

PLAN 

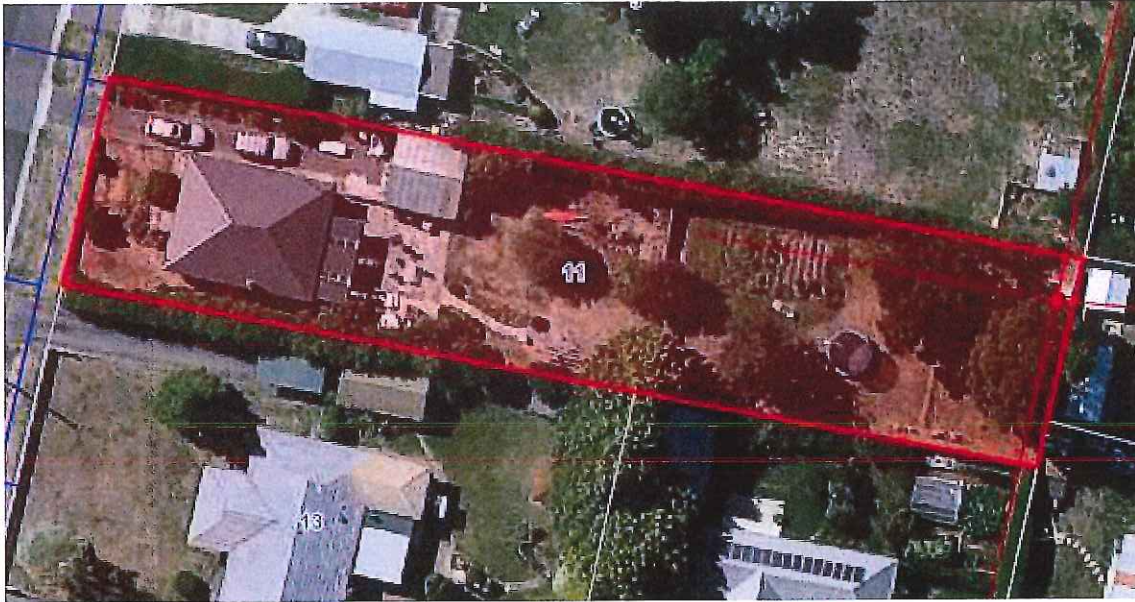
PLANNING APPLICATION PLN-19-0231

11 BURGHLEY STREET, LONGFORD

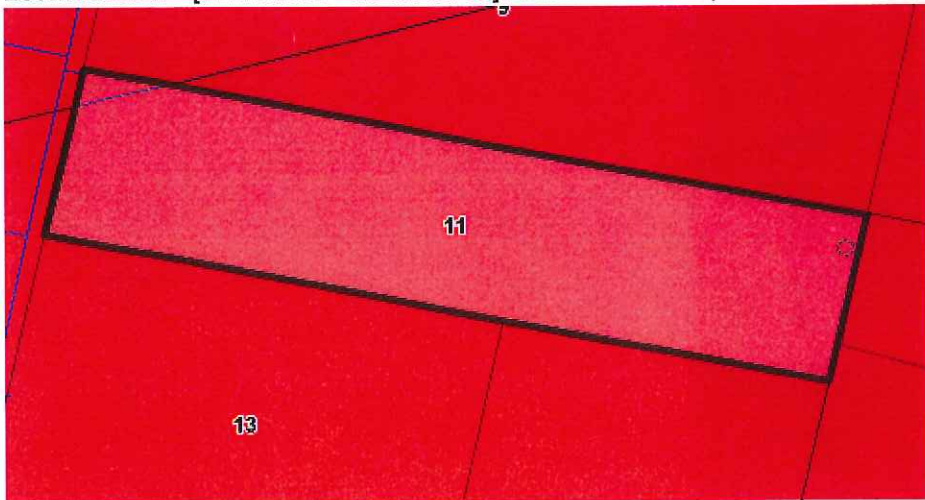
**ATTACHMENTS**

- A Application & plans
- B Responses from referral agencies
- C Representations & applicant's response

**AERIAL PHOTOGRAPH & SERVICES MAP for 11 BURGHLEY STREET, LONGFORD**



**ZONING MAP - [PLANNING SCHEME 1995] AMMENDMENTS; RESIDENTIAL SERVICED ZONE**



**EXHIBITED**

1-89  
PLANNING APPLICATION  
Proposal

Description of proposal: 2 LOT SUBDIVISION

(attach additional sheets if necessary)

If applying for a subdivision which creates a new road, please supply three proposed names for the road, in order of preference:

1..... 2..... 3.....

Site address: 11 BURGHELY STREET, LONGFORD

CT no: 32979/1

Estimated cost of project \$ 20,000 (include cost of landscaping, car parks etc for commercial/industrial uses)

Are there any existing buildings on this property?  Yes / No  
If yes - main building is used as RESIDENTIAL

If variation to Planning Scheme provisions requested, justification to be provided:

1) PASSING BAY WIDTH 4.5m RATHER THAN 5m

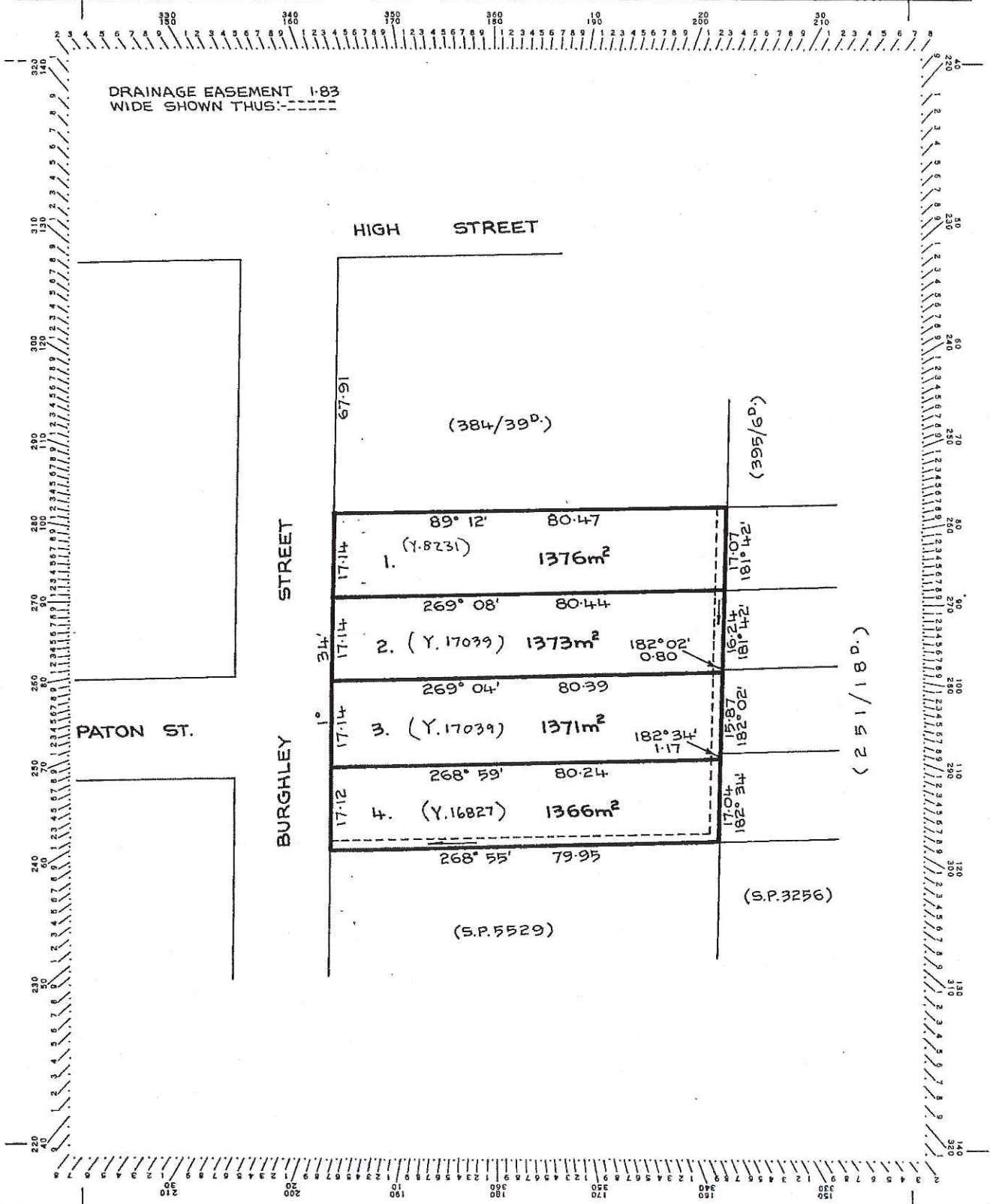
2) WITHIN ATTENUATION AREA

(attach additional sheets if necessary)

Is any signage required? .....

(if yes, provide details)

Owner: L.T.ACT 1980.	PLAN OF SURVEY of land situated in the <b>TOWN OF LONGFORD</b> <b>SEC.U.</b> COMPILED FROM 43/91.D.O. SCALE 1:800 MEASUREMENTS IN METRES	Registered Number: <b>D.32979</b>
Title Reference: Y.8231.		Approved: 26 OCT 1987
Grantee: PART OF 3-3-32 JOHN JOSEPH MOORE.		 Recorder of Titles





General Manager  
Northern Midlands Council  
PO Box 156  
LONGFORD TAS 7301

21 November 2019

Dear Sir/madam,

**RE: Planning Application, 2 Lot Subdivision – 11 Burghley Street, Longford**

This letter is prepared in support of a proposal for a Subdivision at land identified in CT 32979/1. One additional lot is applied for.

The Recreation and Open Space Code is applicable to the proposal as it is located within the General Residential Zone. We seek to have the requirement for the provision of public open space waived and instead there be a cash payment in lieu in this particular instance.

Please could you provide written consent as a matter of urgency to the Planning Department of the Northern Midlands Council to ensure that the development application proceeds in a timely manner.

Kind Regards,

Rebecca Green

Senior Planning Consultant  
m – 0409 284422  
e – admin@rgassociates.com.au

**Rosemary Jones**

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**From:** Des Jennings  
**Sent:** Tuesday, 26 November 2019 2:02 PM  
**To:** NMC Planning  
**Subject:** Planning Application - 11 Burghley St Longford - Public Open Space

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Hi Michelle,

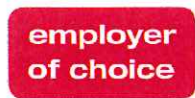
Agreeable to cash payment in lieu of the provision of open space.

Thanks Des

*Des Jennings*



General Manager | Northern Midlands Council  
Council Office, 13 Smith Street (PO Box 156), Longford Tasmania 7301  
T: (03) 6397 7303 | F: (03) 6397 7331  
E: [des.jennings@nmc.tas.gov.au](mailto:des.jennings@nmc.tas.gov.au) | W: [www.northernmidlands.tas.gov.au](http://www.northernmidlands.tas.gov.au)



*T a s m a n i a ' s H i s t o r i c H e a r t*

**From:** NMC Planning <[planning@nmc.tas.gov.au](mailto:planning@nmc.tas.gov.au)>  
**Sent:** Tuesday, 26 November 2019 1:54 PM  
**To:** Des Jennings <[des.jennings@nmc.tas.gov.au](mailto:des.jennings@nmc.tas.gov.au)>  
**Subject:** FW: Planning Application - 11 Burghley St Longford - Public Open Space

Good afternoon Des

Can you please confirm by email if you are happy with the public open space being waived in lieu of cash payment?

