

PLAN 4

PLANNING APPLICATION PLN-20-0269

49 GEORGE STREET, PERTH

ATTACHMENTS

- A Application & plans, correspondence with applicant
- B Responses from referral agencies
- C Representations & applicant's response

2-100
PLANNING APPLICATION
Proposal

Description of proposal: Title Consolidation and Multiple Dwellings

.....
.....
.....
.....
.....

(attach additional sheets if necessary)

If applying for a subdivision which creates a new road, please supply three proposed names for the road, in order of preference:

1..... 2..... 3.....

Site address: 49 George Street, Perth (Access Via Crown)

(Stormwater Connection Via George Street, Perth CT:45677/3)

CT no: 45677/4, 158071/1&2

Estimated cost of project \$3,100,000 (include cost of landscaping, car parks etc for commercial/industrial uses)

Are there any existing buildings on this property? Yes / No
If yes – main building is used as

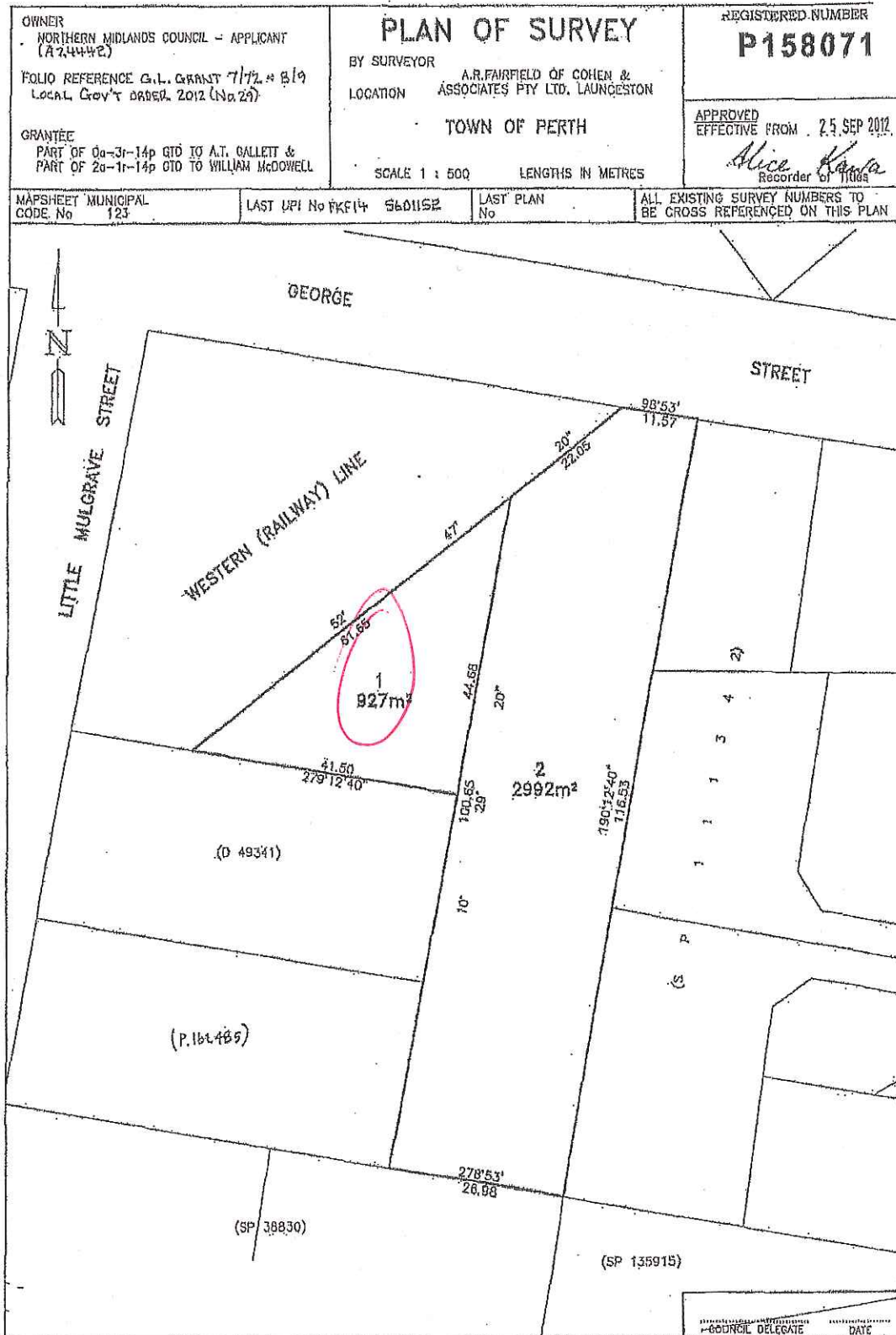
If variation to Planning Scheme provisions requested, justification to be provided:

See attached cover letter
.....
.....
.....
.....

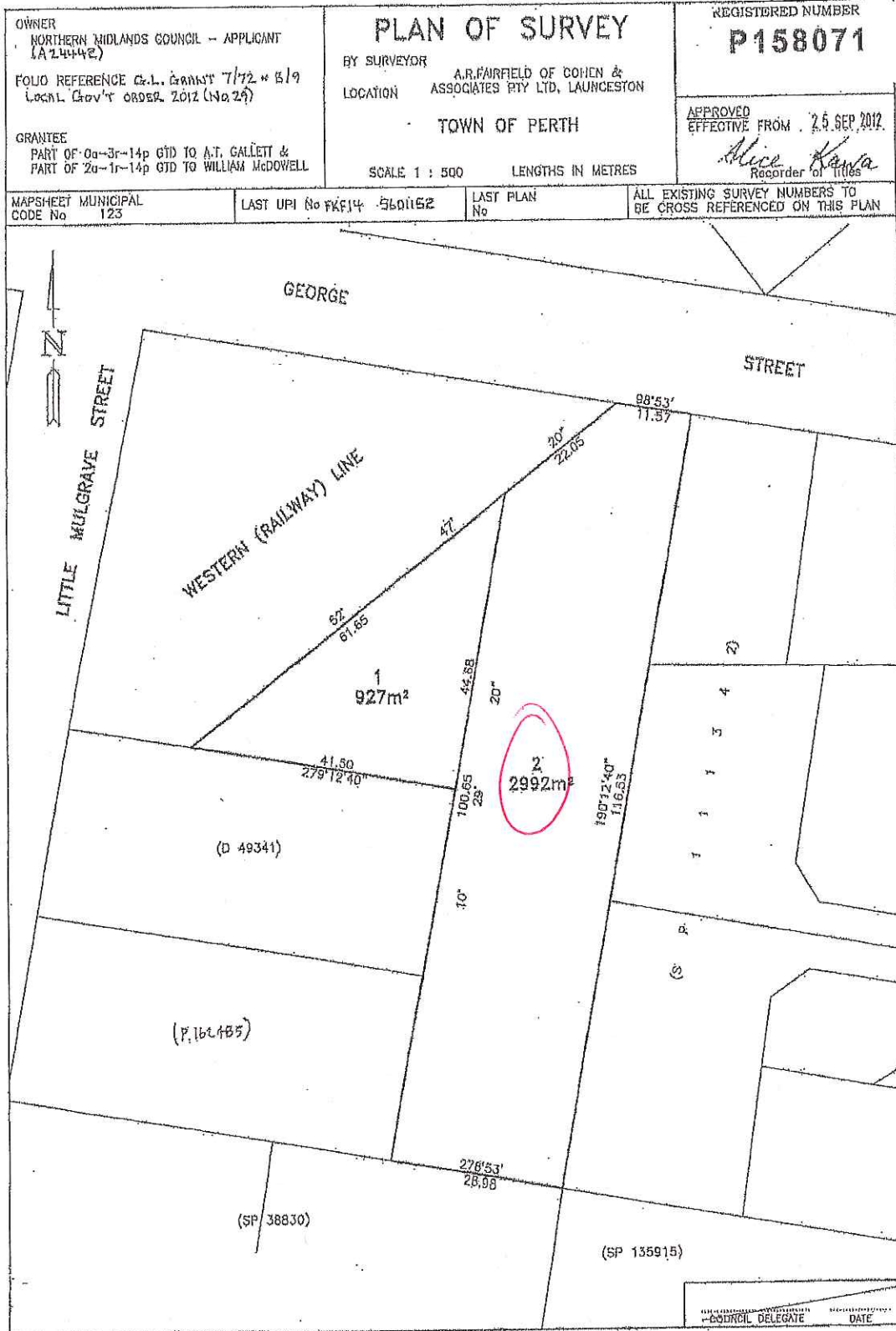
(attach additional sheets if necessary)

Is any signage required? No (if yes, provide details)

EXHIBITED



Handwritten signatures and stamps



EXHIBITED

A.9077

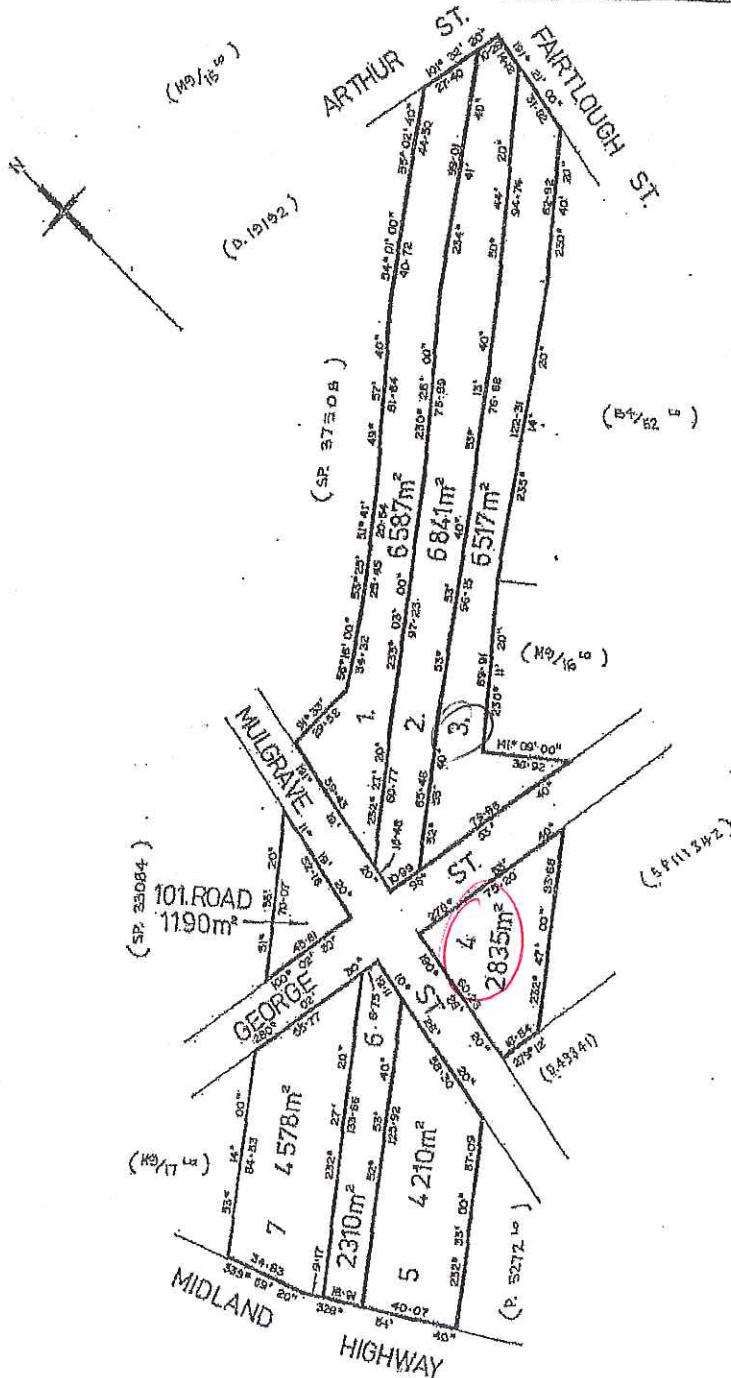
P 45677

Owner: THE AUSTRALIAN NATIONAL RAILWAYS Comm.	PLAN OF SURVEY by Surveyor... BUSH of land situated in the	Registered Number: P45677
Title Reference: CONV. 5/6011 (Lot 100) 5/6132 (Lots 1-3) CROWN LAND (Lots 5-7), LOT 4 - Previous owner unknown	TOWN OF PERTH SECTIONS M,N,P&U	Approved Effective from 10 JUN 1991 <i>Michael Blain</i>
Grant: 2-5077 is VESTED IN THE NAME OF THE AUSTRALIAN NATIONAL RAILWAYS Comm.	SCALE 1:1500 MEASUREMENTS IN METRES	Recorder of Titles

THE WHOLE OF LOT 1, 6587m² LOT 2, 6641m²
LOT 3, 6517m² LOT 4, 2835m² LOT 5, 4210m²
LOT 6, 2310m² LOT 7, 4578m² and LOT 101, 1190m²
VESTED IN THE AUSTRALIAN NATIONAL RAILWAYS
COMMISSION

NOT EXAMINED

TASMANIAN MUNICIPAL	LIST TARIFF	LIST SURVEY PLAN NO.
5/2	LOT NO.	
ALL EXISTING SURVEY INSTRUMENTS TO BE CROSS REF. THIS PLAN		



EXHIBITED

S 134/002

Temp
[Signature]



Department of Primary Industries,
Parks, Water and Environment

GPO Box 1751, Hobart, TAS 7001 Australia
Ph 1300 TAS PARKS / 1300 827 727 Fax 03) 6223 8308
www.parks.tas.gov.au



Enquiries: Tony Ryan
Ph: (03) 6165 3027
Email: propertyservices@parks.tas.gov.au
Our ref: 20/8297

30 March 2021

Mr Lyndon Stubbs
Design to Live Pty Ltd

E: lyndon@designtolive.com.au

Dear Lyndon

**PLANNING APPLICATION
HARLEY DEVELOPMENTS PTY LTD
PROPOSED TITLE CONSOLIDATION AND MULTIPLE STATRUM TITLE DWELLING
DEVELOPMENT AT 49 GEORGE STREET PERTH**

This letter, issued pursuant to section 52(1B) of the *Land Use Planning and Approvals Act 1993*, is to confirm that the Crown consents to the making of the enclosed Planning Permit Application, insofar as the proposed development relates to Crown land managed by the Department of Primary Industries, Parks, Water and Environment.

Crown consent is only given to the lodgement of this application. Any variation will require further consent from the Crown.

This letter does not constitute, nor imply, any approval to undertake works, or that any other approvals required under the *Crown Lands Act 1976* have been granted. If planning approval is given for the proposed development, the applicant will be required to obtain separate and distinct consent from the Crown before commencing any works on Crown land.

If you need more information regarding the above, please contact the officer nominated at the head of this correspondence.

Yours sincerely

A handwritten signature in black ink, appearing to read "Jesse Walker".

Jesse Walker
Team Leader (Unit Manager, Policy & Projects)
Tasmania Parks and Wildlife Service

EXHIBITED



Mail: 202 Wellington St, South Launceston 7249
A.B.N: 71 615 812 747
Phone: 6344 7319
Email: info@designtolive.com.au

30/10/2020

Planning Application Cover letter.

Applicant: Design To Live Pty Ltd.

Development: Proposed Title Consolidation & Proposed Multiple Dwellings

Owner: Harley Commercial Pty Ltd

Address: 49 George Street, Perth, 7300

Zone: General Residential

10.4.2 Setback and building envelope for all dwellings

A1

Relies on performance criteria (P1). The proposed unit development will be accessed from Little Mulgrave Street so this will be considered the primary frontage for the development. Proposed Unit 1 and 5 are located on the primary frontage approx. 3m from the boundary. Whilst this is a lesser distance than the acceptable solution of 4.5m, there are a number of existing dwellings on the same street with a similar or lesser frontage setback than is proposed, including numbers 1,3 & 9 Little Mulgrave street.

On the George street frontage, all units comply with the acceptable solution excluding Unit 15, which is located 2.4m from the boundary (600mm less than the acceptable solution). There is an existing dwelling located at 48 George Street, directly opposite the proposed development that has a setback from the street that is less than 1.0m.

Furthermore, neither George street nor Little Mulgrave Street are through roads, due to the railway network running through the would be intersection of these two roads, meaning there are no adjoining lots in this area that would suffer any loss of amenity with regard to sunlight reduction or overshadowing or visual impacts from the proposed development.

