The driveway will additionally be built with stormwater retention capacity (i.e. onsite water storage capacity), in order to provide added protection to adjacent properties during major storm events with abnormally high rainfall levels.

1.5 - The development will not directly impact soakage drains, tanks and gardens located on the northern neighbour's property, as no excavation is proposed on this property.

**Applicant Concessions:**

1.6 - I am willing to install drainage pits along the northern boundary, which will assist the removal of surface water from the neighbour's property along the boundary after major storm events.



### Objection #2 – Parking

The scheme requires 2 spaces per residential unit plus an additional visitor parking allocation of 1 space for every 3 units. This application falls far short of this requirement, therefore does not comply.

### Applicant Response #2:

2.1 - The planning scheme requires a total of 2 spaces per residential dwelling and 1 space for every 3 or 4 dwellings depending on interpretation (refer Table E6.1 of Car Parking and Sustainable Transport Code).

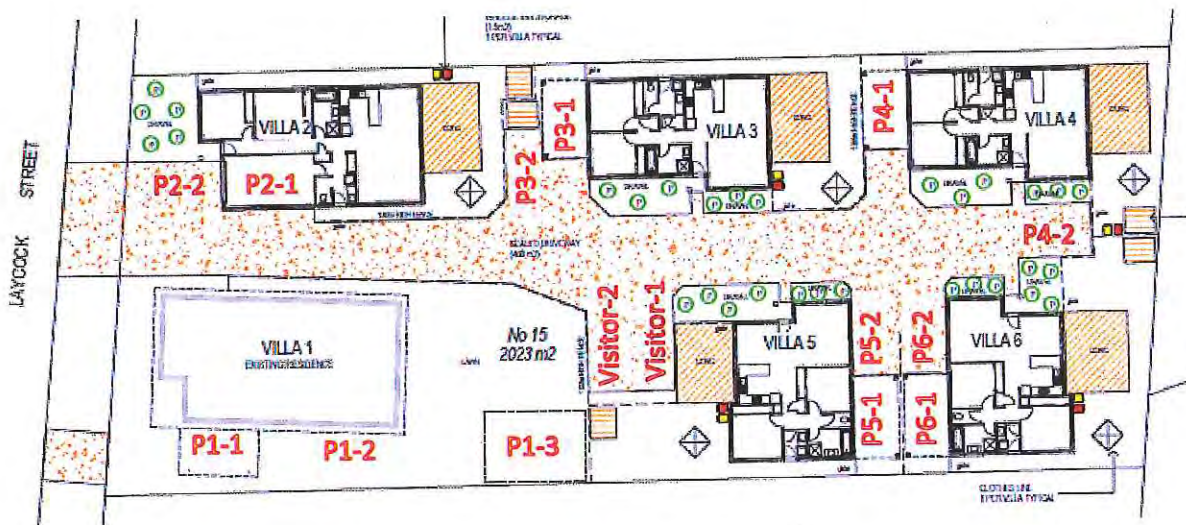
2.2 - The proposed development provides a total of 15 car spaces, including 2 for visitors, as detailed in the planning submission and shown below.

2.3 - The provision of 2 visitor parking spots is compliant whether either 1 every 3 or 4 dwelling is applied (both cases require 2 visitor parking spots).

2.4 - The total number of car spaces complies with the parking requirements outlined in Table E6.1 of the Car Parking and Sustainable Transport Code.

### Applicant Concessions:

2.5 – No changes are proposed to the 15 parking spaces provided (see layout below).



15 Laycock Street, Longford - Applicant response to representation received to Planning Application P17-204 - Multiple dwellings x 5 & new access (vary side [N & S] setbacks & widow/access separation)

**Objection #3 - Turning circles**

From the plans submitted, it appears that the required turning circles as per standards seem not to comply.

**Applicant Response #3:**

3.1 - The provision of appropriate turning circles improves the amenity and safety of a multiple dwelling development by allowing cars to exit driving forwards, and as a result has had significant consideration in the layout design.

3.2 - The provision of turning was reviewed and approved by Council against the access widths specified in Table E6.1 of Car Parking and Sustainable Transport Code.