Regards

*Michelle Campbell*



Administration Officer - Community & Development | Northern  
Midlands Council  
Council Office, 13 Smith Street (PO Box 156), Longford Tasmania 7301  
T: (03) 6397 7303 | F: (03) 6397 7331  
E: [Michelle@nmc.tas.gov.au](mailto:Michelle@nmc.tas.gov.au) | W: [www.northernmidlands.tas.gov.au](http://www.northernmidlands.tas.gov.au)



*T a s m a n i a ' s H i s t o r i c H e a r t*

Planning Department  
Northern Midlands Council  
PO Box 156  
LONGFORD TAS 7301

21 November 2019

Dear Sir/madam,

**RE: Planning Application, 2 Lot Subdivision – 11 Burghley Street, Longford**

This letter is prepared in support of a proposal for Richard Martin and Rebecca Jane Green for a two-lot subdivision at land identified in CT 32979/1.

The proposal is aimed to create Lot 1 – +/- 554m<sup>2</sup>, and Lot 2 – +/- 820m<sup>2</sup> as detailed within the Plan of Subdivision prepared by Cohen & Associates. One lot currently exists; therefore, one additional lot will be created by this proposal.

Lot 1 contains an existing single storey, three-bedroom dwelling. An existing 6.0m x 6.0m garage is to be demolished as part of this proposal. Lot 2 will be vacant residential land. A right of way is proposed over the access strip of Lot 2 in favour of Lot 1 to allow vehicular access to the rear of the existing dwelling. A 3.0m wide easement is proposed around the new and existing sewer.

The subject land is zoned General Residential within the *Northern Midlands Interim Planning Scheme 2013* (the Scheme). The north-western corner of the site (approximately 3.0m in from the boundary is burdened by the Environmental Impacts and Attenuation buffer.

**10-General Residential Zone**

**10.4 Development Standards**

**10.4.2 Setbacks and building envelope for all dwellings**

**Objective: To control the siting and scale of dwellings to:**

- (a) Provide reasonably consistent separation between dwellings on adjacent sites and a dwelling and its frontage; and
- (b) Assist in the attenuation of traffic noise or any other detrimental impacts from roads with high traffic volumes; and
- (c) Provide consistency in the apparent scale, bulk, massing and proportion of dwellings; and
- (d) Provide separation between dwellings on adjacent sites to provide reasonable opportunity for daylight and sunlight to enter habitable rooms and private open space.

Acceptable Solution	Performance Criteria	Proposal Response
A1 Unless within a building area,	P1 A dwelling must:	Not applicable. No

a dwelling, excluding protrusions (such as eaves, steps, porches, and awnings) that extend not more than 0.6m into the frontage setback, must have a setback from a frontage that is:

- (a) If the frontage is a primary frontage, at least 4.5m, or, if the setback from the primary frontage is less than 4.5m, not less than the setback, from the primary frontage, of any existing dwelling on the site; or
- (b) If the frontage is not a primary frontage, at least 3m, or, if the setback from the frontage is less than 3m, not less than the setback, from a frontage that is not a primary frontage, of any existing dwelling on the site; or
- (c) If for a vacant site with existing dwellings on adjoining sites on the same street, not more than the greater, or less than the lesser, setback for the equivalent frontage of the dwellings on the adjoining sites on the same street; or
- (d) If the development is on land that abuts a road specified in Tables 10.4.2, at least that specified for the road.

- (a) Have a setback from a frontage that is compatible with the existing dwellings in the street, taking into account any topographical constraints; and
  - (b) If abutting a road identified in Table 10.4.2, include additional design elements that assist in attenuating traffic noise or any other detrimental impacts associated with proximity to the road.
- changes proposed.*

A2 A garage or carport must have a setback from a primary frontage of at least:

- (a) 5.5m, or alternatively 1m behind the façade of the dwelling; or
- (b) The same as the dwelling façade, if a portion of the

P2 A garage or carport must have a setback from a primary frontage that is compatible with the existing garages or carports in the street, taking into account any topographical constraints.

*Not applicable.*



dwelling gross floor area is located above the garage or carport; or

- (c) 1m, if the natural ground level slopes up or down at a gradient steeper than 1 in 5 for a distance of 10m from the frontage.

A3 A dwelling, excluding outbuildings with a building height of not more than 2.4m and protrusions (such as eaves, steps, porches, and awnings) that extend not more than 0.6m horizontally beyond the building envelope, must:

- (a) Be contained within a building envelope (refer to Diagrams 10.4.2A, 10.4.2B, 10.4.2C and 10.4.2D) determined by:

- (i) A distance equal to the frontage setback or, for an internal lot, a distance of 4.5m from the rear boundary of a lot with an adjoining frontage; and

- (ii) Projecting a line at an angle of 45 degrees from the horizontal at a height of 3m above natural ground level at the side boundaries and a distance of 4m from the rear boundary to a building height of not more than 8.5m above natural ground level; and

- (b) Only have a setback

P3 The siting and scale of a dwelling must:

- (a) Not cause unreasonable loss of amenity by:

- (i) Reduction in sunlight to a habitable room (other than a bedroom) of a dwelling on an adjoining lot; or

- (ii) Overshadowing the private open space of a dwelling on an adjoining lot; or

- (iii) Overshadowing of an adjoining vacant lot; or

- (iv) Visual impacts caused by the apparent scale, bulk or proportions of the dwelling when viewed from an adjoining lot; and

- (b) Provide separation between dwellings on adjoining lots that

*The existing dwelling on Lot 1 complies with the side and rear setbacks. The single dwelling at the closest point is 1.2m from a side boundary and then extends to a distance of 2.8m.*



within 1.5m of a side boundary if the dwelling:

- (i) Does not extend beyond an existing building built on or within 0.2m of the boundary of the adjoining lot; or
- (ii) Does not exceed a total length of 9m or one-third the length of the side boundary (whichever is the lesser).

is compatible with that prevailing in the surrounding area.

#### 10.4.15 Subdivision

##### 10.4.15.1 Lot Area, Building Envelopes and Frontage

###### Objective

To provide lots with areas and dimensions that enable the appropriate siting and construction of a dwelling, private open space, vehicle access and parking, easements and site features.

Acceptable Solutions	Performance Criteria	Proposal Response
<p>A1 Lots must:</p> <ul style="list-style-type: none"> <li>a) Have a minimum area of at least 450m<sup>2</sup> which:               <ul style="list-style-type: none"> <li>i) Is capable of containing a rectangle measuring 10m by 15m; and</li> <li>ii) Has new boundaries aligned from buildings that satisfy the relevant acceptable solutions for setbacks; or</li> </ul> </li> <li>b) Required for public use by the Crown, an agency, or a corporation all the shares of which are</li> </ul>	<p>P1 Each lot for residential use must provide sufficient useable area and dimensions to allow for:</p> <ul style="list-style-type: none"> <li>a) A dwelling to be erected in a convenient and hazard-free location; and</li> <li>b) On-site parking and manoeuvrability; and</li> <li>c) Adequate private open space.</li> </ul>	<p><i>The proposal complies with the acceptable solution a)i) and ii). Lot 1 is proposed to be +/- 554m<sup>2</sup> which is within the General Residential zone and Lot 2 is proposed to be 820m<sup>2</sup>. Each lot is capable of containing a rectangle measuring 10m by 15m.</i></p>

<p>held by Councils or a municipality; or</p> <p>c) For the provision of utilities; or</p> <p>d) For the consolidation of a lot with another lot with no additional titles created; or</p> <p>e) To align existing titles with zone boundaries and no additional lots are created.</p>	<p>A2 Each lot must have a frontage of at least 3.6m.</p>	<p>P2 Each lot must have appropriate, permanent access by a Right of Carriageway registered over all relevant titles.</p>	<p><i>The proposal complies with the acceptable solution. Lot 1 will have a frontage of 12.6m to Burghley Street whilst Lot 2 is to be provided with a 4.5m wide frontage.</i></p>
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#### 10.4.15.2 Provision of Services

##### Objective

To provide lots with appropriate levels of utility services.

Acceptable Solutions	Performance Criteria	Proposal Response
<p>A1 Each lot must be connected to a reticulated:</p> <p>a) Water supply; and</p> <p>b) Sewerage system.</p>	<p>P1 Each lot created must be:</p> <p>a) In a locality for which reticulated services are not available or capable of being connected; and</p> <p>b) Capable of accommodating an on-site wastewater management system.</p>	<p><i>The proposal complies with the acceptable solution. Each lot is capable of being connected to reticulated water supply and reticulated sewerage system, as demonstrated on Preliminary Services Plan prepared by ipd Consulting.</i></p>
<p>A2 Each lot must be connected to a reticulated stormwater system.</p>	<p>P2 Each lot created must be capable of disposal of stormwater to a legal discharge point.</p>	<p><i>The proposal complies with the acceptable solution. Lot 2 is currently connected to a reticulated stormwater system and this is not to change. Lot 1 will be provided with a new connection</i></p>

*as demonstrated on Preliminary Services Plan prepared by ipd Consulting.*

#### 10.4.15.3 Solar Orientation of Lots

##### Objective

To provide for solar orientation of lots and solar access for future dwellings.

Acceptable Solution	Performance Criteria	Proposal Response
A1 At least 50% of lots must have a long axis within the range of: <ul style="list-style-type: none"> <li>a) North 20 degrees west to north 30 degrees east; or</li> <li>b) East 20 degrees north to east 30 degrees south.</li> </ul>	P1 Dimensions of lots must provide adequate solar access, having regard to the likely dwelling size and the relationship of each lot to the road.	<i>The orientation of the lot will not change. Lot 1 already contains an existing dwelling. Lot 2 complies as well as Lot 1 with the acceptable solution b).</i>
A2 The long axis of residential lots less than 500m <sup>2</sup> , must be within 30 degrees east and 20 degrees west of north.	P2 Lots less than 500m <sup>2</sup> must provide adequate solar access to future dwellings, having regard to the: <ul style="list-style-type: none"> <li>a) Size and shape of the development of the subject site; and</li> <li>b) Topography; and</li> <li>c) Location of access way(s) and roads.</li> </ul>	<i>Not applicable.</i>

#### 10.4.15.4 Interaction, Safety and Security

This clause was not used in this planning scheme.

#### 10.4.15.5 Integrated Urban Landscape

##### Objective

To provide attractive and continuous landscaping in roads and public open spaces that contribute to the:

- a) Character and identity of new neighbourhoods and urban places; or
- b) To existing or preferred neighbourhood character, if any.

Acceptable Solution	Performance Criteria	Proposal Response
A1 The subdivision must not create any new road, public open space or other reserves.	P1 For subdivision that creates roads, public open space or other reserves, the design must demonstrate that: <ul style="list-style-type: none"> <li>a) It has regard to existing, significant features; and</li> <li>b) Accessibility and mobility through public spaces and</li> </ul>	<i>The proposal complies with the acceptable solution. No new road, public open space or other reserve is proposed to be created.</i>

- roads are protected or enhanced; and
- c) Connectivity through the urban environment is protected or enhanced; and
- d) The visual amenity and attractiveness of the urban environment is enhanced; and
- e) It furthers the local area objectives, if any.

**10.4.15.6 Walking and Cycling Network**

**Objective**

- a) To provide safe, convenient and efficient movement through and between neighbourhoods by pedestrians and cyclists; and
- b) To design footpaths, shared path and cycle path networks that are safe, comfortable, well constructed and accessible.
- c) To provide adequate provision to accommodate wheelchairs, prams, scooters and other footpath bound vehicles.