Regards,
Denika McDonald-Hodges (B.EvnDes, M.Arch)

EXHIBITED



DRAWING #	DRAWING
GRGS49-1	COVER PAGE
GRGS49-2	SITE PLAN
GRGS49-3	SITE DETAIL 1
GRGS49-4	SITE DETAIL 2
GRGS49-5	STRATA PLAN
GRGS49-6	GROUND PLAN UNIT TYPE A
GRGS49-7	GROUND PLAN UNIT TYPE B
GRGS49-8	GROUND PLAN UNIT TYPE C
GRGS49-9	EXTERNAL SERVICES
GRGS49-10	ELEVATIONS UNIT TYPE A
GRGS49-11	ELEVATIONS UNIT TYPE B
GRGS49-12	ELEVATIONS UNIT TYPE C
GRGS49-13	SITE SECTIONS
GRGS49-14	SITE SECTIONS
GRGS49-15	LANDSCAPE PLAN
GRGS49-16	PARKING AND TURNING
GRGS49-17	PERSPECTIVES
GRGS49-18	PERSPECTIVES
GRGS49-19	PERSPECTIVES
GRGS49-20	SHADOW DIAGRAM 10.00
GRGS49-21	SHADOW DIAGRAM 12.00
GRGS49-22	SHADOW DIAGRAM 2.00

COUNCIL	ZONE	SITE HAZARDS	TRAIN LINE WITHIN 200M
NORTHERN MIDLANDS	GENERAL RESIDENTIAL	PLANNING OVERLAY	SUBURBANE PROXIMITY URBAN GROWTH
PROPOSED UNIT 1	PROPOSED UNIT 10	LAND TITLE REFERENCE	4567714, 1580711 & 34228985
PROPOSED UNIT 2	PROPOSED UNIT 11	PROPERTY ID	2835, 927 & 2992
PROPOSED UNIT 3	PROPOSED UNIT 12	LOT SIZE (M ²)	TBC
PROPOSED UNIT 4	PROPOSED UNIT 13	ENERGY STAR RATING	TBC
PROPOSED UNIT 5	PROPOSED UNIT 14	BAL RATING	TBC
PROPOSED UNIT 6	PROPOSED UNIT 15	DESIGN WIND CLASS	TBC
PROPOSED UNIT 7		SOIL CLASSIFICATION	TBC
PROPOSED UNIT 8		CLIMATE ZONE	7
PROPOSED UNIT 9		ALPINE AREA	N/A
		CORROSION ENV	N/A

CLIENT/S:	DRAWING COVER PAGE	SIGNATURE:	DATE:
HARRIS DALEY		I/WE APPROVE THESE DRAWINGS TO BE CORRECT PER CONTRACT.	
SITE ADDRESS:		SIGNATURE:	DATE:
49 GEORGE STREET,			

AMENDED

REV.	DATE	DESCRIPTION	DESIGNER	M.L.	JOB NUMBER	GRGS49
R1	30/10/2020	FOR D.A.	DRAWN	L.S.	DRAWING	1/22
R2	17/09/2021	F.I.R.	CHECKED	M.L.	SCALE @A21	NTS

EXHIBITED

NOISE ASSESSMENT - GEORGE ST NOISE / DGF
TRAFFIC IMPACT ASSESSMENT
LS135.001 - CIVIL DESIGN

ATTACHMENTS



MICHELL LYONS
 ACC # 371799313
 ABRN 71 615 812 747
 PH 634 7319

CLIENT/S:
 HARRIS DALEY

SITE ADDRESS:
 49 GEORGE STREET
 CFCAN ATTICE

DRAWING:
 SITE PLAN

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 DATE

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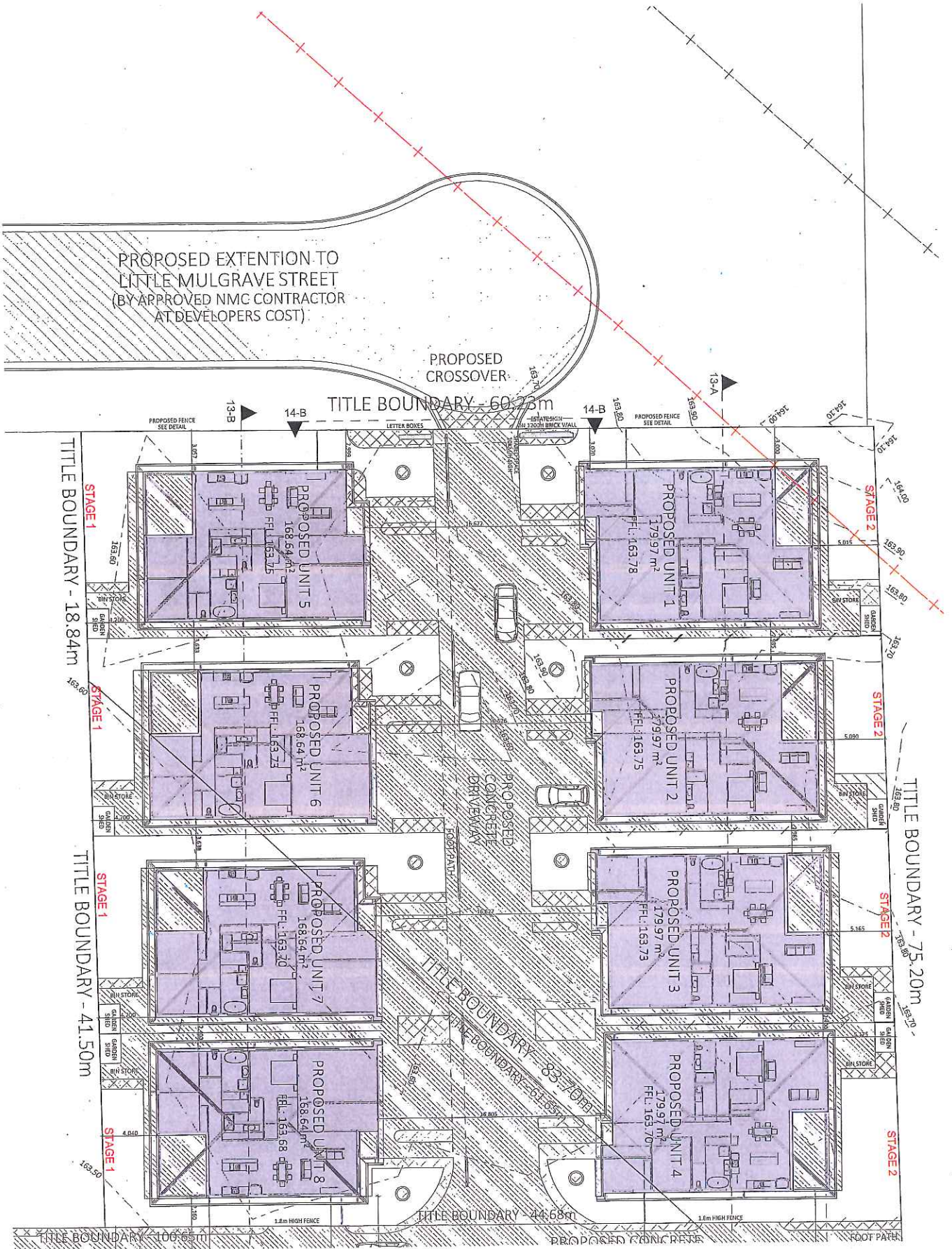
REV.	DATE	DESCRIPTION
R1	30/10/2020	FOR D.A.
R2	17/02/2021	FLR.

DESIGNER	M.L.	JOB NUMBER
DRAWN	L.S.	DRAWING
CHECKED	M.I.	KCA I F (GA7)

GRCS/49
2/22
1:400

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EXHIBITED



MITCHELL LLOYD
 ACC # 371799313
 ABRV: 71 615 812 747
 PH: 6344 7319

CUSTOMER:
 HANRIG DAIRY

SITE ADDRESS:
 49 FRENCHMAN STREET

DRAWING:
 SITE DETAIL 1

DATE:

DATE:

DATE:

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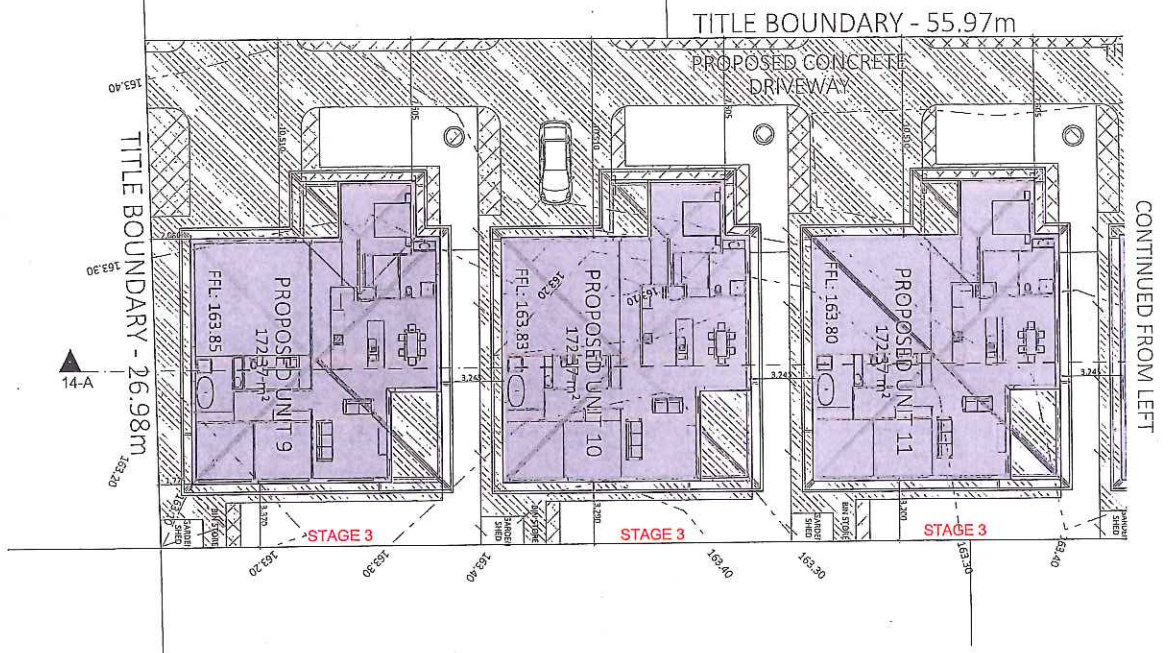
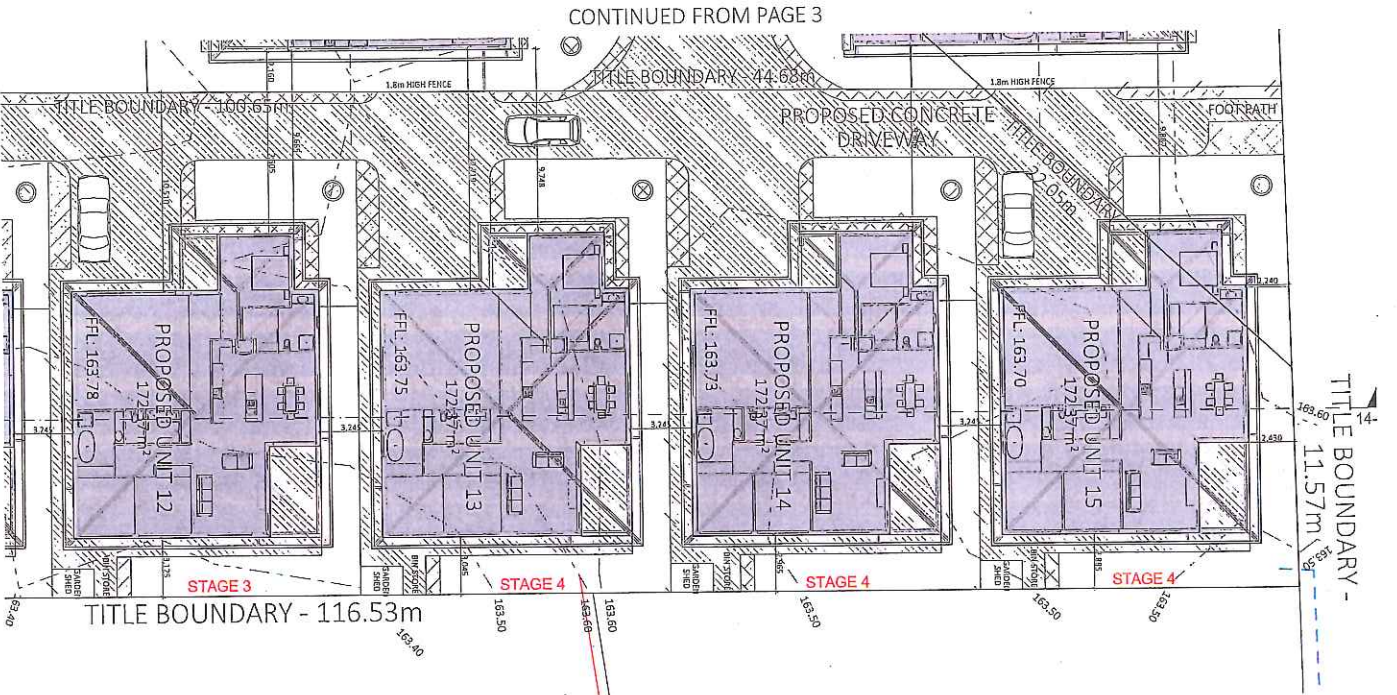
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R2	17/02/2021	FLR.				

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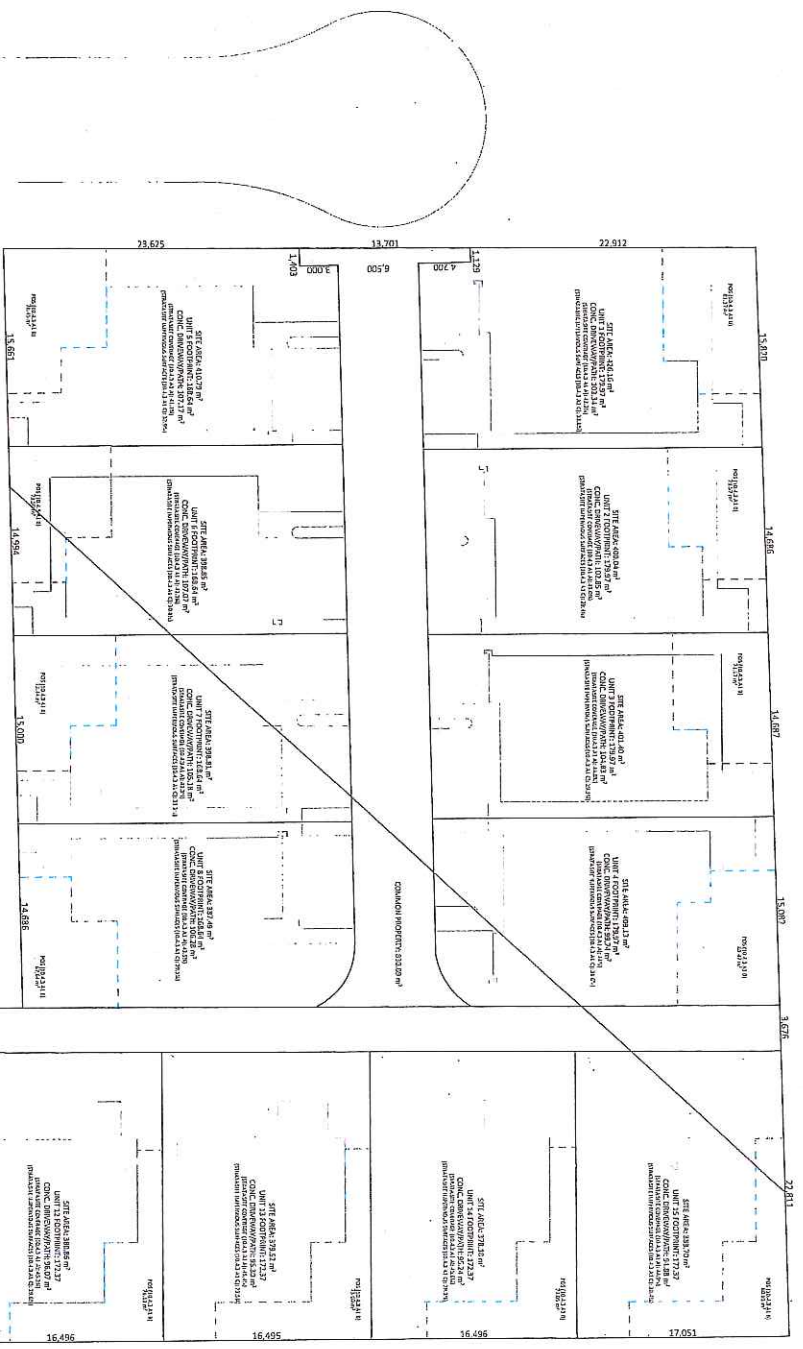
MICHELL LLOYD
 ACC # 37129313
 A/N. 71 615 812 2747
 P/L 6344 7319

CLIENT'S: HARRIS DALEY
 SITE ADDRESS: 49 GEORGE STREET
 DRAWING: SITE DETAIL 2
 I/W/E APPROVE THESE DRAWINGS TO BE CORRECT PER CONTRACT.
 SIGNATURE: DATE: DATE: DATE:
 SIGNATURE: DATE: DATE: DATE:

REV.	DATE	DESCRIPTION	DESIGNER	M.L.	JOB NUMBER	GRGS49
R1	30/10/2020	FOR D.A.				
R2	17/02/2021	F.I.R.				