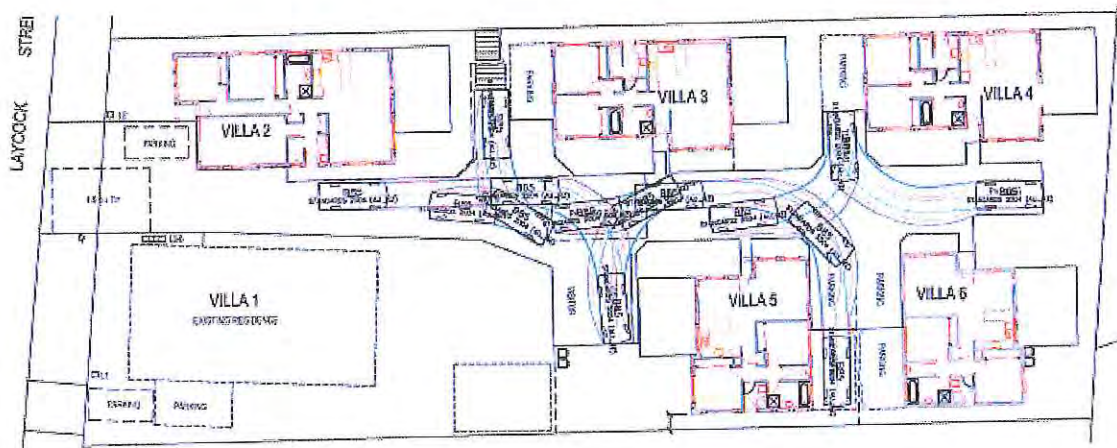
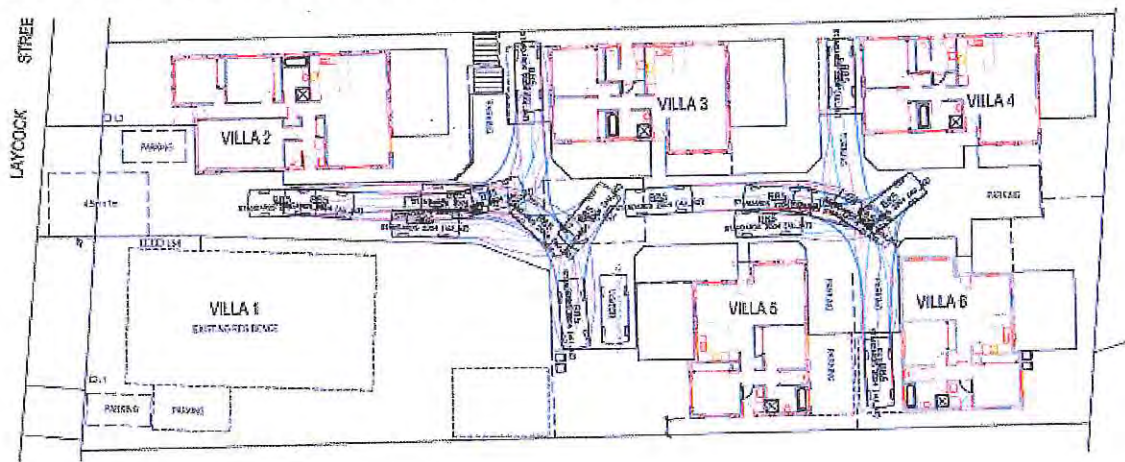
3.3 - Council's assessment was supported by parking turning circle drawings as part of the development application (see below), prepared in accordance with Australian Standards using Transoft Solutions software.

3.4 - Turning movements have also been provisioned adjacent to landscaped gardens, as opposed to immovable buildings or fences to provide additional driver ease/comfort.

3.5 - The driveway design includes angled edges in order to further support turning.

**Applicant Concessions:**

3.5 - No changes are proposed to the current turning circles.





15 Laycock Street, Longford - Applicant response to representation received to Planning Application P17-204 - Multiple dwellings x 5 & new access (vary side [N & S] setbacks & widow/access separation)

**Objection #4 - Traffic movements**

I strenuously dispute the findings of the traffic report in stating that there would be only 25 traffic movements per day. This is less than one movement per vehicle/per day. I personally enter and leave my home between 2-3 times per day, and if this is a basic guide, the stated anticipated traffic flow projections are severely understated, and I would suggest that they have been provided simply to assist the application, and are not a true representation of the actual traffic flow.

**Applicant Response #4:**

4.1 - The 25 traffic movements per day are based on the incremental uplift from the 5 new units only (i.e. they do not include the traffic movements from the existing dwelling, which uses a separate existing driveway).

4.2 - The traffic movements were calculated based on benchmark movements for 2 bedroom units, as provided in the "Guide to Traffic Generating Developments" prepared by RTA.