<b>Acceptable Solution</b>	<b>Performance Criteria</b>	<b>Proposal Response</b>
A1 Subdivision must not create any new road, footpath or public open space.	<p>P1 Subdivision that creates new roads, footpaths, or public open spaces must demonstrate that the walking and cycling network is designed to:</p> <ul style="list-style-type: none"> <li>a) Link to any existing pedestrian and cycling networks; and</li> <li>b) Provide the most practicable direct access for cycling and walking to activity centres, community facilities, public transport stops and public open spaces; and</li> <li>c) Provide an interconnected and continuous network of safe, efficient and convenient footpaths, shared paths, cycle paths and cycle lanes based primarily on the network of arterial roads, neighbourhood roads and</li> </ul>	<i>The proposal complies with the acceptable solution. No new road, footpath or public open space is proposed to be created.</i>

- regional public open spaces; and
- d) Promote surveillance along roads and from abutting dwellings.

#### 10.4.15.7 Neighbourhood Road Network

##### Objective

- a) To provide for convenient, safe and efficient movement through and between neighbourhoods for pedestrians, cyclists, public transport and other motor vehicles using the neighbourhood road network; and
- b) To design and construct road carriageways and verges so that the road geometry and traffic speeds provide an accessible and safe neighbourhood road system for all users.

Acceptable Solution	Performance Criteria	Proposal Response
A1 The subdivision must not create any new road.	<p>P1 The neighbourhood road network must:</p> <p>a) Take account of the existing mobility network of arterial roads, neighbourhood roads, cycle paths, shared paths, footpaths and public transport routes; and</p> <p>b) Provide clear hierarchy of roads and physical distinctions between arterial roads and neighbourhood road types; and</p> <p>c) Provide an appropriate speed environment and movement priority for the safe and easy movement of pedestrians and cyclists and for accessing public transport; and</p> <p>d) Provide safe and efficient access to activity centres for commercial and freight vehicles; and</p> <p>e) Ensure connector roads align between neighbourhoods for safe, direct and efficient movement of pedestrians, cyclists,</p>	<p><i>The proposal complies with the acceptable solution. No new road is proposed to be created.</i></p>

	<p>public transport and other motor vehicles; and</p> <p>f) Provide an interconnected and continuous network of roads within and between neighbourhoods for use by pedestrians, cyclists, public transport and other vehicles and minimise the provision of cul-de-sacs; and</p> <p>g) Provide for service and emergency vehicles to safely turn at the end of a dead-end road; and</p> <p>h) Take into account any identified significant features.</p>
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**10.4.4.6 Integrated Urban Landscape**

**Objective**  
 To provide attractive and continuous landscaping in roads and public open spaces that contribute to the:

- a) Character and identity of new neighbourhoods and urban places; or
- b) To existing or preferred neighbourhood character, if any.

Acceptable Solution	Performance Criteria	Proposal Response
A1 The subdivision must not create any new road, public open space or other reserves.	<p>P1 For subdivision that creates roads, public open space or other reserves, the design must demonstrate that:</p> <ul style="list-style-type: none"> <li>f) It has regard to existing, significant features; and</li> <li>g) Accessibility and mobility through public spaces and roads are protected or enhanced; and</li> <li>h) Connectivity through the urban environment is protected or enhanced; and</li> <li>i) The visual amenity and attractiveness of the urban environment is enhanced; and</li> <li>j) It furthers the local area objectives, if any.</li> </ul>	<i>The proposal complies with the acceptable solution. No new road, public open space or other reserve is proposed to be created.</i>

#### 10.4.4.7 Walking and Cycling Network

##### Objective

- a) To provide safe, convenient and efficient movement through and between neighbourhoods by pedestrians and cyclists; and
- b) To design footpaths, shared path and cycle path networks that are safe, comfortable, well constructed and accessible.
- c) To provide adequate provision to accommodate wheelchairs, prams, scooters and other footpath bound vehicles.

Acceptable Solution	Performance Criteria	Proposal Response
A1 Subdivision must not create any new road, footpath or public open space.	<p>P1 Subdivision that creates new roads, footpaths, or public open spaces must demonstrate that the walking and cycling network is designed to:</p> <ul style="list-style-type: none"> <li>e) Link to any existing pedestrian and cycling networks; and</li> <li>f) Provide the most practicable direct access for cycling and walking to activity centres, community facilities, public transport stops and public open spaces; and</li> <li>g) Provide an interconnected and continuous network of safe, efficient and convenient footpaths, shared paths, cycle paths and cycle lanes based primarily on the network of arterial roads, neighbourhood roads and regional public open spaces; and</li> <li>h) Promote surveillance along roads and from abutting dwellings.</li> </ul>	<i>The proposal complies with the acceptable solution. No new road, footpath or public open space is proposed to be created.</i>

**E1.0 Bushfire-Prone Areas Code** – Not applicable, the site is not mapped as being bushfire-prone.

**E4.0 Road and Railway Code**

**E4.6.1 Use of Road or Rail Infrastructure**



**Objective**

To ensure that the safety and efficiency of road and rail infrastructure is not reduced by the creation of new accesses and junctions or increased use of existing accesses and junctions.

<b>Acceptable Solution</b>	<b>Performance Criteria</b>	<b>Proposal Response</b>
<p>A1 Sensitive use on or within 50m of a category 1 or 2 road, in an area subject to a speed limit of more than 60km/h, a railway or future road or railway, must not result in an increase to the annual average daily traffic (AADT) movements to or from the site by more than 10%.</p>	<p>P1 Sensitive use on or within 50m of a category 1 or 2 road, in an area subject to a speed limit of more than 60km/h, a railway or future road or railway must demonstrate that the safe and efficient operation of the infrastructure will not be detrimentally affected.</p> <p>1.</p>	<p><i>Not applicable. Burghley Street is not a Category 1 or 2 road.</i></p>
<p>A2 For roads with a speed limit of 60km/h or less the use must not generate more than a total of 40 vehicle entry and exit movements per day.</p>	<p>P2 For roads with a speed limit of 60km/h or less, the level of use, number, location, layout and design of accesses and junctions must maintain an acceptable level of safety for all road users, including pedestrians and cyclists.</p>	<p><i>The proposal complies with the acceptable solution for lots. The traffic generation is assessed at less than 40 vehicle entry and exit movements per day.</i></p>
<p>A3 For roads with a speed limit of more than 60km/h the use must not increase the annual average daily traffic (AADT) movements at the existing access or junction by more than 10%.</p>	<p>P3 For limited access roads and roads with a speed limit of more than 60km/h:</p> <p>a) Access to a category 1 road or limited access road must only be via an existing access or junction or the use of development must provide a significant social and economic benefit to the State or region; and</p> <p>b) Any increase in use of an existing access or junction or development of a new access or junction to a limited access road or a category 1, 2 or 3 road must be for a use that is</p>	<p><i>Not applicable. The site is not within a speed limit of more than 60km/h.</i></p>

dependent on the site for its unique resources, characteristics or locational attributes and an alternate site or access to a category 4 or 5 road is not practicable; and

c) An access or junction which is increased in use or is a new access or junction must be designed and located to maintain an adequate level of safety and efficiency for all road users.

**E4.7.1 Development on and Adjacent to Existing and Future Arterial Roads and Railways**

**Objective**

To ensure that development on or adjacent to class 1 or 2 roads (outside 60km/h), railways and future roads and railways is managed to:

- a) Ensure the safe and efficient operation of roads and railways; and
- b) Allow for future road and rail widening, realignment and upgrading; and
- c) Avoid undesirable interaction between roads and railways and other use or development.

Acceptable Solution	Performance Criteria	Proposal Response
<p>A1</p> <p>The following must be at least 50m from a railway, a future road or railway, and a category 1 or 2 road in an area subject to a speed limit of more than 60km/h:</p> <ul style="list-style-type: none"> <li>a) New road works, buildings, additions and extensions, earthworks and landscaping works; and</li> <li>b) Building envelopes on new lots; and</li> <li>c) Outdoor sitting, entertainment and children’s play areas.</li> </ul>	<p>P1</p> <p>Development including buildings, road works, earthworks, landscaping works and level crossings on or within 50m of a category 1 or 2 road, in an area subject to a speed limit of more than 60km/h, a railway or future road or railway must be sited, designed and landscaped to:</p> <ul style="list-style-type: none"> <li>a) Maintain or improve the safety and efficiency of the road or railway or future road or railway, including line of sight from trains; and</li> <li>b) Mitigate significant transport-related environmental impacts, including noise, air</li> </ul>	<p><i>Not applicable. No new road works, buildings, additions or extension, earthworks or landscaping works is proposed as part of this proposal.</i></p>

- pollution and vibrations in accordance with a report from a suitably qualified person; and
- c) Ensure that additions or extensions of buildings will not reduce the existing setback to the road, railway or future road or railway; and
  - d) Ensure that temporary buildings and works are removed at the applicant's expense within three years or as otherwise agreed by the road or rail authority.

#### E4.7.2 Management of Road Accesses and Junctions

##### Objective

To ensure that the safety and efficiency of roads is not reduced by the creation of new accesses and junctions or increased use of existing accesses and junctions.

Acceptable Solution	Performance Criteria	Proposal Response
A1 For roads with a speed limit of 60km/h or less the development must include only one access providing both entry and exit, or two accesses providing separate entry and exit.	P1 For roads with a speed limit of 60km/h or less, the number, location, layout and design of accesses and junctions must maintain an acceptable level of safety for all road users, including pedestrians and cyclists.	<i>Only one access to provide both entry and exit. Lot 1 will benefit over a right-of-way burdening Lot 2 to Burghley Street over the access strip to allow access to the rear of the dwelling on Lot 1. An existing crossover will be utilised for the proposal only.</i>
A2 For roads with a speed limit of more than 60km/h the development must not include a new access or junction.	P2 For limited access roads and roads with a speed limit of more than 60km/h: a) Access to a category 1 road or limited access road must only be via an existing access or junction or the development must provide a significant social and economic	<i>Not applicable.</i>

benefit to the State or region; and

- b) Any increase in use of an existing access or junction or development of a new access or junction to a limited access road or a category 1,2 or 3 road must be dependent on the site for its unique resources, characteristics or locational attributes and an alternate site or access to a category 4 or 5 road is not practicable; and
- c) An access or junction which is increased in use or is a new access or junction must be designed and located to maintain an adequate level of safety and efficiency for all road users.

#### E4.7.3 Management of Rail Level Crossings – Not applicable.

#### E4.7.4 Sight Distances at Accesses, Junctions and Level Crossings

##### Objective

To ensure that use and development involving or adjacent to accesses, junctions and level crossings allows sufficient sight distance between vehicles and between vehicles and trains to enable safe movement of traffic.

Acceptable Solution	Performance Criteria	Proposal Response
A1 Sight distances at	P1	<i>The proposal complies with A1. The existing access point complies with the Safe Intersection Sight Distance shown in Table E4.7.4.</i>
a) An access or junction must comply with the Safe Intersection Sight Distance shown in Table E4.7.4; and	The design, layout and location of an access, junction or rail level crossing must provide adequate sight distances to ensure the safe movement of vehicles.	<i>The existing access point complies with the Safe Intersection Sight Distance shown in Table E4.7.4.</i>
b) Rail level crossings must comply with AS1742.7 Manual of uniform traffic control devices – Railway crossings, Standards Association		

- of Australia; or
- c) If the access is a temporary access, the written consent of the relevant authority has been obtained.

## E6 Car Parking and Sustainable Transport Code

**Table E6.1: Parking Space Requirements**

Use	Parking Requirement		
	Vehicle	Bicycle	Required
Residential	1 space per bedroom or 2 spaces per 3 bedrooms + 1 visitor space for every 5 dwellings	1 space per unit or 1 spaces per 5 bedrooms in other forms of accommodation	2 space – Vehicle 1 bicycle space

### *Proposal Response*

The proposal provides for a minimum of 2 spaces for each lot. The capacity of this area and the site in general is capable of accommodating well in excess of the required parking spaces.