CHECKED	M.L.	SCALE (@A1)
		1:200

AMENDED
 EXHIBITED



CLIENT/S:
 HARRIS DALEY

DRAWING
 STRATA PLAN

I/WE APPROVE THESE DRAWINGS TO BE
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 SIGNATURE:
 DATE:
 PLACE

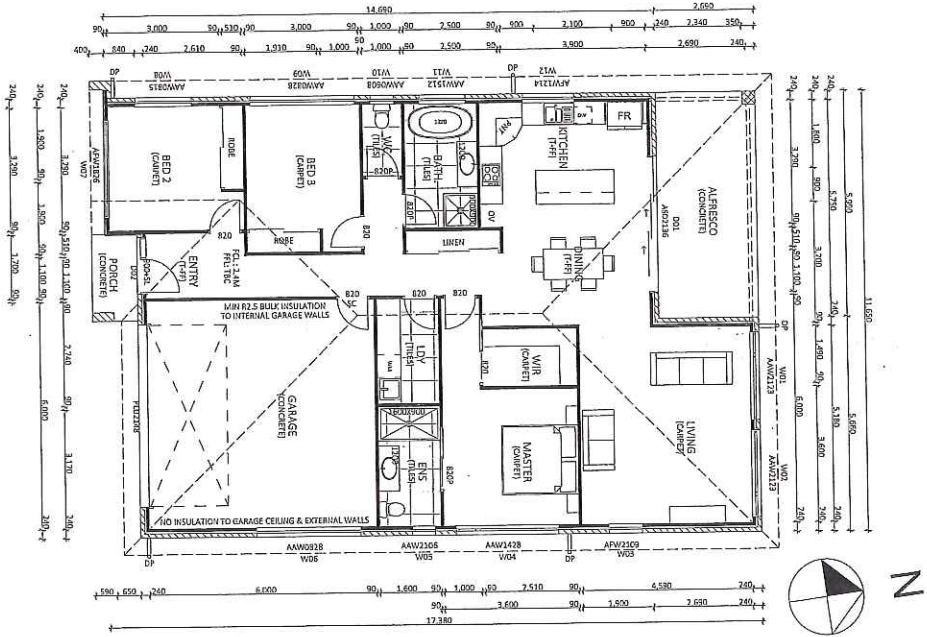
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R2	17/02/2021	F.I.R.		5/22

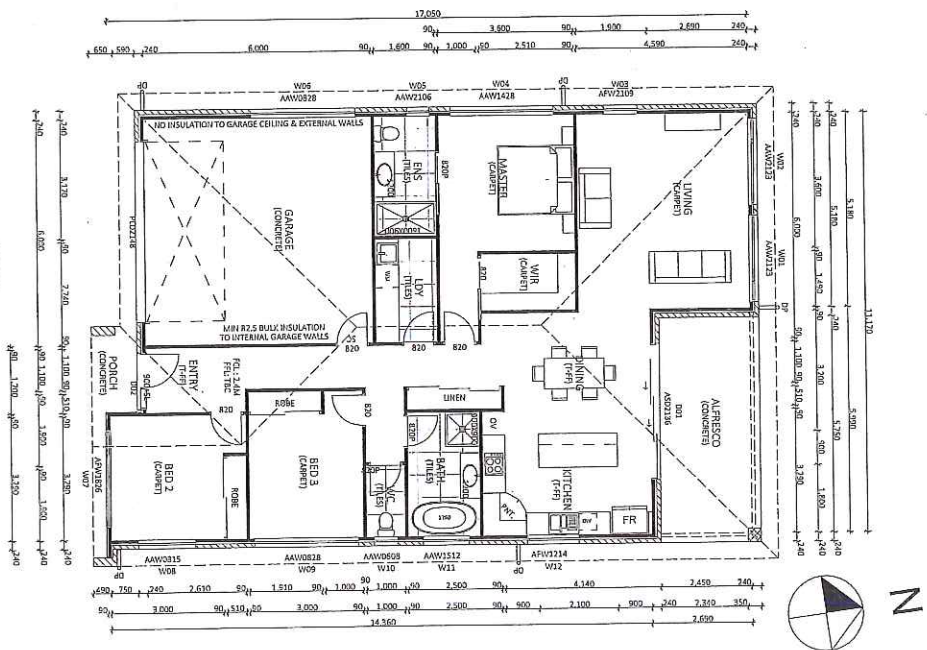
CHK/CR	DATE	SCALE
ML		1:300

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EXHIBITED

UNIT 1 FFL: 163.78
 UNIT 2 FFL: 163.75
 UNIT 3 FFL: 163.73



UNIT 4 FFL: 163.70



MITCHELL LOYD
 ACC # 97799313
 A.B.N. 71 615 812 747
 PH. 6344 7319

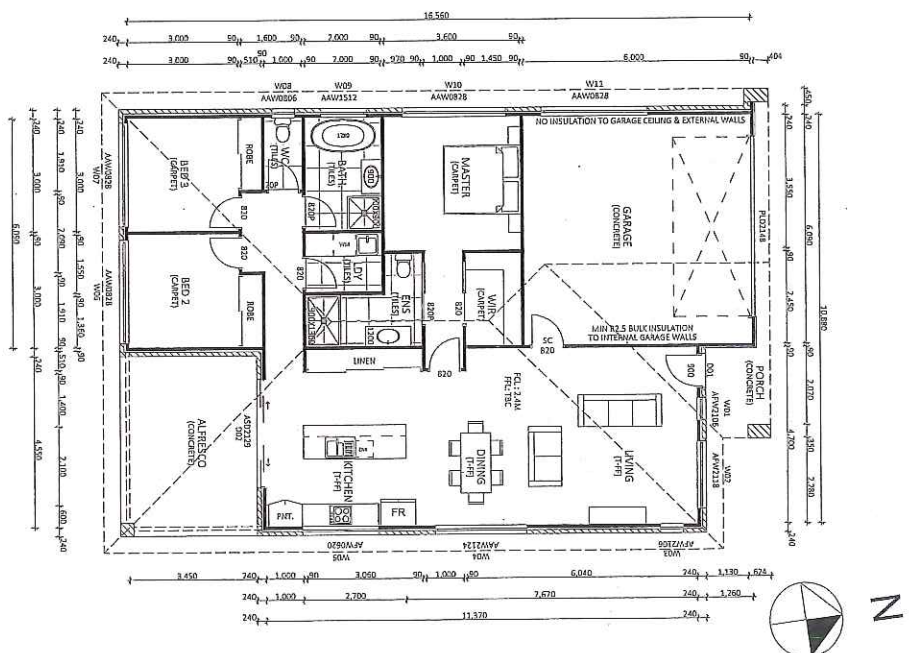
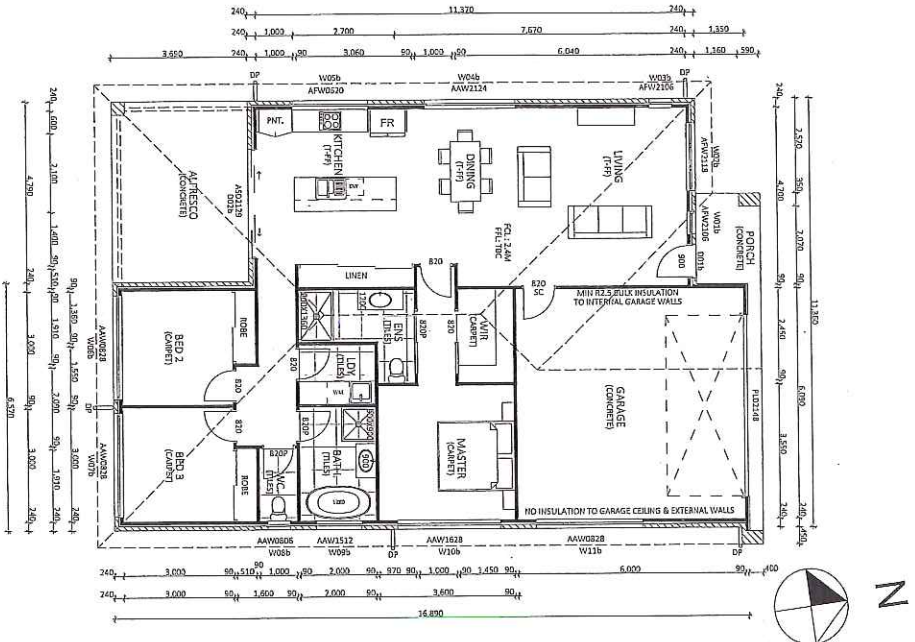
Clients: HARRIS DALEY
DRAWING: GROUND PLAN
UNIT TYPE: A
DATE: 17/02/2023

DATE: 17/02/2023
DATE: 17/02/2023

REV.	DATE	DESCRIPTION	DESIGNER	CHECKED
R1	30/10/2020	FOR D.A.	M.L.	M.I.
R2	17/02/2023	FINAL	L.S.	M.I.

AMENDED EXHIBITED

UNIT 5 FFL: 163.75
 UNIT 6 FFL: 163.73
 UNIT 7 FFL: 163.70



UNIT 8 FFL: 163.68

DESIGN
 MITCHELL LLOYD
 ACC # 237799313
 ABLN 21 615 812 2747
 PH 6344 7319
 F info@44architect.com.au

Clients:
 HARRIS DALEY
Site Address:
 49 GEORGE STREET

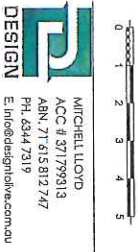
DRAWING
 GROUND PLAN
 UNIT TYPE B
Signature:
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DRAWN	L.S.	DRG549
CHECKED	M.L.	SCALE (@A2) 1:100

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HARRIS DALE
SITE ADDRESS:
47 GEORGE STREET,
MELB 3000

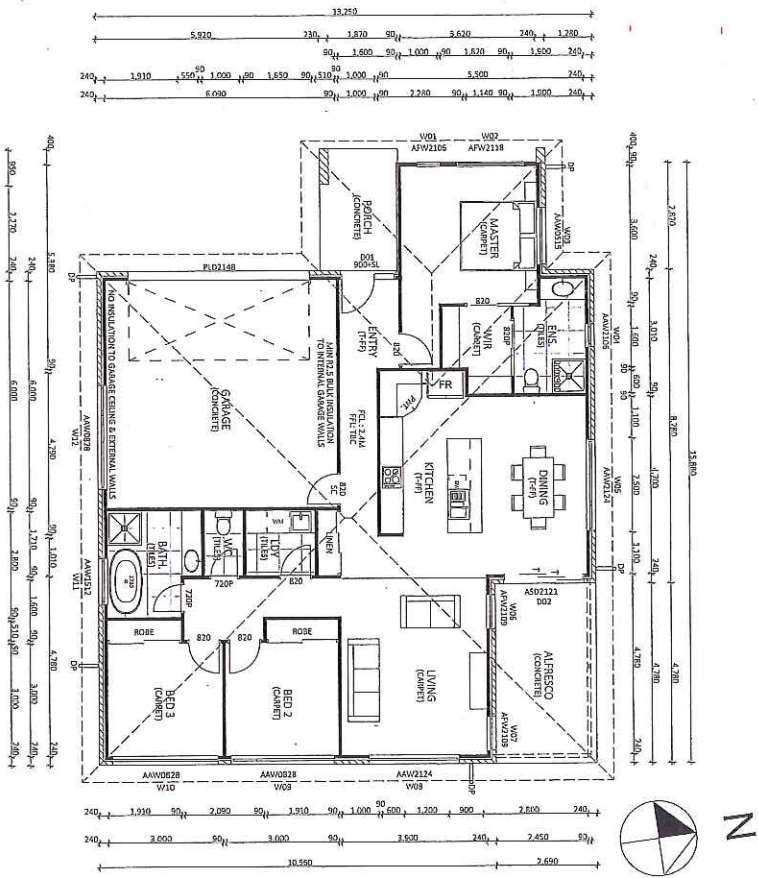
DRAWING
GROUND PLAN
UNIT TYPE C

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R2	17/02/2021	FOR L.S.			8/22
			CHECKED	M.L.	SCALE @A2] 1:100

- UNIT 9 FFL: 163.85
- UNIT 10 FFL: 163.83
- UNIT 11 FFL: 163.80
- UNIT 12 FFL: 163.78
- UNIT 13 FFL: 163.75
- UNIT 14 FFL: 163.73
- UNIT 15 FFL: 163.70



AMENDED

EXHIBIT F

- LEGEND**
- B - BASIN
 - Ba - BATH
 - S - SINK (65Ø)
 - T - LAUNDRY TUB (65Ø)
 - SH - SHOWER
 - WC - WATER CLOSET
 - FW - FLOOR WASTE
 - EV - VENT THROUGH TO ROOF
 - LD - DOWNPIPE
 - LDG - DOWNPIPE
 - R - RODDING EYE
 - HW - HOT WATER CYLINDER
 - X - EXTERNAL TAP
 - P - DRAINAGE PIT (600 x 600mm)
 - Dp - DOWNPIPE (90 Ø)
 - W - WET AREAS
 - SW - STORMWATER LINE (100mm PVC)
 - SWL - STORMWATER LINE (100mm PVC)
 - SL - SEWER LINE (100mm PVC)
 - SLD - SEWER LINE (TBC BY HYDRAULIC DESIGN)

INSTALL INSPECTION OPENINGS AT MAJOR BENDS FOR STORMWATER AND ALL LOW POINTS OF DOWNPIPES. PROVIDE SURFACE DRAIN TO BACK OF BULK EXCAVATION TO DRAIN LEVELLED PAD PRIOR TO COMMENCING FOOTING EXCAVATION.

SERVICES

THE HEATED WATER SYSTEM MUST BE DESIGNED AND INSTALLED WITH PART B2 OF THE NCC VOLUME THREE- PLUMBING CODE OF AUSTRALIA.

HEATMAL INSULATION FOR HEATED WATER PIPING MUST: A) BE PROTECTED AGAINST THE EFFECTS OF WEATHER AND SUNLIGHT; AND B) BE ABLE TO WITHSTAND THE TEMPERATURES WITHIN THE PIPING; AND C) USE THERMAL INSULATION IN ACCORDANCE WITH AS/NZS 4859.1

HEATED WATER PIPING THAT IS NOT WITHIN A CONDITIONED SPACE MUST BE THERMALLY INSULATED AS FOLLOWS:

1. INTERNAL PIPING
 - a) ALL R/W AND RETURN INTERNAL PIPING THAT IS WITHIN AN UNVENTILATED WALL SPACE
 - b) WITHIN AN INTERNAL FLOOR BETWEEN STOREYS; OR
 - c) BETWEEN CEILING INSULATION AND A CEILING MUST HAVE A MINIMUM R-VALUE OF 0.2

2. PIPING LOCATED WITHIN A VENTILATED WALL SPACE AN ENCLOSED BUILDING SUBFLOOR OR A ROOF SPACE

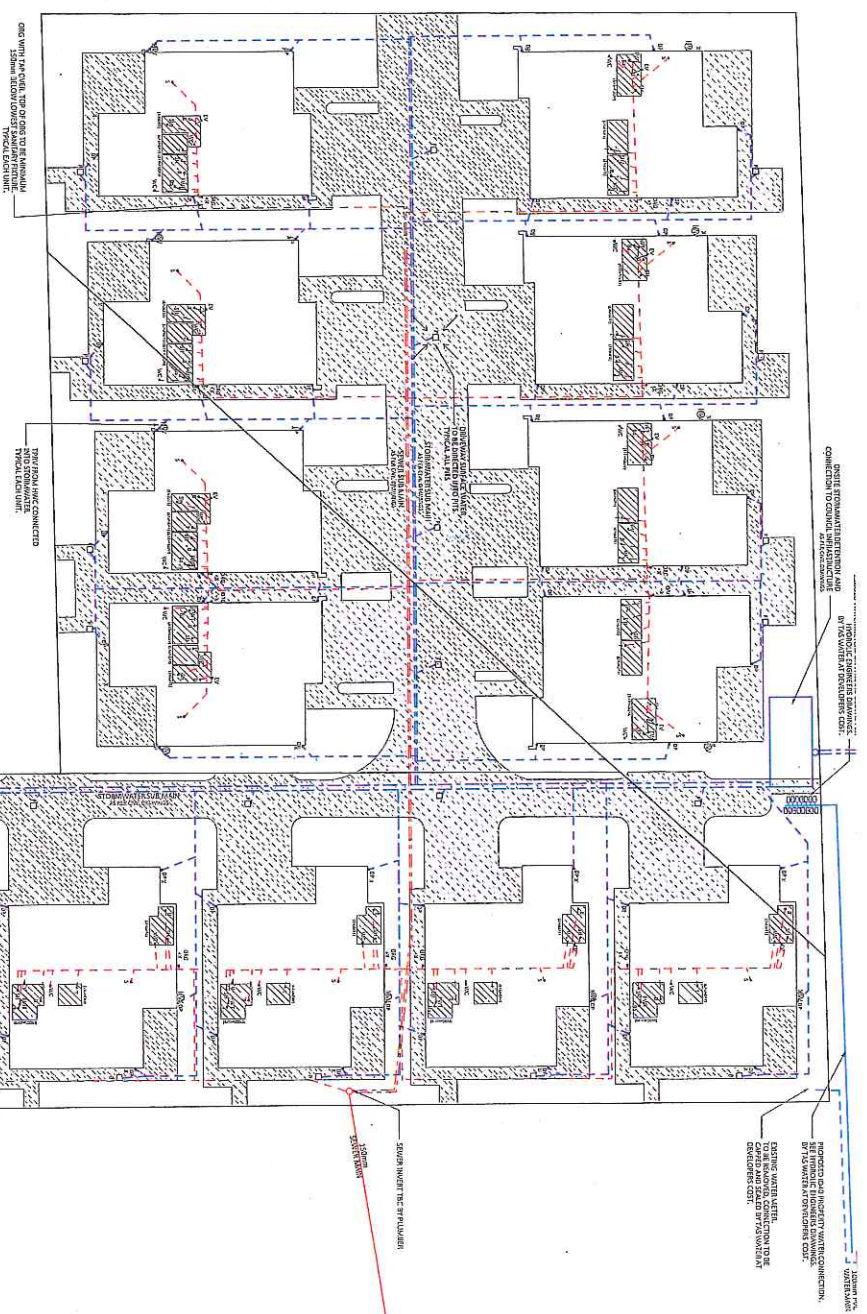
- a) ALL FLOW AND RETURN PIPING
- b) COLD WATER SUPPLY PIPING AND RELIEF VALVE PIPING WITHIN 500mm OF THE CONNECTION TO CENTRAL WATER HEATING SYSTEM,
- c) MUST HAVE A MINIMUM R-VALUE OF 0.45

3. PIPING LOCATED OUTSIDE THE BUILDING OR IN AN UNENCLOSED BUILDING SUBFLOOR OR ROOF SPACE

- a) ALL FLOW AND RETURN PIPING
- b) COLD WATER SUPPLY PIPING AND RELIEF VALVE PIPING WITHIN 500mm OF THE CONNECTION TO CENTRAL WATER HEATING SYSTEM,
- c) MUST HAVE A MINIMUM R-VALUE OF 0.6

PIPING WITHIN AN INSULATED TIMBER FRAMED WALL SUCH AS THAT PASSING THROUGH A WALL STUD IS CONSIDERED TO COMPLY WITH THE ABOVE INSULATION REQUIREMENTS.