4.3 - The RTA Guide is used widely across industry to determine generated vehicle movements and advises 4-5 daily vehicle movements per 2 bedroom unit. For the traffic assessment the conservative upper range of 5 vehicle movements per dwelling was adopted.

4.4 - Assuming two cars per new unit this is equivalent to 2.5 movements per day per vehicle, which aligns with 2-3 times per day noted in the objection.

4.5 - There is approximately a 20 metre separation from the objecting neighbour's driveway and the proposed driveway (see below), which will further alleviate impact of traffic movements.

**Applicant Concessions:**

4.6 - No changes are proposed to amend the Traffic Impact Assessment report or access driveway.



**Objection #5 - Shadowing**

I feel that with the intended dwellings being built so close to my boundary, and given that they will be at least 300mm above normal ground level, I fear that my home will be severely impacted by over shadowing, and this will lead to further loss of amenity to my home. I saw no "shadowing plans" in this application, and I am of the view that this is a considered omission to further assist the application.

**Applicant Response #5:**

5.1 - Shadowing is not an issue to either of the neighbouring properties due to the fact shadows in Tasmania are cast in a southerly direction and the neighbour's dwelling on the southern side is not within range of shadows cast.

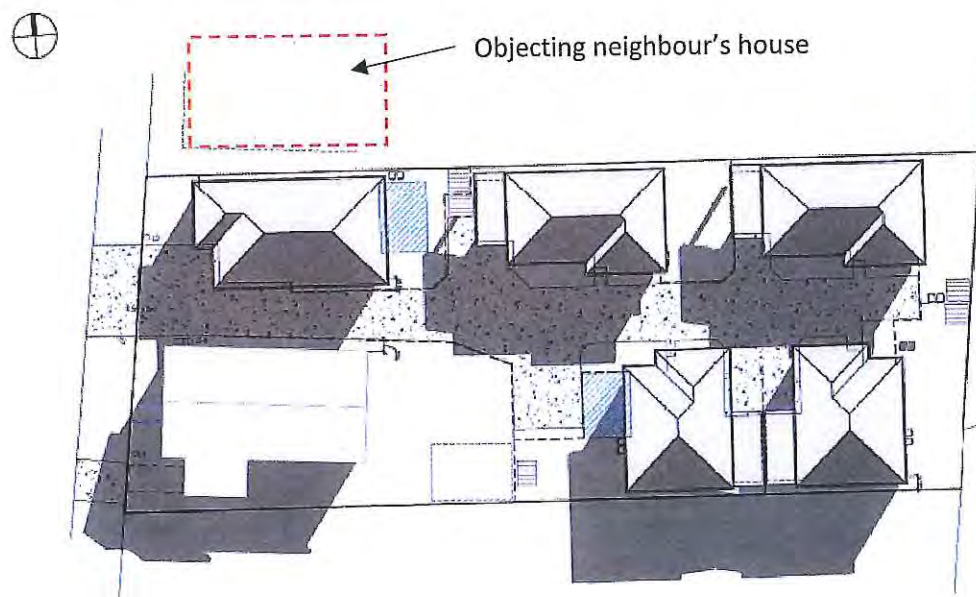
5.2 - Council's Planning Department are required to assess whether shadowing could be an issue, but based on their judgement they did not request a shadowing plan to be prepared for this development application.

5.3 - Despite this I have engaged a drafter (at my cost) to prepare shadow plans to address the neighbour's concerns, as shown below.

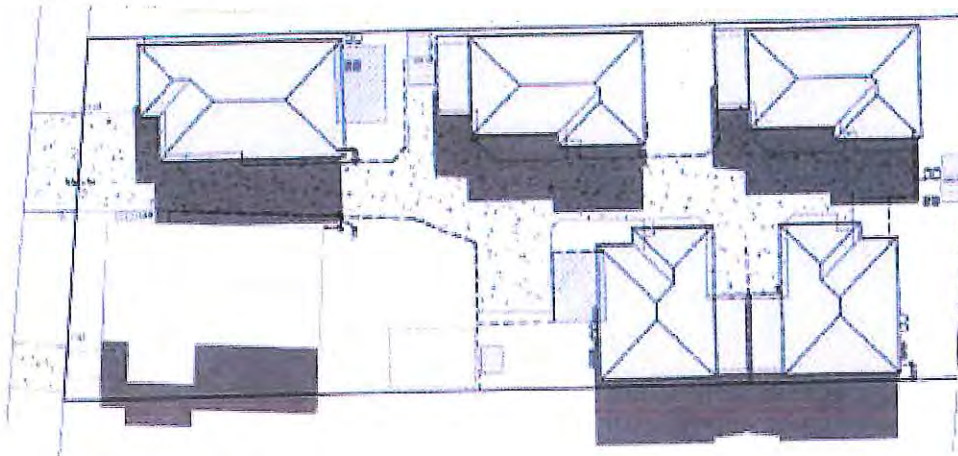
**Applicant Concessions:**

5.4 - No changes are proposed to the layout based on shadowing.