## E6.6 Use Standards

### E6.6.1 Car Parking Numbers

#### Objective

To ensure that an appropriate level of car parking is provided to service use.

Acceptable Solutions	Performance Criteria	Proposal Response
<p>A1 The number of car parking spaces must not be less than the requirements of:</p> <p>a) Table E6.1; or</p> <p>b) A parking precinct plan contained in Table E6.6: Precinct Parking Plans (except for dwellings in the General Residential Zone).</p>	<p>P1 The number of car parking spaces provided must have regard to:</p> <p>a) The provisions of any relevant location specific car parking plan; and</p> <p>b) The availability of public car parking spaces within reasonable walking distance; and</p> <p>c) Any reduction in demand due to sharing of spaces by multiple uses either because of variations in peak demand or</p>	<p>A1 The proposal complies with the acceptable solution. The proposal provides a minimum capacity of 2 spaces for the Residential use (Lot 1 – existing and minimum of 2 future spaces for Lot 2).</p>



- by efficiencies  
gained by  
consolidation; and
- d) The availability and frequency of public transport within reasonable walking distance of the site; and
  - e) Site constraints such as existing buildings, slope, drainage, vegetation and landscaping; and
  - f) The availability, accessibility and safety of on-road parking, having regard to the nature of the roads, traffic management and other uses in the vicinity; and
  - g) An empirical assessment of the car parking demand; and
  - h) The effect on streetscape, amenity and vehicle, pedestrian and cycle safety and convenience; and
  - i) The recommendations of a traffic impact assessment prepared for the proposal; and
  - j) Any heritage values of the site; and
  - k) For residential buildings and



multiple dwellings, whether parking is adequate to meet the needs of the residents having regard to:

- i) The size of the dwelling and the number of bedrooms; and
- ii) The pattern of parking in the locality; and
- iii) Any existing structure on the land.

## E6.7 Development Standards

### E6.7.1 Construction of Car Parking Spaces and Access Strips

#### Objective

To ensure that car parking spaces and access strips are constructed to an appropriate standard.

Acceptable Solutions	Performance Criteria	Proposal Response
<p>A1 All car parking, access strips manoeuvring and circulation spaces must be:</p> <ul style="list-style-type: none"> <li>a) Formed to an adequate level and drained; and</li> <li>b) Except for a single dwelling, provided with an impervious all weather seal; and</li> <li>c) Except for a single dwelling, line marked or provided with other clear physical means to delineate car spaces.</li> </ul>	<p>P1 All car parking, access strips manoeuvring and circulation spaces must be readily identifiable and constructed to ensure that they are useable in all weather conditions.</p>	<p>A1 <i>Not applicable. Lot 1 contains an existing single dwelling where parking is existing. Parking for Lot 2 will be provided when further development is proposed within that Lot.</i></p>

### E6.7.2 Design and Layout of Parking Areas

#### Objective

**To ensure that parking areas are designed and laid out to an appropriate standard.**

<b>Acceptable Solutions</b>	<b>Performance Criteria</b>	<b>Proposal Response</b>
<p>A1.1 Where providing for 4 or more spaces, parking areas (other than for parking located in garages and carports for dwellings in the General Residential Zone) must be located behind the building line; and</p> <p>A1.2 Within the general residential zone, provision for turning must not be located within the front setback for residential buildings or multiple dwellings.</p>	<p>P1 The location of car parking and manoeuvring spaces must not be detrimental to the streetscape or the amenity of the surrounding areas, having regard to:</p> <ul style="list-style-type: none"> <li>a) The layout of the site and the location of existing buildings; and</li> <li>b) Views into the site from the road and adjoining public spaces; and</li> <li>c) The ability to access the site and the rear of buildings; and</li> <li>d) The layout of car parking in the vicinity; and</li> <li>e) The level of landscaping proposed for the car parking.</li> </ul>	<p><i>A1 Not applicable.</i></p>
<p>A2.1 Car parking and manoeuvring space must:</p> <ul style="list-style-type: none"> <li>a) Have a gradient of 10% of less; and</li> <li>b) Where providing for more than 4 cars, provide for vehicles to enter and exit the site in a forward direction; and</li> <li>c) Have a width of vehicular access no less than prescribed in Table E6.2 and Table E6.3; and</li> </ul> <p>A2.2 The layout of car spaces and</p>	<p>P2 Car parking and manoeuvring space must:</p> <ul style="list-style-type: none"> <li>a) Be convenient, safe and efficient to use having regard to matters such as slope, dimensions, layout and the expected number and type of vehicles; and</li> <li>b) Provide adequate space to turn within the site unless reversing from the site</li> </ul>	<p><i>P2 The site is relatively flat with a gradient of less than 10%. Although the access width will be at least 3.0m and a passing bay is provided at the Burghley Street end of Lot 2, the passing bay provides only for a width of 4.5m. With the shorter distance of access to Lots 1 and 2 and the straight nature of the access and flat topography, the access width provides for adequate safety and convenience of users and</i></p>



<p>access ways must be designed in accordance with <i>Australian Standards AS 2890.1 – 2004 Parking Facilities, Part 1: Off Road Car Parking</i>.</p>	<p>would not adversely affect the safety and convenience of users and passing traffic.</p>	<p>passing traffic. Each lot has been designed to provide for vehicles to enter and exit the site in a forward direction whereby adequate area is provided within Lot 2 to manoeuvre and Lot 1 can utilise the benefitting ROW to allow manoeuvring.</p> <p>A2.2 The layout of car spaces and access ways will be designed in accordance with <i>Australian Standards AS 2890.1 – 2004 Parking Facilities, Part 1: Off Road Car Parking</i>.</p>
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**E6.7.3 Parking for Persons with a Disability**

<b>Objective</b>		
To ensure adequate parking for persons with a disability.		
<b>Acceptable Solutions</b>	<b>Performance Criteria</b>	<b>Proposal Response</b>
A1 All spaces designated for use by persons with a disability must be located closest to the main entry point to the building.	P1 No performance criteria.	A1 Not applicable – residential use.
A2 One of every 20 parking spaces or part thereof must be constructed and designated for use by persons with disabilities in accordance with <i>Australian Standards AS/NZ 2890.6 2009</i> .	P2 No performance criteria.	A2 Not applicable – residential use.

**E6.7.4 Loading and Unloading of Vehicles, Drop-off and Pickup**

<b>Objective</b>		
To ensure adequate access for people and goods delivery and collection and to prevent loss of amenity and adverse impacts on traffic flows.		
<b>Acceptable Solutions</b>	<b>Performance Criteria</b>	<b>Proposal Response</b>
A1 For retail, commercial, industrial, service industry or warehouse or storage uses: a) At least one loading bay must be provided in accordance with Table E6.4; and	P1 For retail, commercial, industrial, service industry or warehouse or storage uses, adequate space must be provided for loading and unloading the type of vehicles associated with delivering and	A1 Not applicable – residential use.



- b) Loading and bus bays and access strips must be designed in accordance with Australian Standard AS/NZS 2890.3 2002 for the type of vehicles that will use that site.
- collecting people and goods where these are expected on a regular basis.

## E6.8 Provisions for Sustainable Transport

**E6.8.1 Bicycle End of Trip Facilities** – not used in this planning scheme.

### E6.8.2 Bicycle Parking Access, Safety and Security

#### Objective

To ensure that parking and storage facilities for bicycles are safe, secure and convenient.

Acceptable Solutions	Performance Criteria	Proposal Response
<p>A1.1 Bicycle parking spaces for customers and visitors must:</p> <p>a) Be accessible from a road, footpath or cycle track; and</p> <p>b) Include a rail or hoop to lock a bicycle to that meets <i>Australian Standard AS 2890.3 1993</i>; and</p> <p>c) Be located within 50m of and visible or signposted from the entrance to the activity they serve; and</p> <p>d) Be available and adequately lit in accordance with <i>Australian Standard AS/NZS 1158 2005 Lighting Category C2</i> during the times they will be used; and</p> <p>A1.2 Parking for residents' and employees' bicycles must be under cover and capable of being secured by lock or bicycle lock.</p>	<p>P1 Bicycle parking spaces must be safe, secure, convenient and located where they will encourage use.</p>	<p>A1 <i>Bicycle parking is adequately available within each site.</i></p>
<p>A2 Bicycle parking spaces must have:</p> <p>a) Minimum dimensions of:</p> <p>i) 1.7m in length;</p>	<p>P2 Bicycle parking spaces and access must be of dimensions that provide for their convenient, safe and efficient use</p>	<p>A2 <i>Although not strictly delineated, the site can accommodate bicycle parking with sufficient dimensions to meet the acceptable solution.</i></p>

and
ii) 1.2m in height; and
iii) 0.7m in width at the handlebars; and
b) Unobstructed access with a width of at least 2m and a gradient of no more 5% from a public area where cycling is allowed.

### E6.8.3 Pedestrian Walkways

#### Objective

To ensure pedestrian safety is considered in development.

Acceptable Solutions	Performance Criteria	Proposal Response
A1 Pedestrian access must be provided in accordance with Table E6.5.	P1 Safe pedestrian access must be provided within car park and between entrances to buildings and the road.	A1 Pedestrian access throughout the development as appropriate. No separate access is required due to the number of parking spaces required for two lots.

### E10.0 Recreation and Open Space Code

#### E10.6.1 Provision of Public Open Space

#### Objective

- a) To provide public open space which meets user requirements, including those with disabilities, for outdoor recreational and social activities and for landscaping which contributes to the identity, visual amenity and health of the community; and
- b) To ensure that the design of public open space delivers environments of a high quality and safety for a range of users, together with appropriate maintenance obligations for the short, medium and long term.

Acceptable Solution	Performance Criteria	Proposal Response
A1 The application must include consent in writing from the General Manager that no land is required for public open space but instead there is to be a cash payment in lieu.	P1 Provision of public open space, must: <ol style="list-style-type: none"> <li>a) Not pose a risk to health due to contamination; and</li> <li>b) Not unreasonably restrict public use of the land as a result of:               <ol style="list-style-type: none"> <li>i) services, easements or</li> </ol> </li> </ol>	Attached to this submission, is a letter of request to the General Manager of Council seeking written consent that no land is required for public open space but rather there be cash in lieu contribution for



utilities; and

*the one additional lot to be created.*

ii) stormwater detention basins; and

iii) drainage or wetland areas; and

iv) vehicular access; and

c) Be designed to:

i) provide a range of recreational settings and accommodate adequate facilities to meet the needs of the community, including car parking; and

ii) reasonably contribute to the pedestrian connectivity of the broader area; and

iii) be cost effective to maintain; and

iv) respond to the opportunities and constraints presented by the physical characteristics of the land to provide practically useable open space; and

v) provide for public safety through Crime Prevention Through Environmental Design Principles; and

vi) provide for the reasonable amenity of adjoining land users in the design of facilities and associated works; and

vii) have a clear relationship with adjoining land uses through treatment such as alignment, fencing and



landscaping; and

viii) create attractive environments and focal points that contribute to the existing or desired future character statements, if any.

**E11.0 Environmental Impacts and Attenuation Code** – Applicable to approximately 3m in from the north-western corner of Lot 2. See attached Site Specific Study, noting that this area will only ever be used for driveway access.

The proposal is considered to be consistent with the *Northern Midlands Interim Planning Scheme 2013* and should therefore be considered for approval.