ALL DRAINAGE WORK SHOWN IS PROVISIONAL ONLY AND IS SUBJECT TO AMENDMENT TO COMPLY WITH LOCAL AUTHORITIES. ALL WORK IS TO COMPLY WITH AS-3500 AND LOCAL PLUMBING CODE AND SHOULD BE CARRIED OUT BY A LICENSED PLUMBER.



MITCHELL LLOYD
 ACC# J 93778913
 A/NB 71 15 812 747
 PH: 6344 7319

CLIENT/S:
 HARRIS PALY
SITE ADDRESS:
 49 GEORGE STREET,

DRAWING EXTERNAL SERVICES
I/VE APPROVE THESE DRAWINGS TO BE CORRECT PER CONTRACT.
SIGNATURE:
 S/C/M ATT'D:
DATE:
 N.A.T.F.

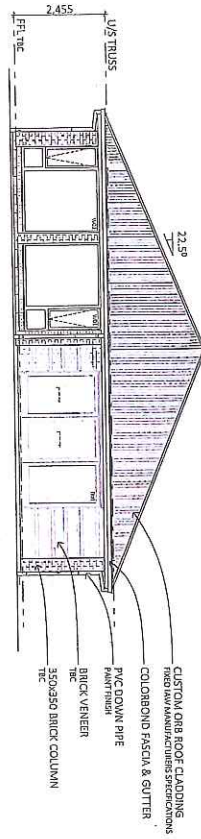
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R2	17/02/2021	F.I.R.			9/22

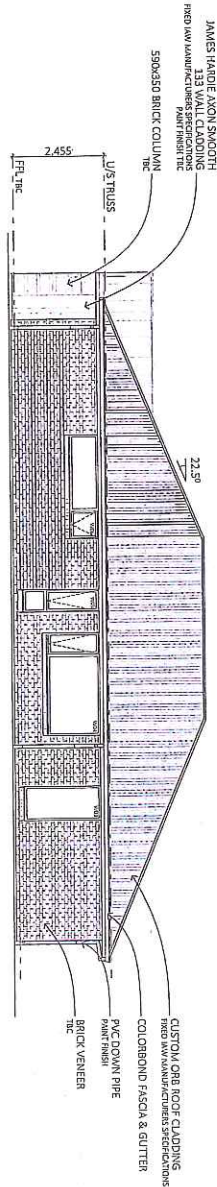
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AMENDED
EVAPORATED

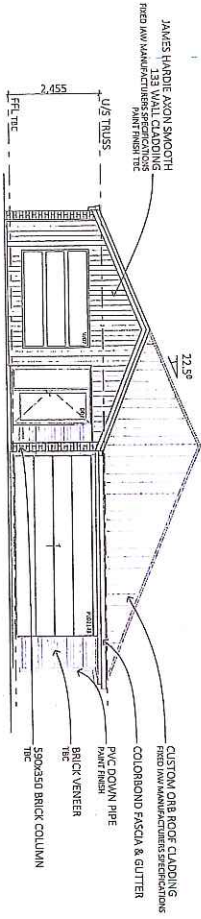
NORTHERN ELEVATION
UNITS 1, 2 & 3
MIRRORED FOR UNIT 4



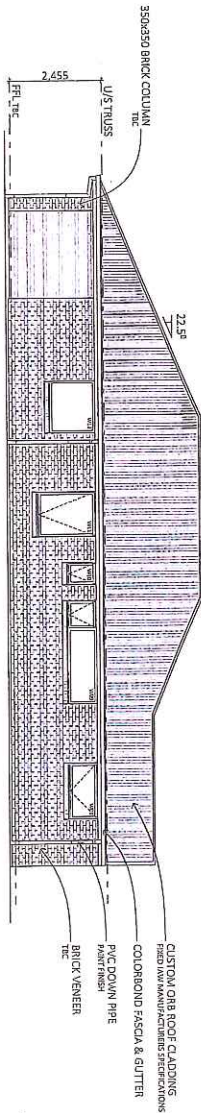
EASTERN ELEVATION
UNITS 1, 2 & 3
MIRRORED FOR WEST ELEVATION OF UNIT 4



SOUTHERN ELEVATION
UNITS 1, 2 & 3
MIRRORED FOR UNIT 4



WESTERN ELEVATION
UNITS 1, 2 & 3
MIRRORED FOR EAST ELEVATION OF UNIT 4



CLIENT/S:
HARRIS DALEY
SITE ADDRESS:
49 GEORGE STREET

DRAWING
ELEVATIONS
UNIT TYPE A

I/WE APPROVE THESE DRAWING TO BE
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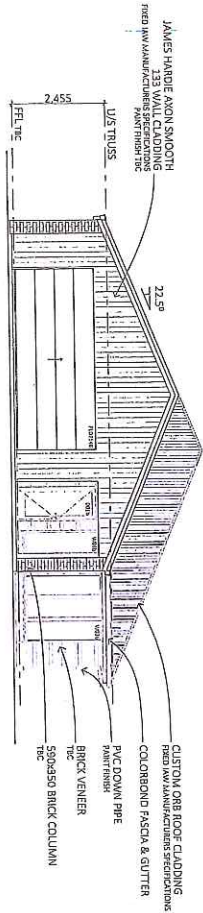
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R2	17/02/2021	FOR D.A.				

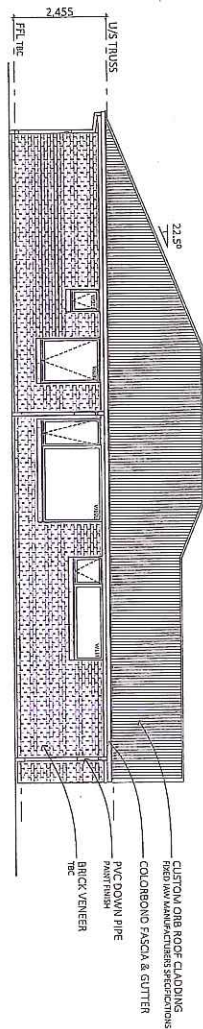
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EXHIBITED

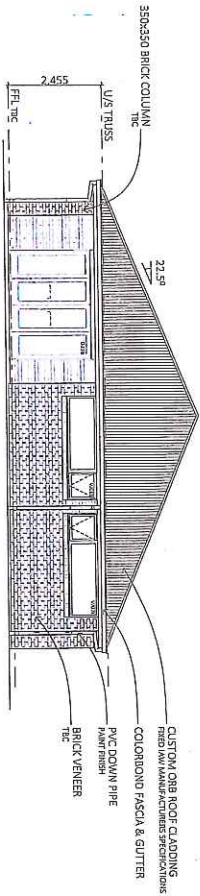
AMENDED



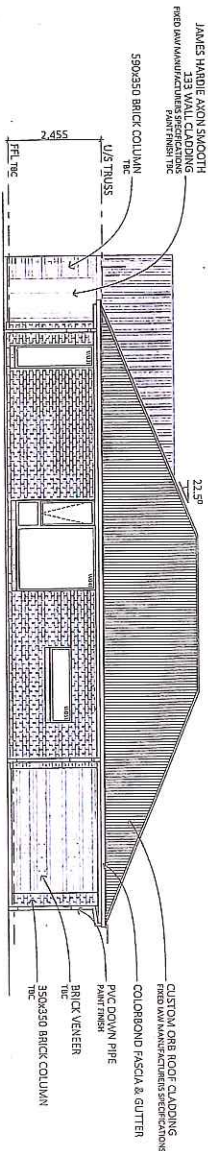
NORTHERN ELEVATION
UNITS 5, 6 & 7
MIRRORED FOR UNIT 8



EASTERN ELEVATION
UNITS 5, 6 & 7
MIRRORED FOR WEST ELEVATION OF UNIT 8



SOUTHERN ELEVATION
UNITS 5, 6 & 7
MIRRORED FOR UNIT 8



WESTERN ELEVATION
UNITS 5, 6 & 7
MIRRORED FOR EAST ELEVATION OF UNIT 8



RECIPRO
MIRCHELL LLOYD
ACC # 97129813
ABN 71 615 812 247
PH 434 731 9

CLIENT/S:
HARRIS DALEY
SITE ADDRESS:
49 GEORGE STREET

DRAWING
ELEVATIONS
UNIT TYPE B

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R2	17/02/2021	FLR.

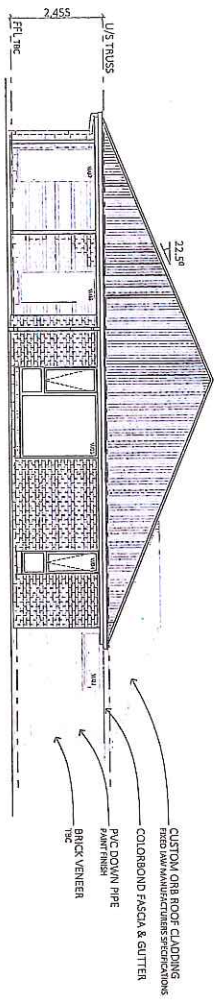
DESIGNER
DRAWN
CHECKED

M.L.
L.S.
M.L.

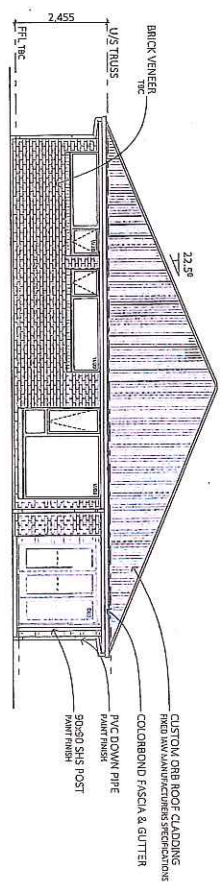
JOB NUMBER
DRAWING

CRGS49
11/22
SCALE @A2)

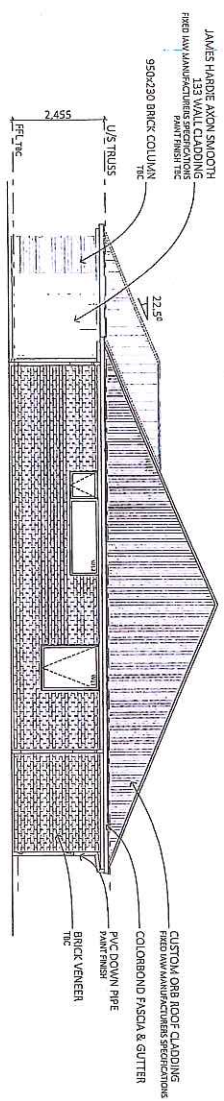
EXHIBITED
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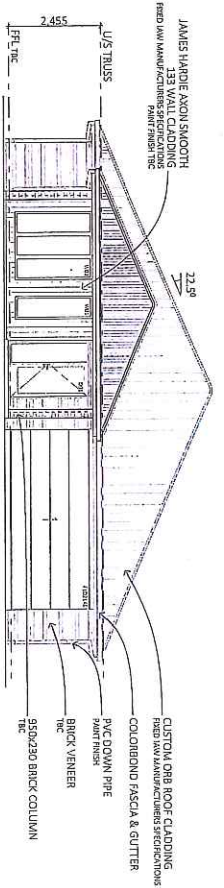
NORTHERN ELEVATION
UNITS 9 - 15



EASTERN ELEVATION
UNITS 9 - 15



SOUTHERN ELEVATION
UNITS 9 - 15



WESTERN ELEVATION
UNITS 9 - 15

2-117

MITCHELL LLOYD
 ACC # 37179913
 ABN 71 613 812 747
 PH. 6344 7319

CLIENT/S:
 HARRIS DALEY

DRAWING
 ELEVATIONS
 UNIT TYPE C

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SIGNATURE:
 DATE:
 N.A.T.

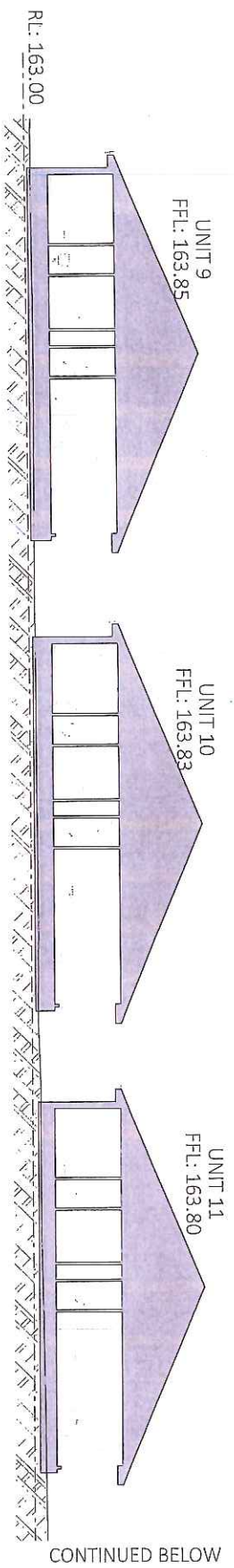
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REV.	DATE	DESCRIPTION
R1	30/10/2020	FOR D.A.
R2	17/02/2021	F.I.R.