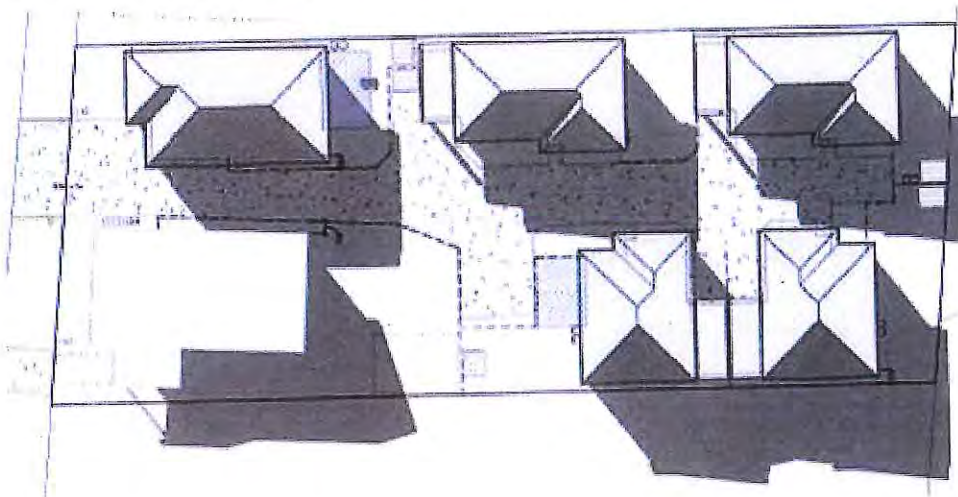
**Shadow Plan 1: 21st June, 10:00 AM**



**Shadow Plan 2: 21<sup>st</sup> June, 12:00 PM**



**Shadow Plan 3: 21<sup>st</sup> June, 3:00PM**



**Objection #6 - Dampness**

The intended constructions are on the southern side of my home. This area along the southern side of my home is very wet and damp during the cooler and wetter months of the year. As my home is an 1800 construction it is built on the ground. This area does not receive adequate sun light to dry the area out and I feel the intended constructions will increase the issue of dampness both indoors and out for my home.

**Applicant Response #6:**

6.1 - The southern side of the property is likely damp due to it getting less sunlight, as a result of shadow direction in Tasmania.

6.2 - The proposed development is unlikely to increase outdoor dampness, as it will have no impact on the sun light exposure received on the northern neighbour's property.

6.3 - Any indoor dampness being experienced may indicate an issue with the existing building, which is not uncommon with older Tasmanian buildings.

**Applicant Concessions:**

6.4 - No changes are proposed to the layout based on general dampness.

**15 Laycock Street, Longford - Applicant response to representation received to Planning Application P17-204 - Multiple dwellings x 5 & new access (vary side [N & S] setbacks & widow/access separation)**

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**Objection #7 - Loss of amenity #1**

When I purchased my home, one of the attractions was the feeling of space, as both adjoining properties were on large allotments of land, allowing an element of privacy for all. At that time neither neighbour had any intention of sub-dividing or further developing their land (I asked both property owners prior to purchase) this development will severely impact on my ability to enjoy my property for the purpose that I acquired it.

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**Applicant Response #7:**

7.1 - No. 9 and no. 15 Laycock Street, Longford are both zoned General Residential under the NMC Interim Planning Scheme 2013, which based on their land areas means that they are both strong candidates to be subdivided/further developed.

7.2 - The fact that both neighbours were contacted by the objecting party in relation to whether they intended to undertake further development, suggests that at purchase it was fully understood that both properties were eligible for sub-division/further development.

7.3 - Any discussions with other owners at the time are not a matter between myself and the objecting party.

7.4 - There are a number of measures that can be undertaken to maintain privacy, such as hedges or high fences (many of which are already in place along the connecting boundary; see image below).

**Applicant Concessions:**

7.5 - I am willing to install lattice at the top (at my cost) of the existing 6 foot high fence along the northern boundary in order to provide even further privacy.