Kind Regards,

Rebecca Green

Senior Planning Consultant

m – 0409 284422

e – admin@rgassociates.com.au

**Site Specific Study for  
11 Burghley Street, LONGFORD  
2 Lot Subdivision**

Response to Planning Scheme provisions of Code E11-  
Environmental Impacts and Attenuation Code, Clause E11.6.1 (P1):

P1 Sensitive use or subdivision for sensitive use within an attenuation area to an existing activity listed in Tables E11.1 and E11.2 must demonstrate by means of a site specific study that there will not be an environmental nuisance or environmental harm, having regard to the:

a) degree of encroachment:

*How close is the emitting operation?*

Approximately 997m to Abattoir.

*What is between the subject site and the emitting operation?*

A number of dwellings, sports grounds and business premises.

b) nature of the emitting operation being protected by the attenuation area:

*What emissions does the operation produce? (noise and odours etc).*

Noise and odours.

*Are these emissions prevalent at this site?*

Nil due to distance.

*If so, how do the emissions affect the subject site?*

.....  
.....

**degree of hazard or pollution that may emanate from the emitting operation:**

*Are the emission produced having negative effects on the site?*

No.

*Is the degree of impact at the site increased, lessened or the same as a result of the structure?*

No change as the area of the site within the attenuation will continue to be utilised as a driveway only. No new sensitive use is proposed within the attenuation distance.

**c) the measures within the proposal to mitigate impacts of the emitting activity to the sensitive use:**

*Are there any manmade or natural buffers offered on site, or in the surrounding area, that may reduce the impact of the emitting operation? (i.e. distance of residential development between the subject site and emitting operation)*

The separation distance is approximately 997m and the area of 3m of the site within the attenuation area is driveway, therefore there is no impact from the emitting operation on the proposal.

Signed:



Date: 21 November 2019

# PLAN OF SUBDIVISION

**REF:**  
179-50  
(7911)

**DISCLAIMER:** This is a preliminary plan prepared without field survey and forms part of an application to subdivide the land described and is not to be used for any other purpose. Contours and levels may be transcribed from other sources and their accuracy has not been verified. These should not be used. The dimensions, area, location of improvements and number of lots are approximate and may vary as a result of decisions by the Municipality, Land Use Planning Review Panel, engineering or other advice. Easements are not shown as these are to be determined at the time of survey. The plan is not to be copied unless this note is included.

**COHEN & ASSOCIATES P/L**  
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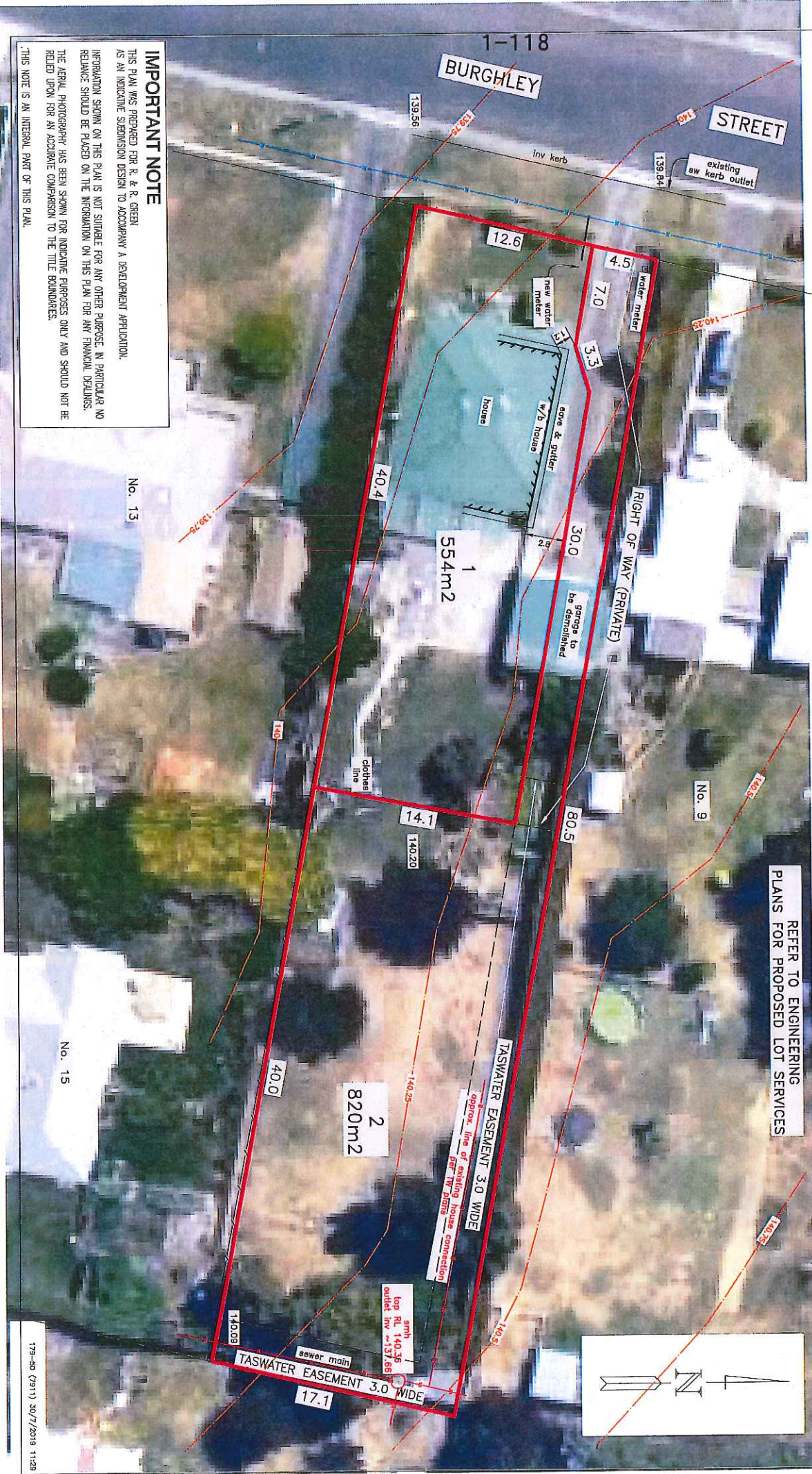


APR 70 689 298 435

**Municipality:** NORTHERN MIDLANDS  
**Site Address:** 11 BURGHELY STREET, LONGFORD

**Owners:** R.M. & R.J. GREEN  
**Title Refs:** 32979-1  
**Dates:** REV 3: 02-12-2019  
**Scale:** 1 : 250 @ A3

**REFER TO ENGINEERING PLANS FOR PROPOSED LOT SERVICES**



**IMPORTANT NOTE**

THIS PLAN WAS PREPARED FOR R. & R. GREEN AS AN INDICATIVE SUBDIVISION DESIGN TO ACCOMPANY A DEVELOPMENT APPLICATION. INFORMATION SHOWN ON THIS PLAN IS NOT SUITABLE FOR ANY OTHER PURPOSE. IN PARTICULAR NO RELIANCE SHOULD BE PLACED ON THE INFORMATION ON THIS PLAN FOR ANY FINANCIAL DECISIONS. THE AERIAL PHOTOGRAPHY HAS BEEN SHOWN FOR INDICATIVE PURPOSES ONLY AND SHOULD NOT BE RELED UPON FOR AN ACCURATE COMPARISON TO THE TITLE BOUNDARIES.  
THIS NOTE IS AN INTEGRAL PART OF THIS PLAN.

No. 13

No. 9

No. 15



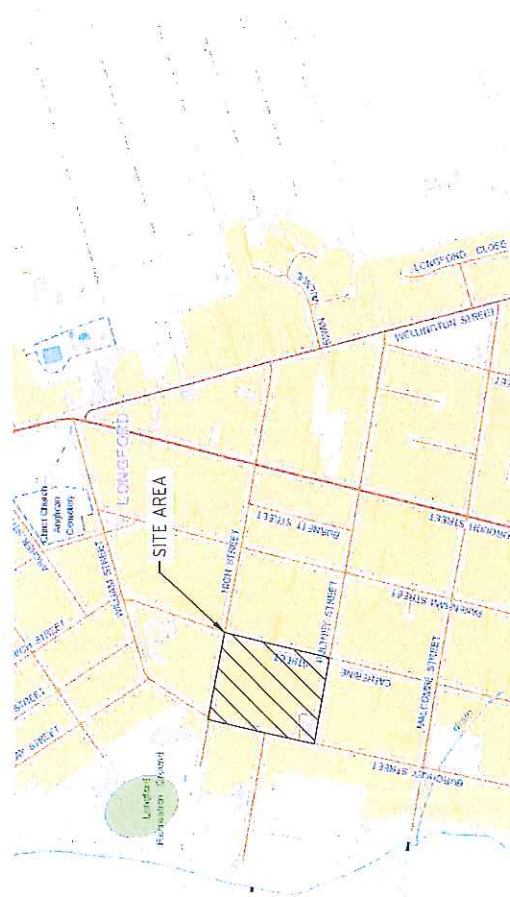
# 2 LOT SUBDIVISION

## 11 BURGHELEY STREET, LONGFORD

FOR  
RM & RJ GREEN

1-119

DRAWING INDEX	
DRAWING NUMBER	DRAWING TITLE
1824-01	COVER SHEET
1824-02	GENERAL NOTES
1824-03	GENERAL LAYOUT
1824-04	SEWER LONG SECTION



LOCALITY PLAN  
SCALE - N.T.S.

**WARNING**  
BEWARE OF UNDERGROUND SERVICES  
THE LOCATION OF UNDERGROUND SERVICES ARE APPROXIMATE ONLY AND THE EXACT POSITION SHOULD BE PROVEN ON SITE. NO GUARANTEE IS GIVEN THAT ALL SERVICES ARE SHOWN.



DO NOTE SCALE	DATE	DESCRIPTION
DIMENSIONS IN MILLIMETERS DRAWING PRACTICES TO AS100 - 1992	TC 08.11.2019	
THIS DRAWING IS THE PROPERTY OF IPD CONSULTING. IT IS CONFIDENTIAL AND MUST NOT BE LOANED, COPIED OR REPRODUCED IN WHOLE OR IN PART WITHOUT PRIOR WRITTEN CONSENT OF THE COMPANY.	CHECKED	NZ 08.11.2019
	DESIGNED	TC 08.11.2019
	DESIGN APP.	NZ 08.11.2019
	APPROVED	M/AW 11.11.2019

PROJECT NAME	1824 - R. GREEN
DRAWING TITLE	11 BURGHELEY STREET, LONGFORD
COVER SHEET	
SCALE AT A3	N.T.S.
DRAWING NUMBER	1824 - 001
SHEET	1 OF 4
DISCIPLINE	CI
REVISION	A

**IPD CONSULTING**

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PRELIMINARY

TABLE 1 - TRENCH FILL MATERIALS AND COMPACTION REQUIREMENTS

TRENCH ZONE	TRAFFICABLE		NON-TRAFFICABLE	
	MATERIAL REQUIREMENT	MIN. COMPACTION REQUIREMENT	MATERIAL REQUIREMENT	MIN. COMPACTION REQUIREMENT
EMBEDMENT	GRADED GRANULAR EMBEDMENT MATERIAL COMPLYING WITH AS2566	NON-COHESIVE MATERIAL (GRAVEL) - DENSITY INDEX MIN. 70% (AS1289.5.6.1)	GRADED GRANULAR EMBEDMENT MATERIAL COMPLYING WITH AS2566	NON-COHESIVE MATERIAL (GRAVEL) - DENSITY INDEX MIN. 60% (AS1289.5.6.1)
TRENCH FILL	GRADED GRANULAR EMBEDMENT MATERIAL COMPLYING WITH AS2566	NON-COHESIVE MATERIAL (GRAVEL) - DENSITY INDEX MIN. 70% (AS1289.5.6.1)	GRADED GRANULAR EMBEDMENT MATERIAL COMPLYING WITH AS2566	NON-COHESIVE MATERIAL (GRAVEL) - DENSITY INDEX MIN. 60% (AS1289.5.6.1)
SURFACE ZONE	PAVEMENT REINSTATEMENT - MATERIALS COMPLYING WITH DIER SPECIFICATIONS, DEPTHS EQUAL TO EXISTING.	REFER DIER SPECIFICATIONS	TOPSOIL NOMINALLY 150 THICK	LIGHTLY COMPACTED
MIN. PIPE COVER	900		600	