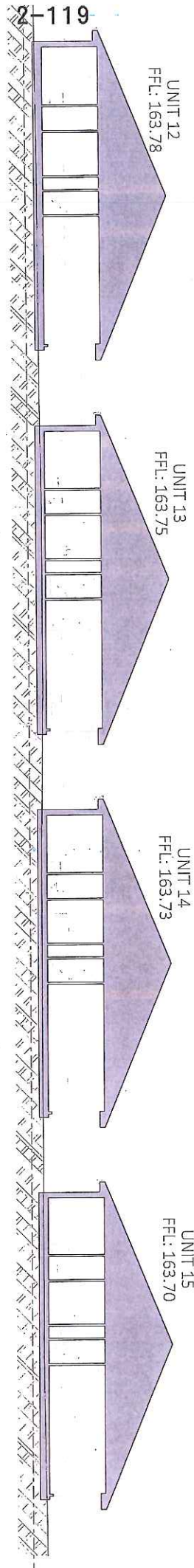
DESIGNER	M.L.	JOB NUMBER
DRAWN	L.S.	GRGS49

CHECKED M.I.
 SCALE 1:100

EXHIBITED
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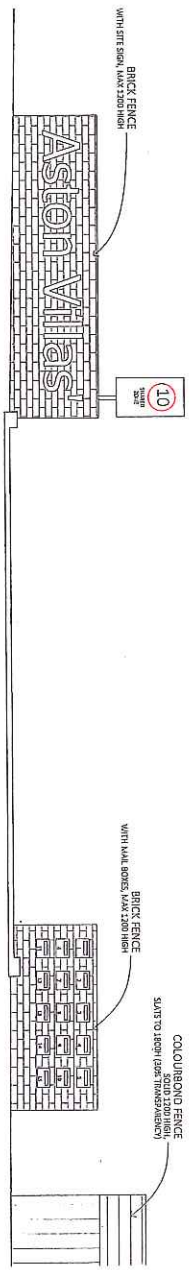


CONTINUED FROM ABOVE



SECTION 14A-A
SCALE 1:120

SECTION 14B-B
SCALE 1:50



DESIGN
MITCHELL LLOYD
ACC # 571799313
ABN: 71 615 812 747
PH: 6344 7319
E: info@edfdesign.com.au

CLIENT/S:
HARRIS DALEY
SITE ADDRESS:
47 GEORGE STREET
SYDNEY NSW

DRAWING
SITE SECTIONS
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DATE:
DATE:

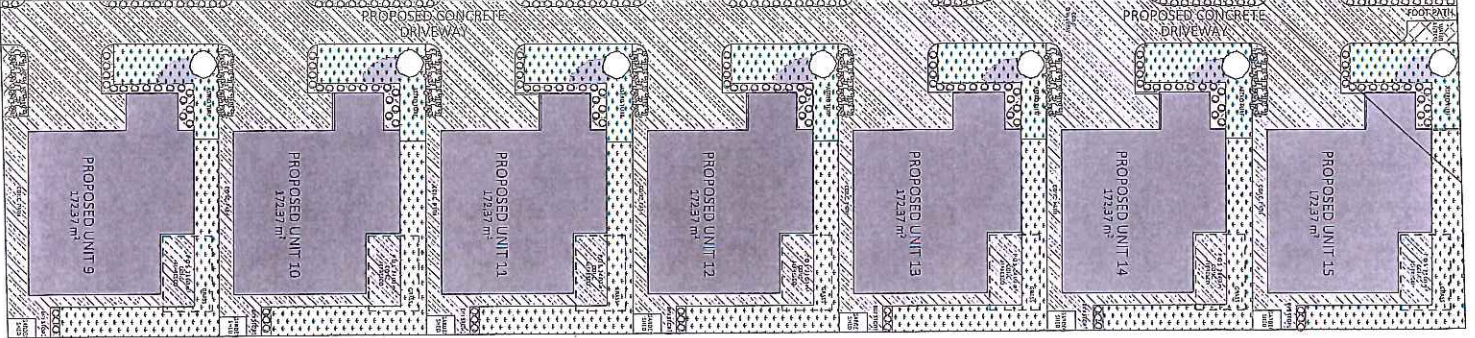
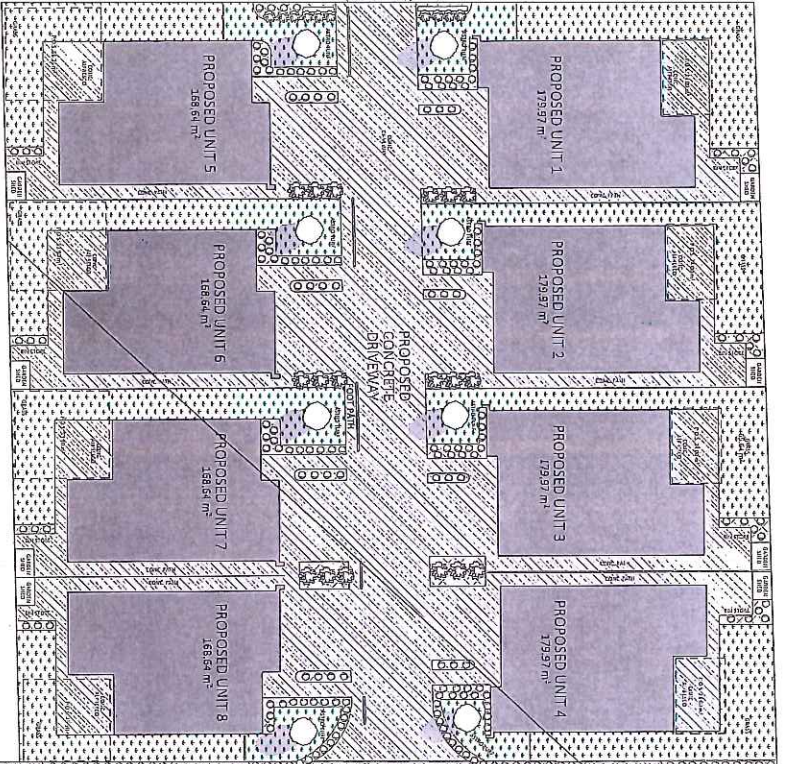
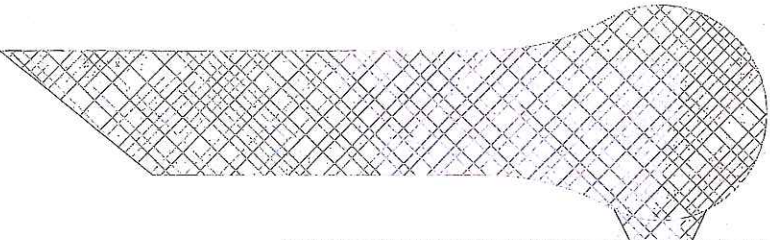
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REV.	DATE	DESCRIPTION	DESIGNER	M.L.	JOB NUMBER
R1	30/10/2020	FOR D.A.			GRG349
R2	17/02/2021	F.L.R.			

CHECKED: M.L. SCALE (@A2) VAR.
DRAWN: U.S. DRAWING: 14/22
EXHIBITED
AMENDED

KEY

- ASSORTED SHAUBS
MAX MATURE HEIGHT 2m
- PITTOSPORUM TREES PATTERSON
MAX MATURE HEIGHT 2.5m
- ORNAMENTAL PEACH TREES
MAX MATURE HEIGHT 2m
MINIMUM PLANTING HEIGHT 1.2m



MITCHELL LLOYD
ACC # 97799313
ABN: 71 615 812 272
PH: 6344 7319
E: info@mitchelllloyd.com.au

CLIENTS:
HARRIS DALEY
SITE ADDRESS:
49 GEORGE STREET

DRAWING:
LANDSCAPE
PLAN

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SIGNATURE: _____ **DATE:** _____

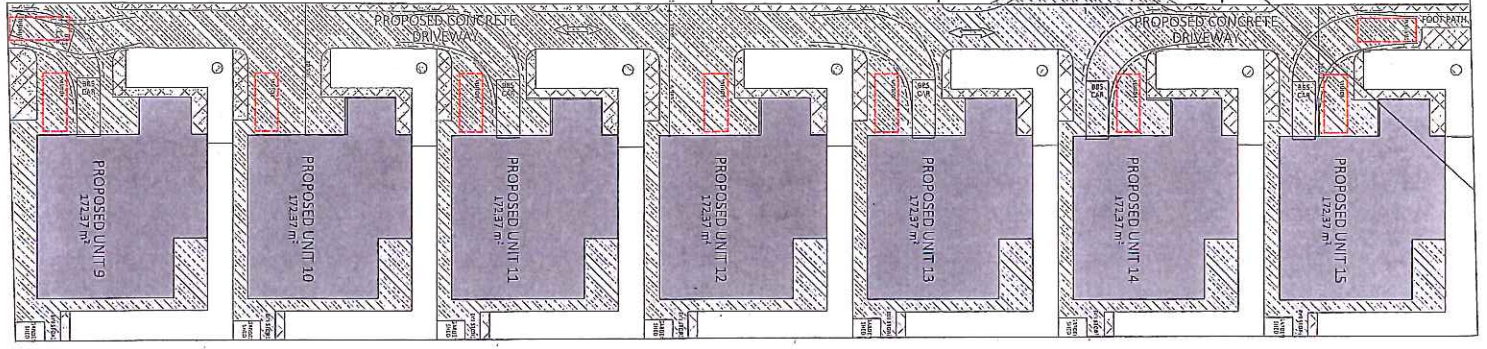
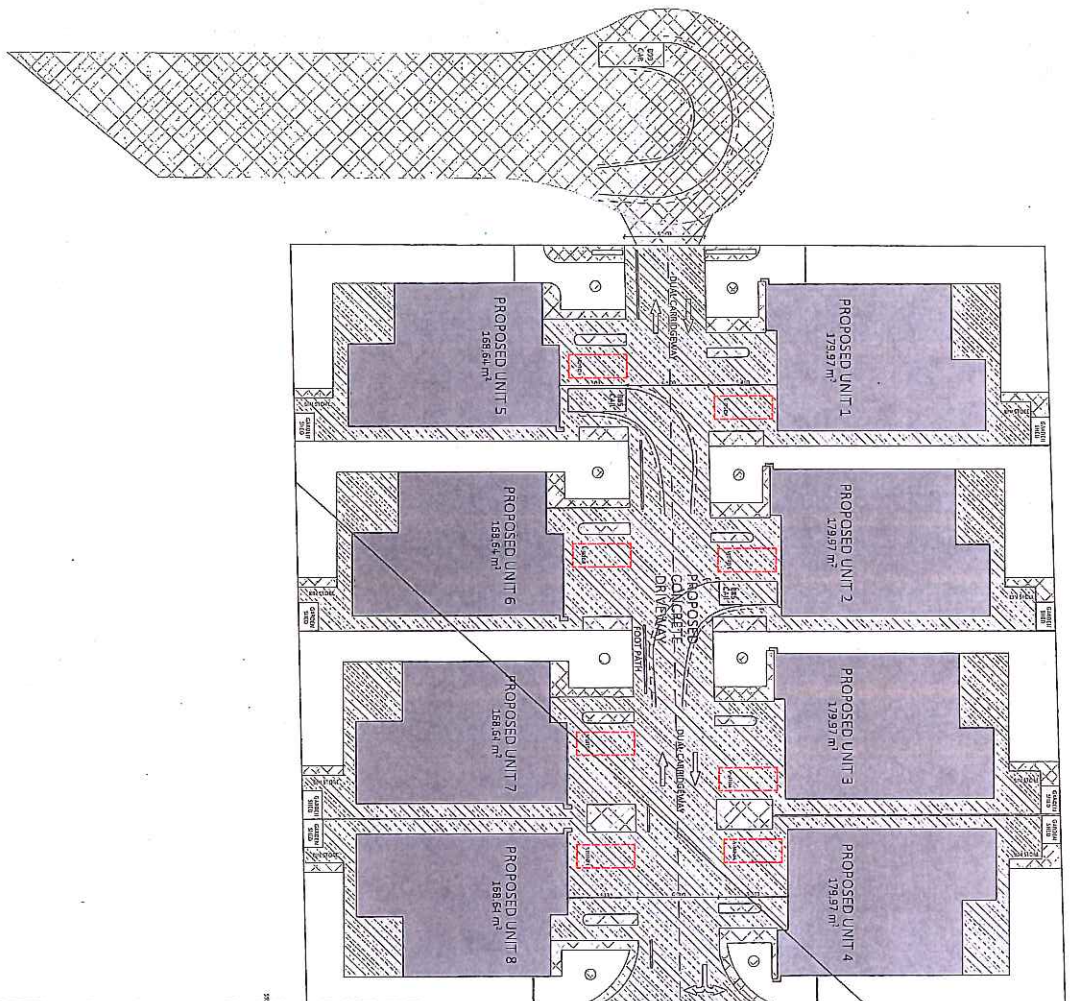
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REV.	DATE	DESCRIPTION	DESIGNER	CHECKED	M.L.	JOB NUMBER	GRGS:49
R1	30/10/2020	FOR D.A.					
R2	17/02/2021	F.I.L.D.					

DESIGNER: DRAWN: _____ **M.L.** **JOB NUMBER:** _____ **GRGS:49**

CHECKED: _____ **M.L.** **SCALE:** (A2) 1:300

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DESIGN
 MITCHELL LOYD
 ACC # 37179913
 ABN, 71 815 812 747
 PH, 6344 7319

CLIENT/S:
 HARRIS DALEY
SITE ADDRESS:
 49 GEORGE STREET,

DRAWING AND TURNING

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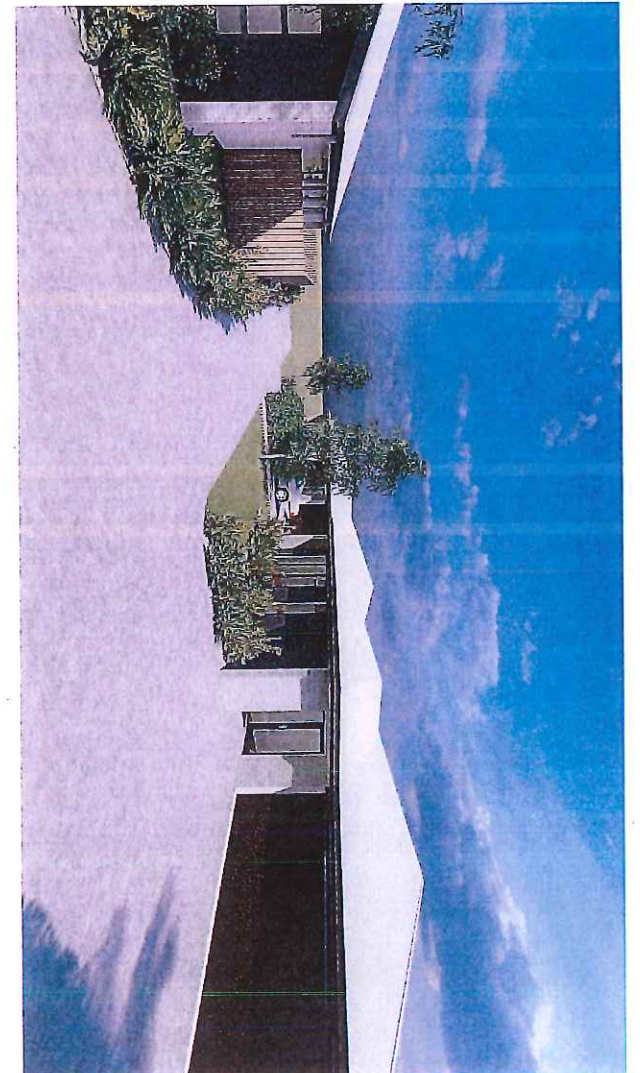
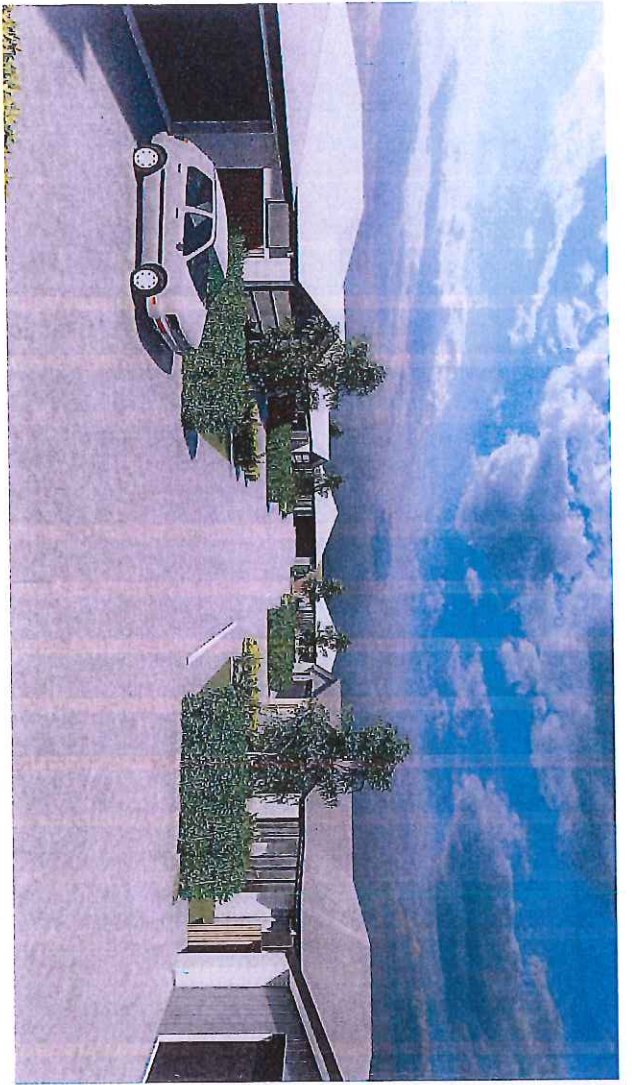
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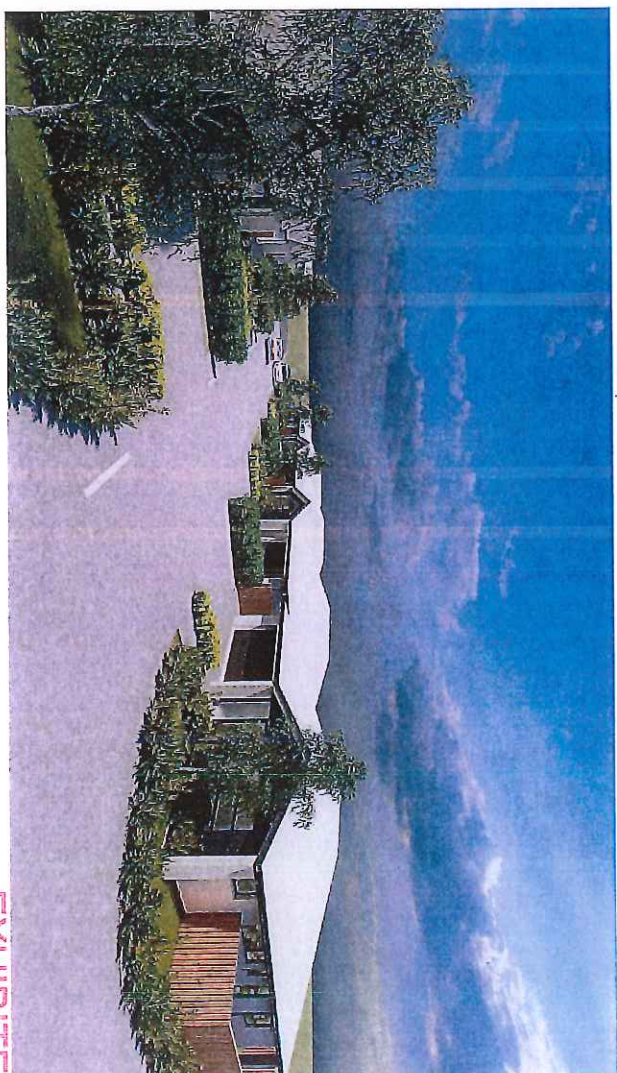
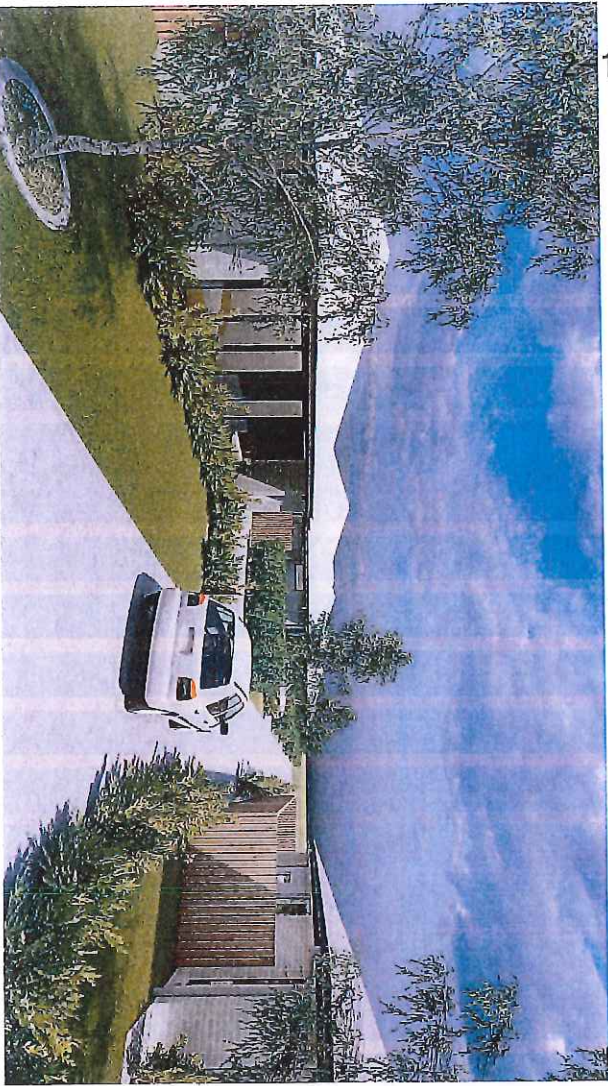
REV.	DATE	DESCRIPTION	DESIGNER	M.L.	JOB NUMBER
R1	20/10/2020	FOR D.A.			GRCS49
R2	17/07/2021	P.L.R.			16/22