**Objection #8 -**

At an average land allocation of approx 330 sq/m, this is not the type of development that is charastic of the majority of other developments in the township of Longford. I think this proposal is detrimental to the amenity of the Longford Township.

**Applicant Response #8:**

8.1 - The development land allocation exceed the 325m2 guidelines set out for multiple dwellings within the Section 10.4.1 of the NMC Interim Planning Scheme 2013.

8.2 - There are a numerous multiple dwelling developments dispersed throughout the whole extent of the township of Longford that are at this level of density.

8.3 - The proposed development will not have a detrimental impact on the amenity of the Longford Township, as it is in line with the existing Planning Scheme.

**Applicant Concessions:**

8.4 - No changes are proposed to the land allocations submitted.

**Objection #9 – Loss of amenity #2**

With minimal boundary setbacks on my boundary, my hobbies and interests will have a negative effect on the occupants of the proposed development. This will undoubtedly lead to complaints and possibly conflict and will again erode my ability to use my home for my own enjoyment.

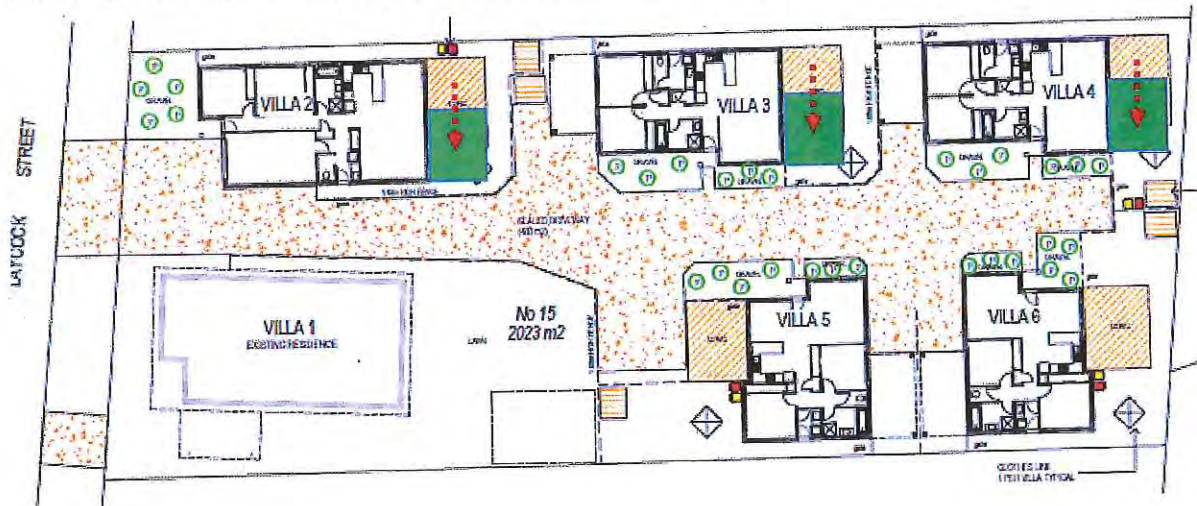
**Applicant Response #9:**

9.1 - Any hobbies and interests currently undertaken should not be an issues, as long as they comply with Local Council’s requirements and allowed activities within the township of Longford.

9.2 - The existing garden beds, established hedges, and trees along the northern neighbour’s fence line provide natural separation (as shown previously); which will be supported further by the addition of lattice atop the fence (see item 7.5)

**Applicant Concessions:**

9.3 - I am willing to relocate the private open space areas away from the fence (see image below) to provide greater separation (and privacy) between congregation points from the neighbour’s yard.



**15 Laycock Street, Longford - Applicant response to representation received to Planning Application P17-204 - Multiple dwellings x 5 & new access (vary side [N & S] setbacks & widow/access separation)**

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**Additional comments**

I will like the opportunity to address this issue in person at a schedule meeting; however I have a pre-planned trip oversea where I am leaving on September 12th and returning 13th October 2017. I request that all schedule meeting be held after that date in October for my ability to attend any meeting.

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**Applicant Response:**

I am available to discuss the development with the objecting party prior to October 13<sup>th</sup> communicating via the Northern Midlands Council. This will allow me to get a better understanding of their concerns and potentially get a resolution prior to going to the council meeting.

Otherwise I am happy to attend the council meeting on October 16<sup>th</sup>, which is after the objecting party has returned from overseas.

Kind regards,

Sam Chugg