1-120

- GENERAL PIPE WORK NOTES:
- WRITTEN DIMENSIONS TAKE PRECEDENCE OVER SCALED, IMPLIED OR ANY OTHER DIMENSION.
  - CONFIRM THE LOCATION AND EXTENT OF ALL ABOVE GROUND AND UNDERGROUND SERVICES PRIOR TO SETOUT AND COMMENCEMENT OF WORKS. ALL SERVICES MAY NOT BE SHOWN ON THE DESIGN DRAWINGS. IT IS THE CONTRACTORS RESPONSIBILITY TO IDENTIFY AND LOCATE ALL SERVICES. PIPE MATERIAL, FITTINGS AND CLASS TO BE AS SPECIFIED ON THE DESIGN DRAWINGS.
  - MINIMUM WIDTH OF TRENCH IS THE WIDTH OF UNSUPPORTED TRENCH OR THE CLEAR WIDTH INSIDE A TRENCH SUPPORT SYSTEM.
  - SIDES OF TRENCH TO BE KEPT VERTICAL TO A MINIMUM OF 150mm ABOVE PIPE.
  - OVER EXCAVATION OF TRENCHES SHALL BE BACKFILLED AND COMPACTED TO BE IN ACCORDANCE WITH THE REQUIREMENTS FOR THE EMBEDMENT ZONE.
  - GEOTEXTILE FABRIC TO SURROUND EMBEDMENT ZONE TO PREVENT THE MIGRATION OF FINES WHERE REQUIRED BY AS2566 OR DIRECTED BY THE DESIGN ENGINEER. GEOTEXTILE FABRIC TO COMPLY WITH THE REQUIREMENTS OF AS2566. PIPES AND FITTINGS TO BE LAID, INSTALLED AND JOINTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS AND AS2566.
  - MINIMUM CLEARANCE AT CROSSINGS OF EXISTING SERVICES TO BE 200mm.
  - DETECTABLE MARKER TAPE TO BE LAID ON TOP OF PIPE EMBEDMENT ZONE OVER ALL NON-METALLIC PIPELINES. DETECTABLE MARKER TAPE TO BE ATTACHED TO METAL SURFACE FITTINGS TO PROVIDE CONNECTION POINTS FOR LOCATING DEVICES.
  - CONFIRM ALL PIPE LENGTHS ON SITE PRIOR TO CUTTING OR FABRICATION.
  - ALL PIPEWORK AND ASSOCIATED STRUCTURES SHALL BE UNDERTAKEN IN ACCORDANCE WITH THE RELEVANT WSAA CODES.
  - ALL MATERIALS, EQUIPMENT, DESIGN AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF ALL RELEVANT AUSTRALIAN STANDARDS, THE BUILDING CODE OF AUSTRALIA, AND ALL RELEVANT ACTS, REGULATIONS AND CODES OF PRACTICE.
  - BASE SURVEY PROVIDED BY COHEN AND ASSOCIATES PTY. LTD.
  - TYPE A INSPECTION SHAFT IN ACCORDANCE WITH WSAA CODE STANDARD DRAWINGS MRWA-S-300 TO MRWA-S-306.
  - REUNDANT SEWER MHS AND PIPES TO BE DECOMMISSIONED IN ACCORDANCE WITH CLAUSE 12.6 OF THE TASWATER SEWER CODE SUPPLEMENT.
- GENERAL NOTES:
- ANY AND ALL WORKS ON TASWATER ASSETS, INCLUDING LIVE CUT-IN OF SEWER MUST BE COMPLETED BY TASWATER (OR THEIR DELEGATED CONTRACTOR), AT THE DEVELOPERS COST.
  - ALL TASWATER INFRASTRUCTURE TO BE TESTED AND INSPECTED BY TASWATER PRIOR TO BACKFILL.
  - PRODUCTS USED ON SITE ARE TO BE AS PER THE APPROVED PRODUCTS CATALOGUE (REFER TO WWW.MRWA.COM.AU/PAGES/PRODUCTS.ASPX) FOR CITY WEST WATER.
  - MINIMUM PIPE COVER TO BE AS PER TABLE 4.8 OF WSA-02-2014-3.1 MRWA EDITION V2.0.
  - SEWER INSPECTION OPENING ARE TO BE IN ACCORDANCE WITH STANDARD DRAWINGS MRWA-S-300 TO MRWA-S-314 OF THE SEWERAGE CODE OF AUSTRALIA.
  - MAINTENANCE STRUCTURE LOCATIONS TO BE IN ACCORDANCE WITH 6.2 OF THE SEWERAGE CODE OF AUSTRALIA.
  - REDUCED LEVELS INDICATIVE ONLY AND SUBJECT TO CHANGE. LEVELS TO BE CONFIRMED ON SITE AND CHECKED WITH SUPERINTENDENT.

- SHALL EITHER BE:
- REMOVED IF LESS THAN 1 M DEEP TO OBVERT, OR
  - FULLY FILLED WITH FLOWABLE GROUT.
- ALL SEWER SUPPLY CONSTRUCTION TO: SEWERAGE SUPPLY CODE OF AUSTRALIA (WSA 02-2014-3.1) - PART 2: CONSTRUCTION.
  - PROVIDE ELECTROMAGNETIC METAL IMPREGNATED TAPE IN ALL NON-METALLIC PIPE TRENCHES. ENSURE TAPE TERMINATIONS ARE ACCESSIBLE.
  - NEW PIPEWORK SHALL BE:
    - 20.1 AS SPECIFIED ON SEWER LONG SECTIONS.
    - 20.2 PROPERTY CONNECTIONS: LESS THAN 9.5M TO BOUNDARY - 100DIA. PVC(SN10) RRR AND IN ACCORDANCE WITH MRWA-S-300 TO MRWA-S-306 INCLUDING INSPECTION OPENINGS AS SHOWN.

**WARNING**  
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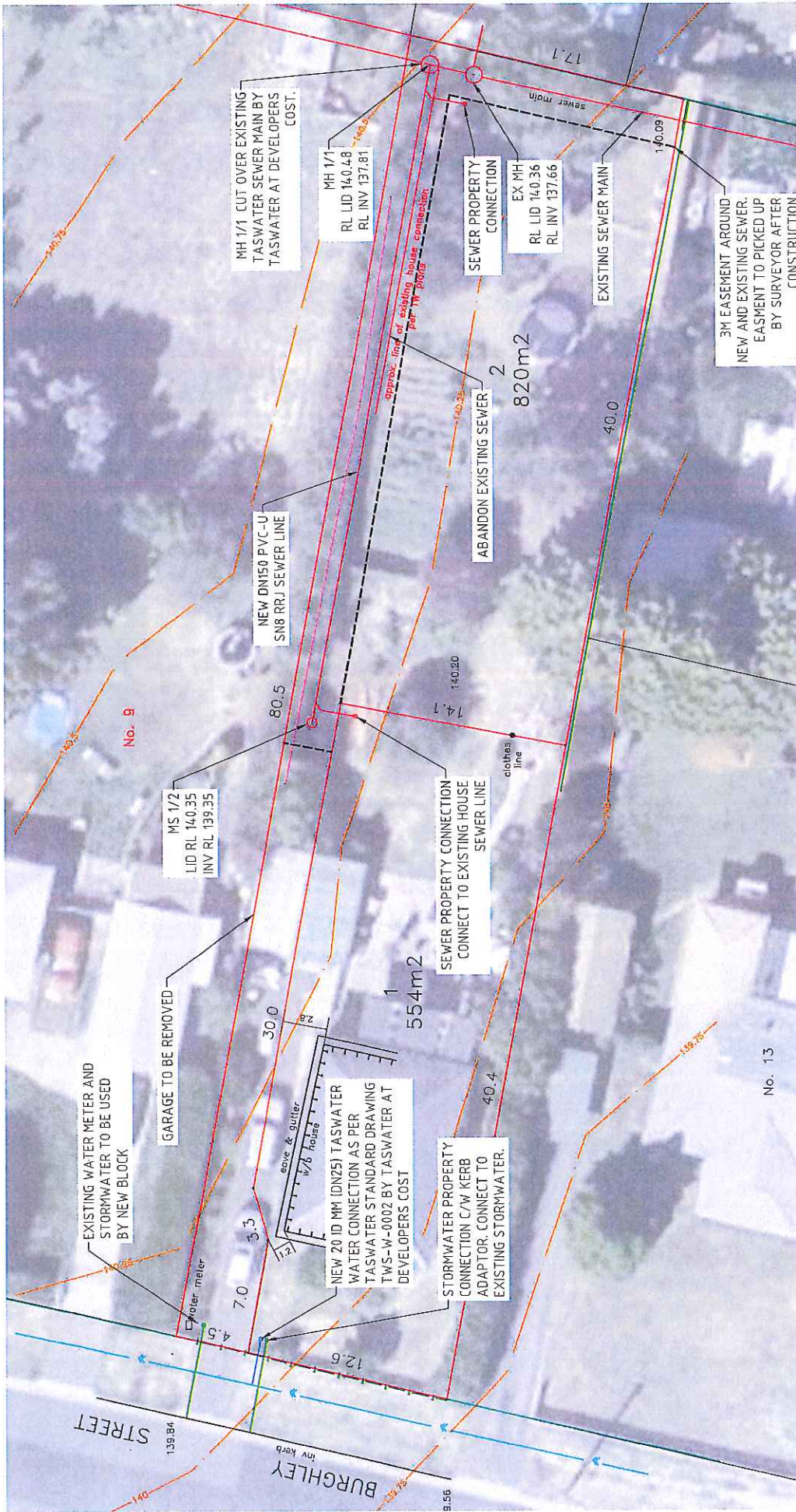
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08.11.2019	TC	DRN	1:1	DRAWING PRACTICES TO AS1100 - 1992
08.11.2019	NZ	CHK	1:1	THIS DRAWING IS THE PROPERTY OF IPD CONSULTING. IT IS CONFIDENTIAL AND MUST NOT BE LOANED, COPIED OR REPRODUCED IN WHOLE OR IN PART WITHOUT PRIOR WRITTEN CONSENT OF THE COMPANY.
08.11.2019	NZ	DESIGN APP.	1:1	
11.11.2019	MAW	APPROVED	1:1	

REV	DATE	DESCRIPTION	DRN	CHK
A	11.11.2019	PRELIMINARY	TC	NZ

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PROJECT NAME  
1824 - R. GREEN  
DRAWING TITLE  
11 BURGHELEY STREET, LONGFORD  
GENERAL NOTES  
SCALE AT A3  
N.T.S  
DRAWING NUMBER  
1824 - 002  
SHEET  
2 OF 4  
DISCIPLINE  
CI  
REVISION  
A

PRELIMINARY



SERVICE PLAN  
SCALE 1:250

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DO NOTE SCALE	DATE	DRAWN	CHECKED	DESIGNED	DESIGN APP.	APPROVED
DIMENSIONS IN MILLIMETERS DRAWING PRACTICES TO AS1100 - 1992	TC 08.11.2019	TC 08.11.2019	NZ 08.11.2019	TC 08.11.2019	NZ 08.11.2019	MAW 11.11.2019