DESIGNER
 M.L.
DRAWN
 L.S.
CHECKED
 M.L.

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122



MITCHELL LYON
 ACC # 27179313
 ABN 71 615 812 747
 PH 6344 7319

CLIENT/S:
 HARRIS DALEY
SITE ADDRESS:
 49 GEORGE STREET,
 SYDNEY NSW

DRAWING
 PERSPECTIVES

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SIGNATURE:
 (SIGNATURE)
DATE:
 17/12/22

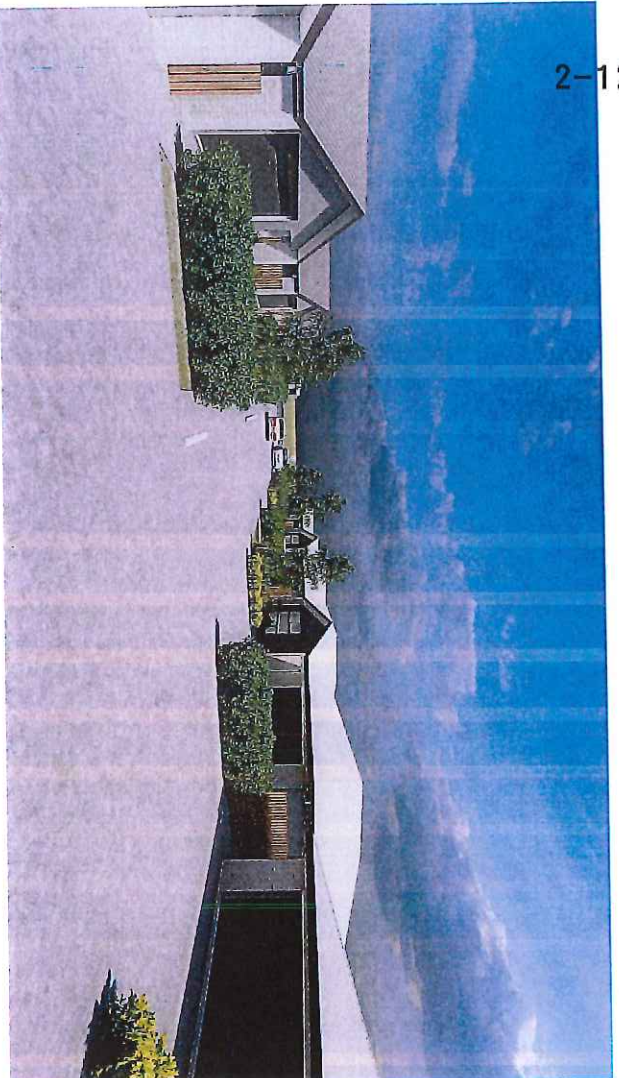
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REV.	DATE	DESCRIPTION	DESIGNER	M.L.	JOB NUMBER	GRGS/49
R1	30/10/2020	ICR D.A.				
R2	17/02/2021	FLR				

CHECKED	M.I.	DATE	NTS

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2-123



MITCHELL LLOYD
 ACC # 971799313
 A.B.N. 71 615 812 747
 PH. 6344 7319

CLIENT/S:
 HARRIS DALEY
 SITE ADDRESS:
 49 GEORGE STREET

DRAWING
 PERSPECTIVES

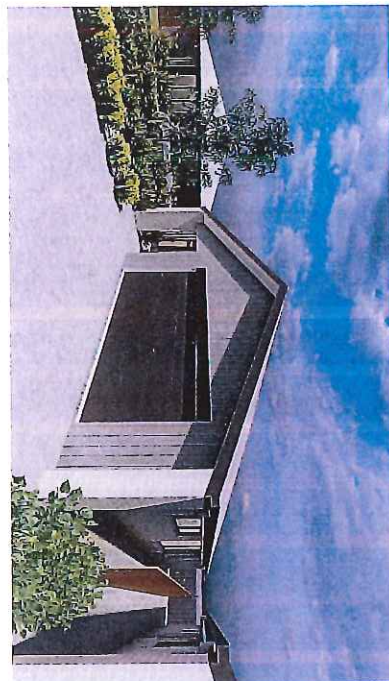
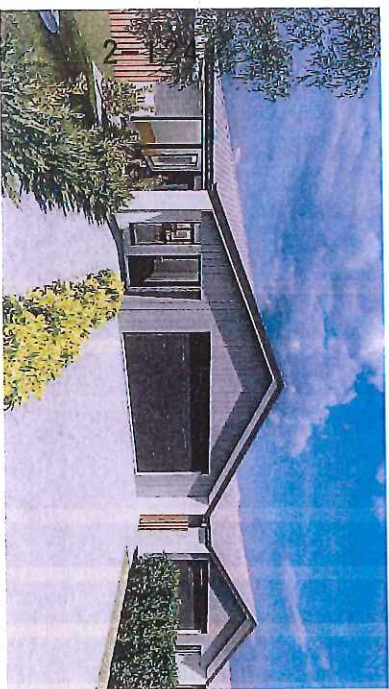
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R1	20/10/2020	FOR D.A.	DRAWN	L.S.	GRGS49
R2	17/02/2021	FOR D.A.	CHECKED	M.L.	SCALE @A21

DATE: _____
 DATE: _____
 DATE: _____
 DATE: _____
 DATE: _____

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MITCHELL LLOID
 ACC # 37179313
 AEN 714 613 812/747
 PH. 6344 7319

CLIENTS:
 HARRIS DALEY
SITE ADDRESS:
 40 GERRARD STREET
 EAST

DRAWING
PERSPECTIVES

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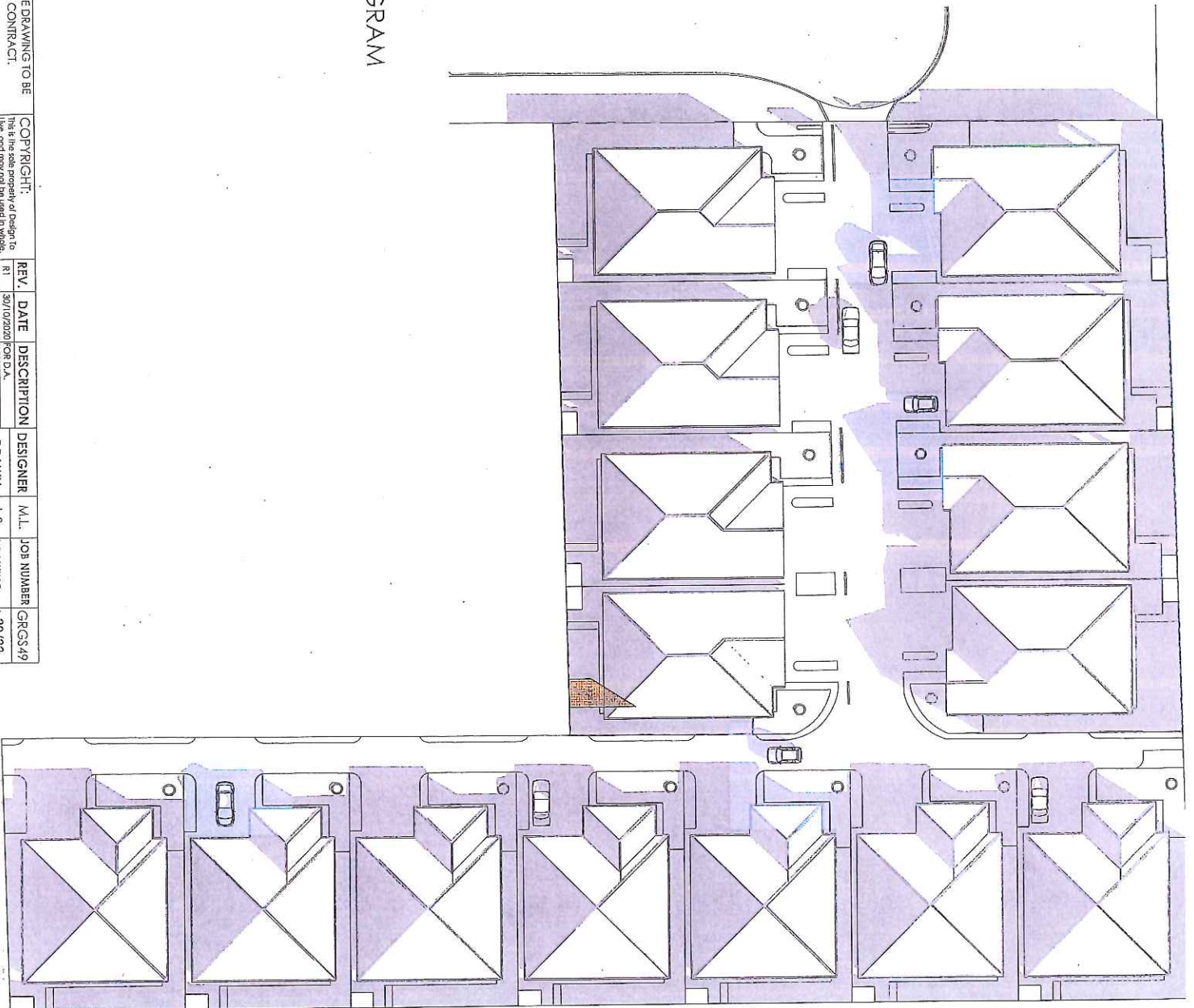
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REV.	DATE	DESCRIPTION	DESIGNER	M.L.	JOB NUMBER
R1	30/10/2021	FOR D.A.	DRAWN	L.S.	GRGS-19
R2	17/02/2021	F.A.R.	CHECKED	M.A.I	19/22

EXHIBITED
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SHADOW DIAGRAM
21ST JUNE 10.00am



CLIENT/S: HARRIS DALEY
 DRAWING: SHADOW
 SIGNATURE: I/WE APPROVE THESE DRAWING TO BE CORRECT PER CONTRACT.
 DATE: _____
 NAME: _____

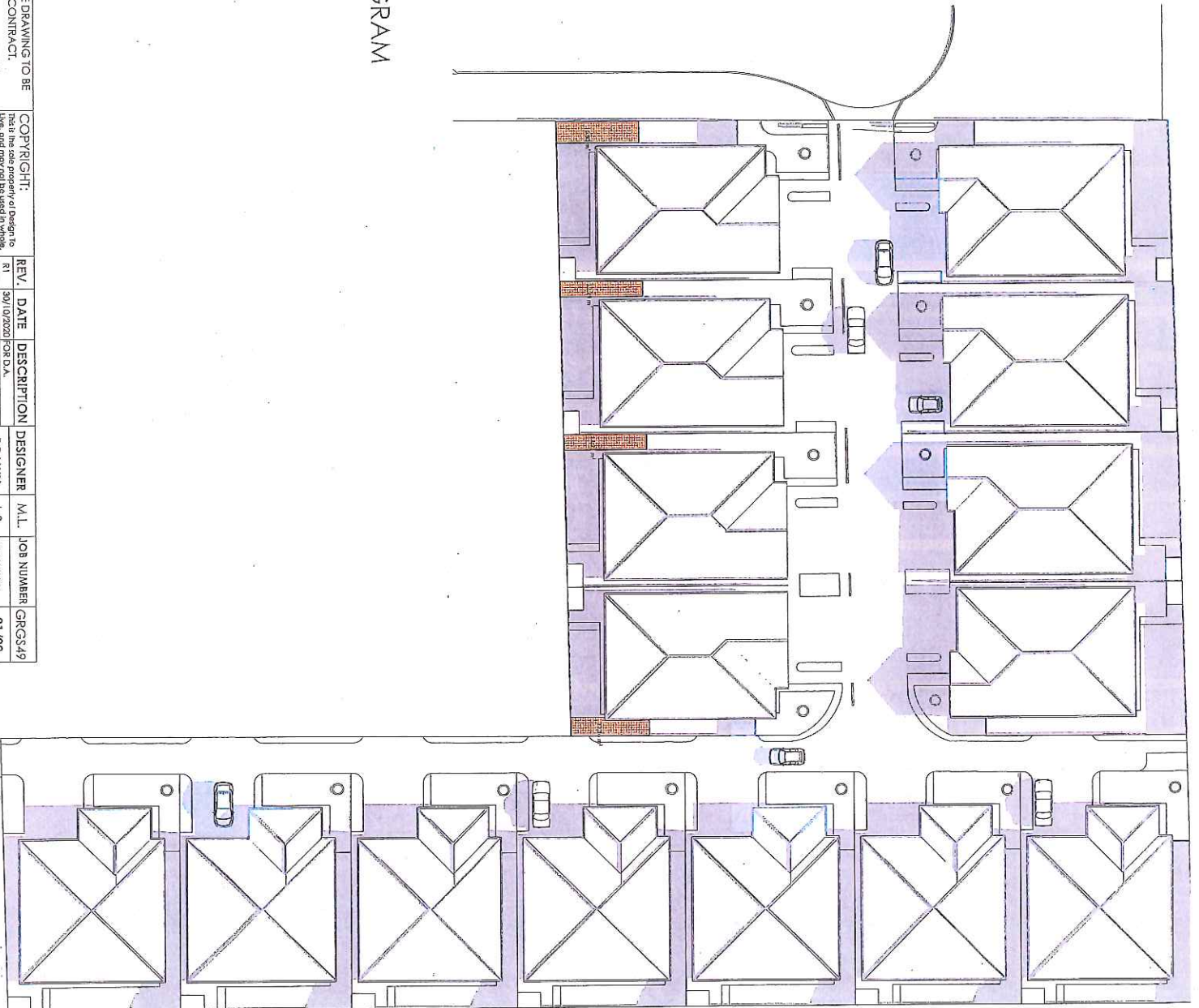
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REV.	DATE	DESCRIPTION	DESIGNER	M.L.	JOB NUMBER	GRCS/49
R1	30/09/2020	FOR D.A.	DRAWN	L.S.	DRAWING	20/22
R2	17/09/2021	FILE	CHECKED	M.I.	SCALE 1:600	1:300