11.11.2019 PRELIMINARY SCALE 1:250

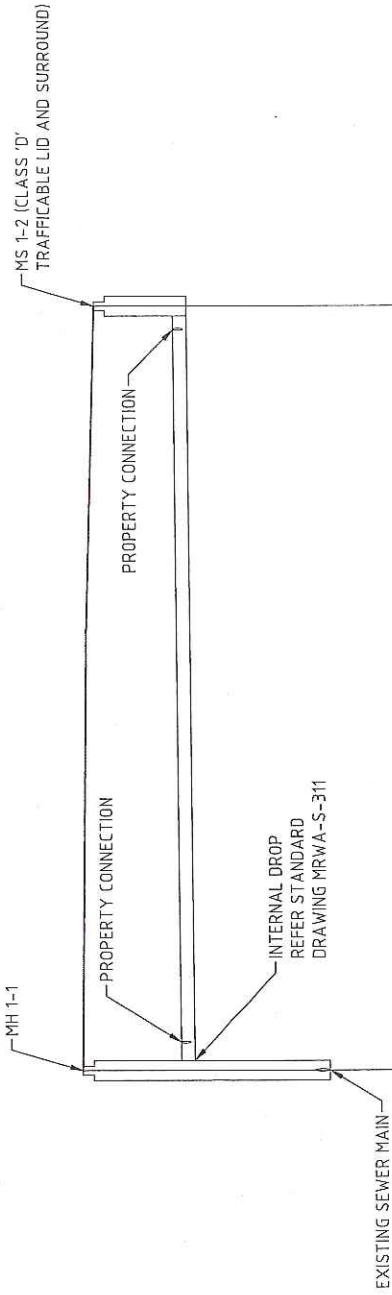
REV	DATE	DESCRIPTION	DRN	CHK
A	11.11.2019	PRELIMINARY	TC	NZ

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PROJECT NAME  
1824 - R. GREEN  
DRAWING TITLE  
11 BURGHELEY STREET, LONGFORD  
GENERAL LAYOUT  
SCALE AT A3  
DRAWING NUMBER  
N.T.S. 1824 - 003

PRELIMINARY

SHEET	DISCIPLINE	REVISION
3 OF 4	CI	A



COMPACTED FOR BACKFILL AS PER TSD-G01

DATUM R.L.	135.00								
PIPELINE GRADE		1 in 100	1.00%						
DIAMETER & TYPE		DN150 uPVC RRR CLASS SN8							
DEPTH TO INVERT FROM FSL	2.67	1.54							
PIPE INVERT LEVEL	137.81	138.94							
EXISTING SURFACE	140.48								
CHAINAGE	0.000						(40.70)		

SEWER LINE LONGSECTION

VERTICAL SCALE 1:100  
HORIZONTAL SCALE 1:500

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BEWARE OF UNDERGROUND SERVICES  
THE LOCATION OF UNDERGROUND SERVICES ARE APPROXIMATE ONLY AND THE EXACT POSITION SHOULD BE PROVEN ON SITE. NO GUARANTEE IS GIVEN THAT ALL SERVICES ARE SHOWN.



DO NOTE SCALE DIMENSIONS IN MILLIMETERS  
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DRAWN	CHECKED	DESIGNED	DESIGN APP.	DATE
TC	NZ	TC	NZ	08.11.2019
				08.11.2019
				08.11.2019
				11.11.2019

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SCALE 1:25

REV	DATE	DESCRIPTION	DRN	CHK	APPROVED	M/A/W
A	11.11.2019	PRELIMINARY	TC	NZ		

**IPD CONSULTING**  
ABN: 96 021 714 878  
LEVEL 2, 126 CHARLES STREET  
LAUNCESTON, TASMANIA  
P.O. BOX 1371 LAUNCESTON T.A.S. 7250  
PHONE: 0459 574 975  
EMAIL: admin@ipdconsulting.com.au

PROJECT NAME  
1824 - R. GREEN  
DRAWING TITLE  
11 BURGHELEY STREET, LONGFORD  
SEWER LONG SECTION  
SCALE AT A3  
N.T.S  
DRAWING NUMBER  
1824 - 004

PRELIMINARY

REVISION	DISCIPLINE	SHEET	NO.
A	CI	4 OF 4	

## Submission to Planning Authority Notice

Council Planning Permit No.	PLN-19-0231	Council notice date	26/11/2019
<b>TasWater details</b>			
TasWater Reference No.	TWDA 2019/01744-NMC	Date of response	2/12/2019
TasWater Contact	David Boyle	Phone No.	6345 6323
<b>Response issued to</b>			
Council name	NORTHERN MIDLANDS COUNCIL		
Contact details	Planning@nmc.tas.gov.au		
<b>Development details</b>			
Address	11 BURGHEY ST, LONGFORD	Property ID (PID)	6729265
Description of development	Subdivision - 2 lots		
<b>Schedule of drawings/documents</b>			
Prepared by	Drawing/document No.	Revision No.	Date of Issue
IPD Consultants	Subdivision Plan/1824-003 Sh 3	A	11/11/2019
<b>Conditions</b>			
<p>Pursuant to the <i>Water and Sewerage Industry Act 2008 (TAS)</i> Section 56P(1) TasWater imposes the following conditions on the permit for this application:</p>			
<b>CONNECTIONS, METERING &amp; BACKFLOW</b>			
<ol style="list-style-type: none"> <li>1. A suitably sized water supply with metered connections / sewerage system and connections to each lot of the development must be designed and constructed to TasWater's satisfaction and be in accordance with any other conditions in this permit.</li> <li>2. Any removal/supply and installation of water meters and/or the removal of redundant and/or installation of new and modified property service connections must be carried out by TasWater at the developer's cost.</li> <li>3. Prior to commencing construction of the subdivision, any water connection utilised for construction must have a backflow prevention device and water meter installed, to the satisfaction of TasWater.</li> </ol>			
<b>ASSET CREATION &amp; INFRASTRUCTURE WORKS</b>			
<ol style="list-style-type: none"> <li>4. Plans submitted with the application for Engineering Design Approval must, to the satisfaction of TasWater show, all existing, redundant and/or proposed property services and mains.</li> <li>5. Prior to applying for a Permit to construct new infrastructure the developer must obtain from TasWater Engineering Design Approval for new TasWater infrastructure. The application for Engineering Design Approval must include engineering design plans prepared by a suitably qualified person showing the hydraulic servicing requirements for sewerage to TasWater's satisfaction.</li> <li>6. Prior to works commencing, a Permit to Construct must be applied for and issued by TasWater. All infrastructure works must be inspected by TasWater and be to TasWater's satisfaction.</li> <li>7. In addition to any other conditions in this permit, all works must be constructed under the supervision of a suitably qualified person in accordance with TasWater's requirements.</li> <li>8. Prior to the issue of a Consent to Register a Legal Document all additions, extensions, alterations or upgrades to TasWater's water and sewerage infrastructure required to service the development, generally as shown on the concept servicing plan "IPD Consultants Subdivision Plan/1824-003 Sh 3",</li> </ol>			

are to be constructed at the expense of the developer to the satisfaction of TasWater, with live connections performed by TasWater.

9. After testing, to TasWater's requirements, of newly created works, the developer must apply to TasWater for connection of these works to existing TasWater infrastructure, at the developer's cost.
10. At practical completion of the water and sewerage works and prior to TasWater issuing a Consent to a Register Legal Document the developer must obtain a Certificate of Practical Completion from TasWater for the works that will be transferred to TasWater. To obtain a Certificate of Practical Completion:
  - a. Written confirmation from the supervising suitably qualified person certifying that the works have been constructed in accordance with the TasWater approved plans and specifications and that the appropriate level of workmanship has been achieved;
  - b. A request for a joint on-site inspection with TasWater's authorised representative must be made;
  - c. Security for the twelve (12) month defects liability period to the value of 10% of the works must be lodged with TasWater. This security must be in the form of a bank guarantee;
  - d. As constructed drawings must be prepared by a suitably qualified person to TasWater's satisfaction and forwarded to TasWater.
11. After the Certificate of Practical Completion has been issued, a 12 month defects liability period applies to this infrastructure. During this period all defects must be rectified at the developer's cost and to the satisfaction of TasWater. A further 12 month defects liability period may be applied to defects after rectification. TasWater may, at its discretion, undertake rectification of any defects at the developer's cost. Upon completion, of the defects liability period the developer must request TasWater to issue a "Certificate of Final Acceptance". The newly constructed infrastructure will be transferred to TasWater upon issue of this certificate and TasWater will release any security held for the defects liability period.
12. The developer must take all precautions to protect existing TasWater infrastructure. Any damage caused to existing TasWater infrastructure during the construction period must be promptly reported to TasWater and repaired by TasWater at the developer's cost.
13. Ground levels over the TasWater assets and/or easements must not be altered without the written approval of TasWater.

#### **FINAL PLANS, EASEMENTS & ENDORSEMENTS**

14. Prior to the Sealing of the Final Plan of Survey, a Consent to Register a Legal Document must be obtained from TasWater as evidence of compliance with these conditions when application for sealing is made.  
*Advice: Council will refer the Final Plan of Survey to TasWater requesting Consent to Register a Legal Document be issued directly to them on behalf of the applicant.*
15. Pipeline easements, to TasWater's satisfaction, must be created over any existing or proposed TasWater infrastructure and be in accordance with TasWater's standard pipeline easement conditions.

#### **DEVELOPMENT ASSESSMENT FEES**

16. The applicant or landowner as the case may be, must pay a development assessment and Consent to Register a Legal Document fee to TasWater, as approved by the Economic Regulator and the fees will be indexed, until the date they are paid to TasWater, as follows:
  - a. \$211.63 for development assessment; and




b. \$149.20 for Consent to Register a Legal Document  
 The payment is required by the due date as noted on the statement when issued by TasWater.

**Advice**

**General**  
 For information on TasWater development standards, please visit  
<https://www.taswater.com.au/Development/Technical-Standards>  
 For application forms please visit <http://www.taswater.com.au/Development/Forms>

**Declaration**

The drawings/documents and conditions stated above constitute TasWater’s Submission to Planning Authority Notice.

**Authorised by**  
  
**Jason Taylor**  
 Development Assessment Manager

**TasWater Contact Details**

Email	development@taswater.com.au	Web	www.taswater.com.au
Mail	GPO Box 1393 Hobart TAS 7001		

REFERRAL OF DEVELOPMENT APPLICATION PLN-19-0231 TO WORKS & INFRASTRUCTURE DEPARTMENT

Property/Subdivision No: 101800.06  
 Date: 26 November 2019  
 Applicant: Rebecca Green & Associates  
 Proposal: 2 Lot subdivision  
 Location: 11 Burghley Street, Longford

W&I referral PLN-19-0231, 11 Burghley Street, Longford

Planning admin: W&I fees paid.

**Jonathan - if you require further information, advise planning section as soon as possible – there are only 14 days from receipt of permitted applications and 21 days from receipt of discretionary applications to stop the clock.**

Please inspect the property and advise regarding stormwater/drainage, access, traffic, and any other engineering concerns.