MICHELL LOND
 ACC # 371799313
 ABRN 71 613 812 747
 PH: 6344 7319

EXHIBITED
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SHADOW DIAGRAM
21ST JUNE 12 MIDDAY



DESIGN
MICHHELL LOYD
ACC # 37179913
ASN, 71 615 812 747
Ph. 6944 2319
E.info@designlab.com.au

CLIENT/S:
HARRIS DALEY
SITE ADDRESS:
67 GEORGE STREET
ADELAIDE, SA 5000

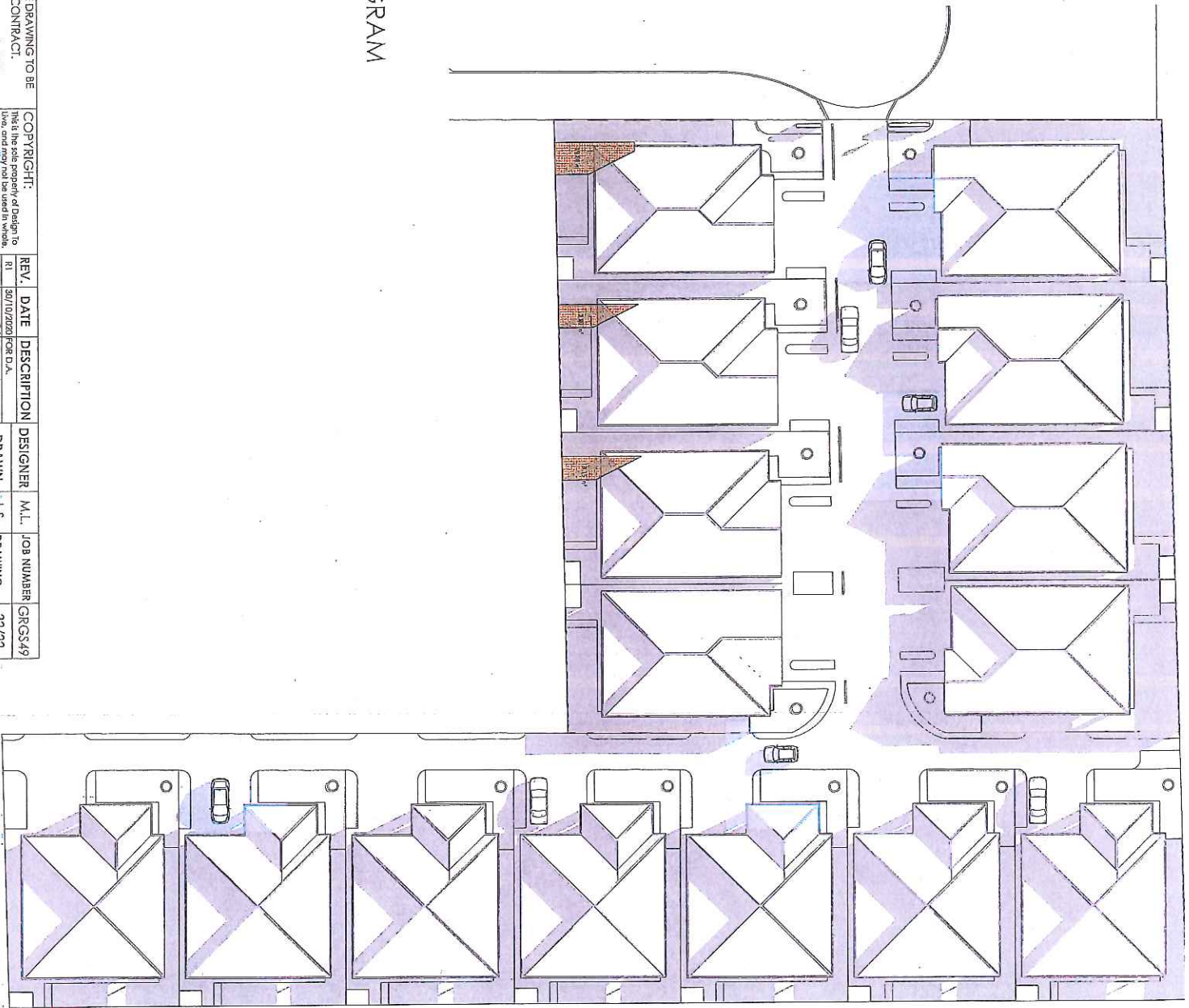
DRAWING
SHADOW
DIAGRAM 12.00
SIGNATURE:
SIGNATURE:
DATE:
DATE:
DATE:

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REV.	DATE	DESCRIPTION	DESIGNER	M.L.	JOB NUMBER	GRGS#
R1	30/10/2020	FOR D.A.	DRAWN	L.S.	DRAWING	21/22
R2	17/09/2021	FLR.	CHECKED	M.L.	SCALE @A2)	1:300

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SHADOW DIAGRAM
21ST JUNE 2.00pm



MITCHELL LLOYD
ARCHITECTS
ABN 71 615 812 747
PH 4544 7319

CLIENT/S:
HARRIS DALE
SITE ADDRESS:
49 GEORGE STREET

DRAWING
SHADOW
DIAGRAM 2.00

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R1	30/10/2020	FOR D.A.	DRAWN	L.S.	DRAWING	22/22
R2	17/02/2021	F.I.R.	CHECKED	M.I.	SCALE @A1	1:300

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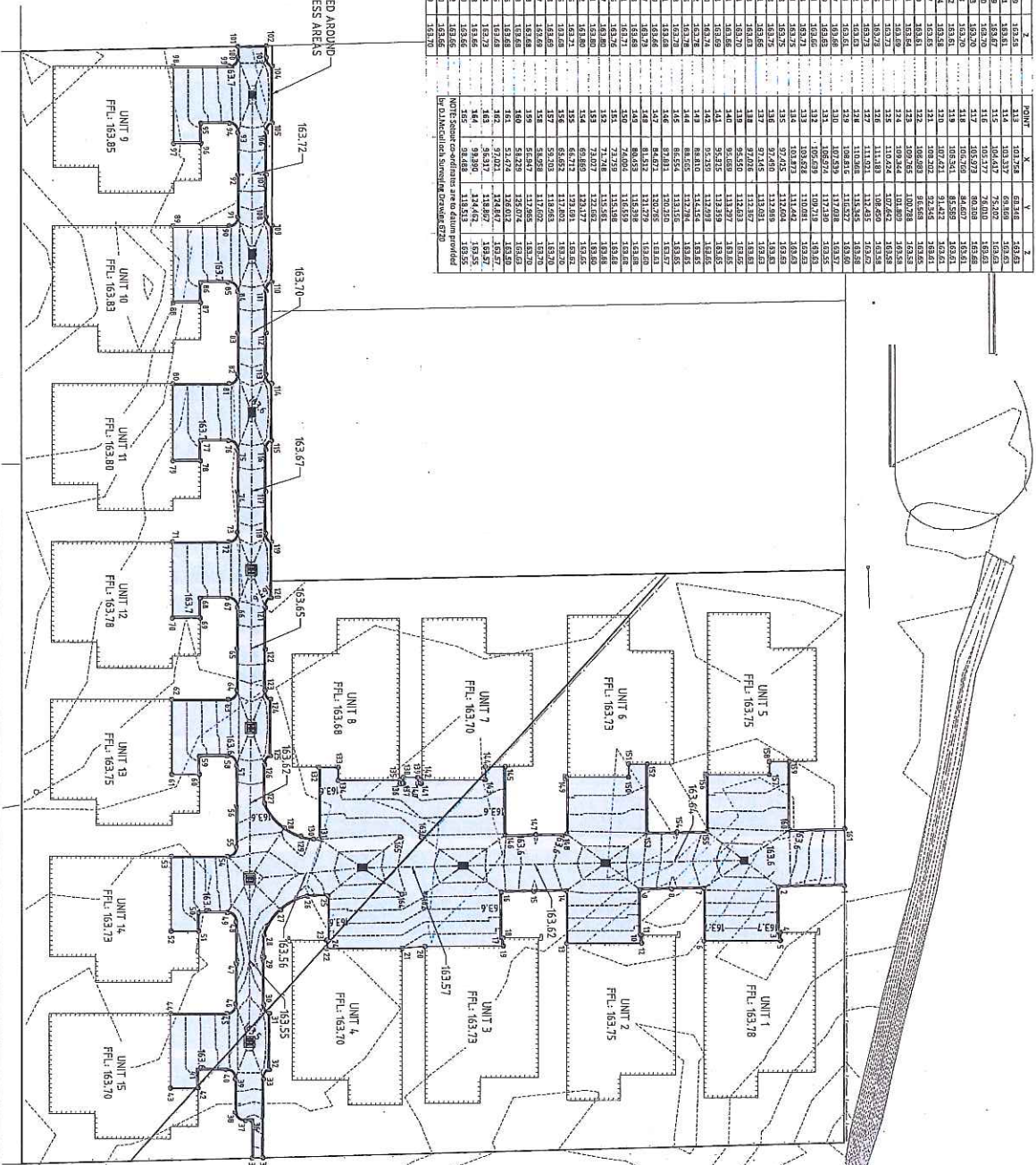
2-129

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4	61.210	136.321	104	114.419	107.489	194	103.724	61.344
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98	61.210	136.321	198	114.419	107.489	288	103.724	61.344
99	61.210	136.321	199	114.419	107.489	289	103.724	61.344
100	61.210	136.321	200	114.419	107.489	290	103.724	61.344

NOTE: See site plan for details of unit boundaries.

WARNING
 THE LOCATION OF EXISTING SERVICES IS APPROXIMATE ONLY AND NOT ALL SERVICES ARE SHOWN. THE EXACT LOCATION SHOULD BE PROVEN ON SITE BY THE CONTRACTOR AT HIS OWN RISK AND AT HIS OWN EXPENSE.

UNDO BARRIER KERB TO BE PROVIDED AROUND ENTIRE PAVEMENT EXCEPT ACCESS AREAS



- LEGEND**
- PROPERTY BOUNDARY
 - PROPOSED STORMWATER EXISTING CONTOURS (0.1M INTERVALS)
 - PROPOSED CONTOURS (0.025M INTERVALS)
 - EXISTING FENCE
 - PROPOSED UNITS
 - PAVEMENT BREAKLINES
 - NEW CONCRETE DRIVEWAY

EXHIBITED

LANDSTORM ENGINEERING
 brady@landstorm.com.au

HARRIS DALEY

PROPOSED MULTIPLE DWELLINGS CIVIL DESIGN PAVEMENT SETOUT
 49 GEORGE STREET, PERTH

DRAWING CHECK		DATE	
DESIGNED	BY	DATE	
CHECKED	BY	DATE	
APPROVED	BY	DATE	
DATE	SCALE	PROJECT	
15/01/2023	AS SHOWN	LS135.001-C02	
DRAWING CHECK		DATE	
DESIGNED	BY	DATE	
CHECKED	BY	DATE	
APPROVED	BY	DATE	
DATE	SCALE	PROJECT	
15/01/2023	AS SHOWN	LS135.001-C02	
DRAWING CHECK		DATE	
DESIGNED	BY	DATE	
CHECKED	BY	DATE	
APPROVED	BY	DATE	
DATE	SCALE	PROJECT	
15/01/2023	AS SHOWN	LS135.001-C02	

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16th October 2020

Mr Tim Daley
Harley Developments
tim@harrisdaley.com.au

Dear Tim,

Re: Assessment of Railway Noise Impact at 9 George Street, Perth.

Control of noise in Tasmania is regulated under the *Environmental Management and Pollution Control Act 1994*, and the *Environmental Management and Pollution Control (Miscellaneous Noise) 2016 regulations*. The Act defines environmental nuisance, which can include noise, and the regulations provide limitations on the levels and timing of the noise that may be emitted by various types of plant, vehicles or activities. Neither the act nor the regulations specify limitations to noise emitted by railway operations or the maximum acceptable noise levels inside or outside residences. However the Tasmanian *Environmental Protection Policy (Noise) 2009 (EPP)* does provide guidance on desirable maximum noise levels for different activities based on research originally published by the World Health Organization.

Table 1 of the EPP; *Acoustic environmental indicator levels* of the EPP, provides two measures of noise levels inside bedrooms that relate to avoiding sleep disturbance. The first relates to the equivalent continuous or "average" noise level ($L_{Aeq, 8hr}$) measured over an eight hour period and the second is the maximum noise level ($L_{Amaxfast}$) that should not be exceeded more than ten times during the night. The guideline limits for these measures are 30 dB(A) and 45 dB(A) respectively.

The proposed subdivision at 49 George Street, Perth, adjoins the Western Junction to Burnie railway line. The level of noise that may be expected from trains operating on this line was obtained from the *Noise Management Plan for the Tasmanian Railway Network*, January 2007. This plan specifies noise limits for Tasmanian railway operations of 70 dB(A) ($L_{Aeq, 24hr}$) and 95 dB(A) (L_{Amax}), measured 15m from the centerline of the railway.

The nearest unit in the proposed development is Unit 1, located in the NW corner of the site. (See Figure 1 below). The exterior walls of the rear bedrooms in the unit are located approximately 27m from the centerline of the railway line. (See Figure 2 below). Being a further 12m away from the line than the position where the limits are stated in the management plan, the maximum estimated external noise levels outside these bedrooms will be 64.9 and 89.9 dB(A) respectively.

The design drawings for the proposed house, indicate that it is to be built with brick veneer walls, with plasterboard lining, and R2 cavity insulation, a pitched sheet metal roof with standard foil sarking, min R4 ceiling insulation and a plasterboard ceiling, and double glazed windows.

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The noise attenuation ability of this construction has been estimated using the methodology and data provided in; *AS3671-1989 Acoustics – Road traffic noise intrusion – Building siting and construction*.

The noise reduction from outside to inside was found to be sufficient to reduce the outside noise to levels lower than the EPP guideline levels, in Bedroom 2, but not in Bedroom 3, unless a special window with a minimum acoustic noise reduction rating of 41 dB(A) is used.

Based on the above assumptions and calculations, and assuming that an acoustic rated window is specified in Bedroom 2, it may be concluded that the level of noise from the railway, inside Unit 1, is unlikely to cause sleep disturbance and therefore that residents should not suffer from environmental harm or reduced amenity as a result of railway noise.

Railway noise levels are acceptable at all other units in the proposed development.

Please do not hesitate to contact me if you have any queries.

Yours sincerely,



Douglas Ford
Senior Mechanical Engineer / Noise Specialist

Received
09.12.20

2-133

TRAFFIC IMPACT ASSESSMENT REPORT

Report prepared for:

49 George Street
Perth TAS 7300

Title Ref: 45677/4, 158071/1 & 158071/2

Property Id: 3428985

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EXHIBITED

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Document Information

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 Project Reference: 20/21 TAS 129
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 Version Number: V1
 Effective Date: 8 December 2020
 Date Approved: 8 December 2020

Document History

Version	Effective Date	Description of Revision	Prepared by:	Reviewed by:

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Appendix

Architectural Plans

1. Introduction

1.1 Overview

RJK Consulting Engineers has been commissioned by Design to Live to undertake a Traffic Impact Assessment (TIA) relating to proposed development at 49 George Street, Perth. Specifically, this TIA addresses the access provision of the proposed development in addressing Codes E4.7.1, E4.7.2 and E4.7.4. The proposed layout is provided in this report for reference.

The proposed development is located within the Northern Midlands Council Local Government Area (LGA) and is subject to their relevant planning controls. This TIA will form part of the Development Application and be submitted for proposal to Northern Midlands Council. It has been prepared in accordance with the Department of State Growth (DSG) guidelines.

1.2 Scope of Works

This traffic assessment will assess traffic impacts and any issues arising from the new access point, in particular:

- That the level of use, number of vehicles and layout maintains an acceptable level of safety, for all road users including pedestrians and cyclists;
- That the car parking and manoeuvring spaces are convenient, safe and efficient to use;
- Show the turning circles for all car parking spaces, including the swept paths.
- That the safety and efficiency of the railway is not compromised.

It will also demonstrate:

- Impact on the surrounding road network;
- The intersection with Main Road;
- Any recent crash statistics in the vicinity;
- Any changes required to accommodate the additional traffic.

1.3 Report Objectives

The objective of this report is to evaluate the impact of traffic generated by the project. It will also aid in the planning and design of sustainable development proposals by taking into consideration:

- Safety and capacity;
- Equity and social justice;
- Efficiency and the environment and;
- Traffic projections for 10 years.

RJK's objectives for this study include:

- Review and collate background documents in relation to the development;
- Assessing access performance in accordance with code E4;
- Identify any mitigating measures required as a result of the proposal.

1.4 Reference Documents & Data Sources

RJK Consulting Engineers have been provided by the client relevant information on the development. These detail an outline of the work and that the development generally proposes no significant change to the existing traffic arrangements.