Is there is a house on one of the lots?	Yes
Is it connected to all Council services?	Yes
Are any changes / works required to the house lot?	Yes. Needs new driveway
Are the discharge points for stormwater, infrastructure that is maintained by Council? (This requires a check to ensure the downstream infrastructure is entirely owned, maintained, operated by Council and have been taken over as Council assets.)	Yes

**Stormwater:**

Does the physical location of stormwater services match the location shown on the plan? (Requires an on-site inspection)	Yes
Is the property connected to Council's stormwater services?	Yes
If so, where is the current connection/s?	Kerb on Burghley Street
Can all lots access stormwater services?	Yes
If so, are any works required?	Yes. House will need new Stormwater connection.
Is stormwater detention required	No
Has a stormwater detention design been submitted	NA
If so, is it designed for 20- year ARI with overland flow path to road or any other low risk Council approved place of discharge.	NA
If no to above , has the design for 100 – year ARI been done.	NA
If yes to any of the above, does it comply with Councils stormwater policy	NA
Is the design approved by works & infrastructure	NA
Please quote drawing numbers and any other relate documentation (email etc.)	NA
Additional Comments/information	NA

**Stormwater works required:**

Works to be in accordance with Standard Drawing TSD-R09 – a 100mm stormwater connection.



<i>Multiple Dwellings: Works to be in accordance with Standards – a 150mm stormwater connection</i>	
Is there kerb and gutter at the front of the property?	Yes
Are any kerb-and-gutter works required?	No

**Road Access:**

Does the property have access to a made road?	Yes
If so, is the existing access suitable?	Yes
Does the new lot/s have access to a made road?	Yes
If so, are any works required?	Yes, see below.
Is off-street parking available/provided?	Yes

**Road / access works required:**

*Works to be in accordance with Standard Drawing TSD-R09 - concrete driveway crossover & hotmix sealed apron from the edge of Burghley Street to the property boundary of Lot/s 1 & 2.*

Is an application for vehicular crossing form required?	Yes
Is a footpath required?	No
Extra information required regarding driveway approach and departure angles	No
Are any road works required?	No
Are street trees required?	No
Additional Comments:	An Engineer's design is <b>not</b> required.

Engineer's comment:

Council services for this subdivision can be addressed by standard conditions.

**WORKS & INFRASTRUCTURE DEPARTMENT CONDITIONS****STANDARD CONDITIONS FOR SMALL SUBDIVISIONS**W.1 Stormwater

Each lot must be provided with a connection to the Council's stormwater system, constructed in accordance with Council standards and to the satisfaction of Council's Works & Infrastructure Department.

W.2 Access (Urban)

- a) A concrete driveway crossover and hotmix sealed apron must be constructed from the edge of Burghley Street to the property boundary of each lot in accordance with Council standards and TSD-R09.
- b) Access works must not commence until an application for vehicular crossing has been approved by Council.

W.3 As constructed information

As Constructed Plans and Asset Management Information must be provided in accordance with Council's standard requirements.

W.4 Municipal standards & certification of works

Unless otherwise specified within a condition, all works must comply with the Municipal Standards including specifications and standard drawings. Any design must be completed in accordance with Council's subdivision design guidelines to the satisfaction of the Works & Infrastructure Department. Any construction, including maintenance periods, must also be completed to the approval of the Works & Infrastructure Department.

W.5 Works in Council road reserve

- a) Works must not be undertaken within the public road reserve, including crossovers, driveways or kerb and guttering, without prior approval for the works by the Works Manager.
- b) Twenty-four (24) hours notice must be given to the Works & Infrastructure Department to inspect works within road reserve, and before placement of concrete or seal. Failure to do so may result in rejection of the vehicular access or other works and its reconstruction.

W.6 Separation of hydraulic services

- a) All existing pipes and connections must be located.
- b) Where required, pipes are to be rerouted to provide an independent system for each lot.
- c) Certification must be provided that services have been separated between the lots.

W.8 Pollutants

- a) The developer/property owner must ensure that pollutants such as mud, silt or chemicals are not released from the site.
- b) Prior to the commencement of the development authorised by this permit the developer/property owner must install all necessary silt fences and cut-off drains to prevent soil, gravel and other debris from escaping the site. Material or debris must not be transported onto the road reserve (including the nature strip, footpath and road pavement). Any material that is deposited on the road reserve must be removed by the developer/property owner. Should Council be required to clean or carry out works on any of their infrastructure as a result of pollutants being released from the site the cost of these works may be charged to the developer/property owner.

W.9 Nature strips

Any new nature strips, or areas of nature strip that are disturbed during construction, must be topped with 100mm of good quality topsoil and sown with grass. Grass must be established and free of weeds prior to Council accepting the development.

*Jonathan Galbraith (Engineering Officer)*

*Jake Walley (Engineering Officer)*

*Date: 26/11/2019*

Pamela & Peter Coleman,  
15 Burghley Street,  
Longford Tas. 7301.

18<sup>th</sup> December, 2019.

Mr. Des Jennings,  
General Manager,  
Northern Midlands Council,  
Smith Street,  
Longford Tas 7301.

NORTHERN MIDLANDS COUNCIL					
Location					
File No.					
Property					
Attachments					
REC'D 19 DEC 2019					
	I	A		I	A
GM			MYR		
P&DM			CHS		
GSL			PLAN		
ERDM			BLD		
WM			HLT		
HR					

Dear Mr. Jennings,

**Re: Proposed subdivision at 11 Burghley Street, Longford. PLN-19-0231**  
Prepared by Rebecca Green & Associates for: Rebecca Green & Richard Martin

We wish to lodge our formal objection to the proposed subdivision at 11 Burghley Street, Longford. This property is adjacent to our property at 15 Burghley Street.

The attraction to purchasing our property at 15 Burghley Street was principally the small town ambience and sense of privacy afforded by the large block and those adjacent to it. Our outdoor area and living area, which are the most commonly used areas of our property, face north directly onto the proposed new residential site at 11 Burghley Street. The sub-division of large private blocks such as these will diminish the appeal of Longford for new residents. We respectfully submit that preference should be given to ensuring the maintenance of the "small town" way of life, and the quiet enjoyment by residents of their property over financial gain by a minority of land owners.

We would also like to express our concerns regarding any storm water impacting on our block as we already suffer from problems with water run-off.

In the event that a sub-division is approved, we would like the Council to note our strong objection to the construction of anything other than a single storey dwelling on the new residential land. We also request that we are notified in the event of a building proposal for this site. We believe that the proposed sub-division and any subsequent development on this site will reduce the value of our property.

**We draw your attention to the relevant sections of the Northern Midlands Interim Planning Scheme 2013:**

### 3.1 Social and Economic Context

There is a strong attachment to the existing scale, lifestyle, character and values expressed through the physical environment of our towns and villages.

### 10.4.2 Setbacks and building envelope for all dwellings

Objective: To control the siting and scale of dwellings to:

- (c) provide consistency in the apparent scale, bulk, massing and proportion of dwellings; and
- (d) provide separation between dwellings on adjacent sites to provide reasonable opportunity for daylight and sunlight to enter habitable rooms and private open space.

*Performance Criteria 3:*

The siting and scale of a dwelling must:

(a) not cause unreasonable loss of amenity by:

- (i). reduction in sunlight to a habitable room (other than a bedroom) of a dwelling on an adjoining lot; or
- (ii). overshadowing the private open space of a dwelling on an adjoining lot; or
- (iii). overshadowing of an adjoining vacant lot; or
- (iv). visual impacts caused by the apparent scale, bulk or proportions of the dwelling when viewed from an adjoining lot.

**10.4.13.2 Site coverage**

*Objective*

- a) To ensure that the site coverage of residential development respects the existing neighbourhood character or desired future character statements, if any; and
- b) To reduce the impact of increased stormwater run-off on the drainage system.

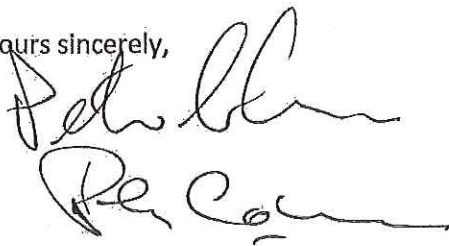
**10.4.15 Subdivision**

**10.4.15.1 Lot Area, Building Envelopes and Frontage**

A1 Lots must:

- e) be to align existing titles with zone boundaries and no additional lots are created.

Yours sincerely,



Pamela & Peter Coleman

To the General Manager of the Northern Midlands Council

Dear Sir,

We are submitting a 'representation' on the proposed '2 lot subdivision' for 11 Burghley St. Longford. Ref. No. PLN-19-0231

We are the owners of 9 Burghley St. Longford.

We are not against the subdivision, but we are concerned of the potential removal of the Hawthorn Hedge between the properties. The hedge offers our property privacy and the potential for another resident overlooking our property concerns us.

Longford has a rich history and many fence lines are lined by Hawthorn hedges, dating back to the first settlement in the area in the early 1800's. The Hawthorn hedge between the properties would date back to the early 1900's.

Regards,

Anthony & Susan Gibbons

265 Bridgenorth Road

Legana

7277

Ph. Anthony 0458 580065

Ph. Susan 0427 137484

**Rosemary Jones**

---

**From:** Rebecca Green <admin@rgassociates.com.au>  
**Sent:** Monday, 23 December 2019 11:28 PM  
**To:** NMC Planning  
**Subject:** RE: Email to applicant, representations received to PLN19-0231, 11 Burghley St Longford.

Good evening Rosemary,

Please see signed agreed extension of time to allow consideration of planning application for subdivision at 11 Burghley Street, Longford to be undertaken at Council's January meeting.

I would like to provide a response to the representations received.

Representation no. 1

The developer's acknowledge the concerns of the author(s) and agree wholeheartedly of the attractiveness of the Hawthorn Hedge, particularly in relation to privacy, that is located between the subject site and the immediately adjacent property to the north. It is not the intention of the subject application to remove the said Hawthorn Hedge. We further note that the subject site is not heritage listed, nor located within a heritage precinct or a Tourist Road Corridor where such hedgerow may have particular importance within the Planning Scheme. We hope that this confirmation of the intent of this application would enable the author to consider withdrawal of their representation potentially?

Representation no. 2

We would like to reiterate that the subdivision of the subject site is permitted and the only two discretions sought relate to the variation to access width and a small portion of the driveway being located within attenuation distance, no further discretions are sought by this application. The proposed lot sizes of both lots complies with acceptable solution 10.4.15.1 a) of the *Northern Midlands Interim Planning Scheme 2013* as the lots exceed the required minimum lot size of 450m<sup>2</sup>. It is noted that subclauses a) through to e) of Clause 10.4.15.1 are OR options, whereby the proposal only needs to comply with one of the options to satisfy the requirements. It is noted that subclause e) would typically apply only to adjustment of boundaries where at least two zones apply, which does not apply in this circumstance. The subdivision therefore meets the intent of the General Residential zone noting that the purpose provides for a range of dwelling types at suburban densities and encourages residential development. We wish to further highlight that the author(s) of this representation in fact live on a lot subdivided in 2004, with a similar internal lot arrangement of the proposed development, therefore the proposal is consistent with the character of the area and the zone.

In response to the concerns of future development possibilities on the proposed vacant lot, the Planning Authority must only consider the application at hand, that is subdivision of the subject land. Any future development of the proposed lots must be considered on their own planning merit and under the provisions of the planning scheme in effect at the time of the making of such application, whether that be a no permit required or discretionary application. No further comment is therefore necessary on this basis and speculation of any future development is not a relevant consideration.

In relation to stormwater concerns, it is noted that the proposed development will create two fully serviced lots, with provision of stormwater connection to the Burghley Street kerb.

In relation to property valuation, this is not a relevant consideration of the planning scheme and has no bearing on the matter at hand.

**1-133**

We hope that the Planning Authority considers the application favourably with respect to the two discretions sought, both of which have been adequately addressed within the original application.

Kind regards

Rebecca Green  
Senior Planning Consultant & Accredited Bushfire Hazard Assessor  
Rebecca Green & Associates  
m. 0409 284422  
P.O. Box 2108, Launceston, 7250