The following documents have been referenced as part of this study:

- www.THELIST.tas.gov.au;
- DSG 'Traffic Impact Assessment (TIA) Guidelines';
- DSG Tasmanian State Road Hierarchy;
- Northern Midlands Council Interim Planning Scheme (2013);
- Transport and Main Roads Road Planning and Design Manual – Edition 2; Vol 3;
- Various Austroads publications.

2. Site Description

This chapter reviews the existing road network and transport conditions surrounding the proposed development site.

2.1 Site Location & Description

The site is located on 3 lots: 45677/4, 158071/1 & 158071/2, is approx. 6754m² in total size and zoned General Residential. It is situated on the corner of George and Little Mulgrave Street, Perth, both of which are no-thru roads. It has a railway line running approx. 11.4 metres from the north-west corner boundary.

There is currently an access point on George Street which services the vacant lots. The subject site is identified in **Figure 1**.

Figure 1 - Site Location



3. Proposed Development & Planning Scheme

3.1 Development Details

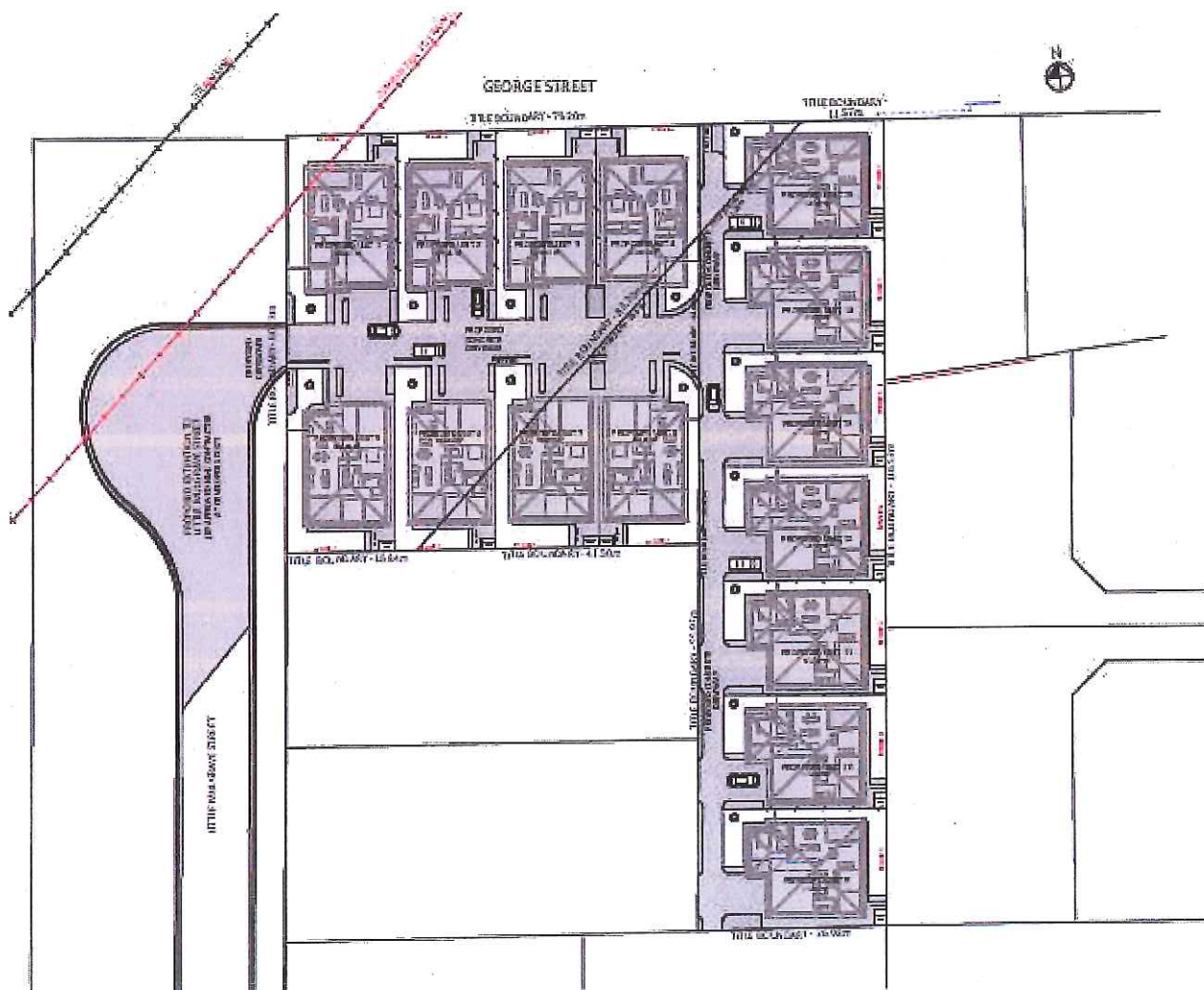
The development as proposed provides for title consolidation and multiple dwellings as per the layout plan below.

The proposal consists of 15 units on the amalgamated title, with a single access to all dwellings located at the end of Little Mulgrave Street. The proposal includes an extension to Little Mulgrave Street and a new access point located within the extension. This access will be the only access point for the site. The new access will need to meet the Austroads Standards in terms of construction. This proposed access will also need to be constructed in accordance with IPWEA LGAT municipal standard drawings in terms of dimensions, etc. and will need to be a level sealed junction of suitable material, in keeping with the urban road profile. These standard drawings can be found on the LGAT website as reference.

The interface at the roadside edge along Little Mulgrave Street appears ideal for the proposed extension / development and alignments can easily be matched in.

It is noted that Little Mulgrave Street can easily accommodate additional vehicle movements arising from this development, based on likely low vehicle numbers existing and generated additional (with satisfactory manoeuvre of existing arrangements during site visits).

Figure 2 - Proposed Layout



3.2 Council Planning Scheme

The proposed development involves land currently zoned General Residential in accordance with the Northern Midlands Council Interim Planning Scheme 2013.

4. Existing Conditions

4.1 General Transport Network

The local transport system consists of Little Mulgrave Street, Main Road, Mary Street and Elizabeth Street.

Little Mulgrave Street

Little Mulgrave Street is a no-thru road which provides access to and from Main Road for residential developments as well as being used for patrons utilising Lions Park, which is located on the corner of Little Mulgrave Street and Main Road. Little Mulgrave Street intersects with Main Road at a give-way intersection with priority to Main Road.

The street is urban in nature and has concrete kerb and channel on both sides. The road surface is in good condition, with a two-coat sealed width of approximately 7.5 metres as you enter Little Mulgrave Street, then widens to approximately 9.7 metres to accommodate parking alongside Lions Park on the western side of the road. At the end of the street the road widens further to accommodate a turning vehicle. At this point the road width is approx. 14.5 metres. The road is flat and is built to an urban standard in keeping with Table 1 - TSD R06 V1.

Based on the small volume of additional traffic generated by the new development, an inspection of the road and surrounding areas identified that the general operations of the street, as currently operating, will not have any noticeable level of service loss. This assessment is also based on traffic count and speed data from NMC for this vicinity.

Speed profiles and traffic count data were not available for Little Mulgrave Street, however as there are 8 properties utilising the street, the current daily traffic flow is estimated as 59.2 (8 x 7.4) for the properties. Additionally, daily traffic count estimates for Lions Park and the amenities usage need to be considered. Based on the availability of parking on both Little Mulgrave Street and Main Road and given the amount of street parking the daily traffic flow is estimated at 40 cars per day. This gives the combined traffic count at around 100 vehicles per day.

Based on the authors assessment and local knowledge of the road network, the traffic volumes for this road are currently satisfactory.

Default speed limit of 50 km/hr applies for Little Mulgrave Street.

Main Road

This is the main road through the township of Perth. It predominantly carries local traffic since the recent opening of the Midland Highway bypass – Perth Link Road Project. The road in the vicinity of the site is sealed, single lane each way and has concrete kerb and channel on the same side of the road as the intersection with Little Mulgrave Street, whilst the opposite side of the road has swale drains.

There is a railway crossing situated approx. 145 metres to the north of the intersection with Little Mulgrave Street.

Default speed limit of 60 km/hr applies for Main Road vicinity of the junction with Little Mulgrave Street.

Mary Street

Mary Street intersects with Main Road opposite the junction with Little Mulgrave Street. It loops around and becomes King Street to intersect further south with Main Road. It services residential developments and has a default speed limit of 50 km/hr.

Elizabeth Street

Elizabeth Street intersects with Main Road approx. 7 metres to the south of the junction with Little Mulgrave Street. It is a feeder road servicing residential developments, Perth Primary School and local community and recreation facilities.

Figure 3 - Local Road Network

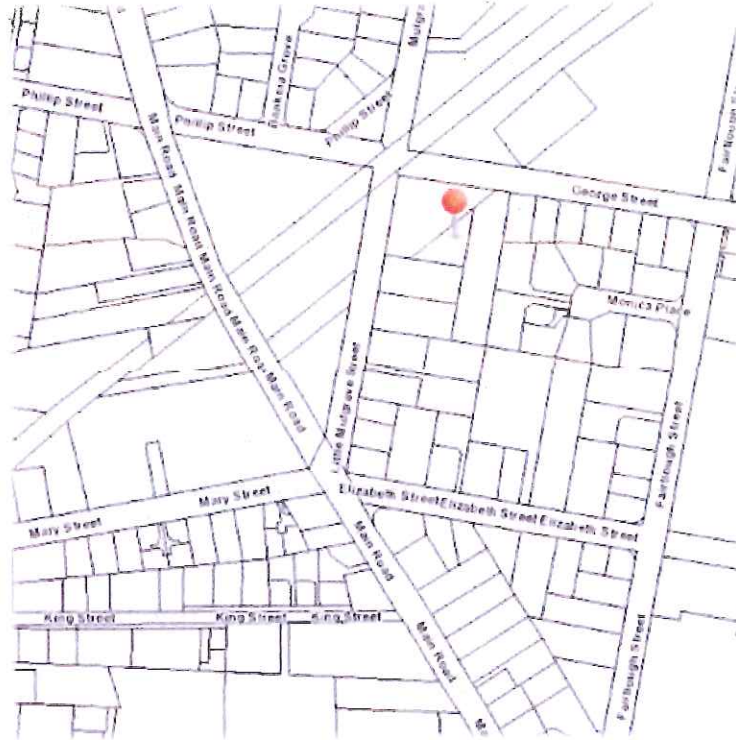


Figure 4 – Streetscape – Little Mulgrave Street taken from junction with Main Road



Figure 5 – Looking north toward the area where the road is proposed to be extended



Figure 6 – Looking south at the area where the road is proposed to be extended

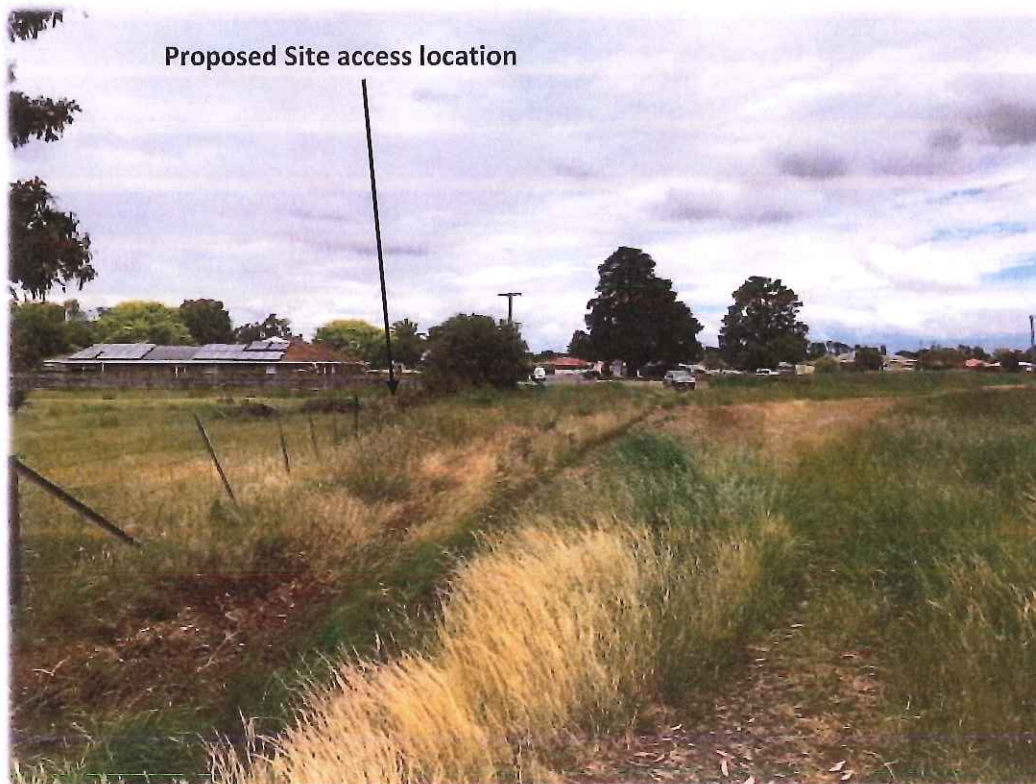


Figure 7 – Approx. location of proposed crossover for the proposed development



Figure 8 – Looking left from vicinity of proposed site access



Figure 9 – Looking right from vicinity of proposed site access



4.2 Traffic Activity & Generation

Northern Midlands Council have advised there is no historic traffic count data for Little Mulgrave Street. Traffic volumes have therefore been estimated at 99 vehicles per day.

DSG have provided historic traffic count data for Main Road taken from near the intersection of Clarence Street from 2019. This identified traffic volumes averaged 6700 per day. Since this time the Midland Highway bypass has been built and as such the counts are considered relative. It is noted that some heavy transport has been reduced with the relocation of services to the bypass.

The development proposes to generate a relatively small amount of additional traffic beyond current levels. Based estimated traffic count, the small volume of additional traffic generated by the new development, along with an inspection of the road and surrounding areas, it is identified that the general operations of the street, as currently operating, will not have any noticeable level of service loss.

Default speed limit of 50 km/hr applies for Little Mulgrave Street and 60 km/hr for Main Road in the vicinity of the junction.

4.3 Crash History

DSG advised 4 crashes in the vicinity of Main Road/Little Mulgrave Street junction, but no crashes in Little Mulgrave Street for the last 9 years. The crash data documents the nearest 2 crashes at the junction where Main Road and Little Mulgrave Street intersect. With another 2 crashes further along Main Road, one to the north and one to the south. These incidents imply that the current traffic volumes and general arrangements for the surrounding area and the site, are currently operating satisfactory. See **Figure 10** for Crash history details & **Figure 11** for Crash locations.

Figure 10 - Crash history details

Crash No.	Crash Date	Severity	Description	Location	Light Cond.	Surface	Units	Unit Types
30063971	15/07/2011	Property Damage Only	U turn	Intersection of Main Road, Elizabeth St, Mary St & Little Mulgrave St, Perth	Daylight	Sealed	2	Light vehicle
30104480	04/12/2012	First Aid	Overtaking	Intersection of Main Road, Elizabeth St, Mary St & Little Mulgrave St, Perth	Daylight	Sealed	2	Heavy vehicle/Light vehicle
133877	06/10/2013	Property Damage Only	U turn	Main Road, Perth	Daylight	Sealed	2	Light vehicle
49428864	24/07/2018	Property Damage Only	Reversing into fixed object or parked vehicle	Main Road, Perth	Daylight	Sealed	2	Light vehicle/heavy vehicle

Figure 11 - Crash Locations

4.4 Road Safety Review

Based on sight observations and the information regarding crash history, the road network in this area appears to function satisfactorily, and provides appropriate width and manoeuvrability based on road hierarchy and the limited traffic numbers.

Due to the development not being seen as a major contributor resulting in a dramatic increase in traffic volumes being generated, further off-site impacts are not considered.

5. Traffic Generation & Assignment

5.1 General

This section of the report describes how traffic generated by the proposal is distributed within the adjacent road network now (considered for time of application – 2020) and in ten years (2030).

5.2 Traffic Growth

The rate of background traffic growth on Little Mulgrave Street for projection purposes is taken to be 2%, as per ABS supplied trend analysis of population growth data.

5.3 Trip Generation & Distribution

In order to analyse the impact of the development on the existing transport infrastructure, it is necessary to assess the number of trips likely to be generated to and from the site and where they are likely to travel. To determine an appropriate rate to forecast trip generation information has been sought from;

- RTA Guide to Traffic Generating Developments;
- Austroads Guide to Traffic Management Part 12; and
- Department of Main Roads' Road Planning and Design Manual: Chapter 3.

5.4 Trip Generation

The development comprises of fifteen 3 bedroom dwellings. Traffic generated from the site is likely to provide an increase in vehicle movements along Little Mulgrave Street, Main Road and the wider network, adding around 333 vehicles per day (refer RTA Urban Mixed Use dwellings which indicates 7.4 per dwelling).

The existing Little Mulgrave Street and Main Road can easily accommodate this increase in traffic based on site inspections, authors understanding and local knowledge of the area and site, and with the lessening of traffic due to the highway bypass.

Due to the development not being seen as a major contributor resulting in a dramatic increase in traffic volumes being generated, further off-site impacts are not considered.

5.5 Surrounding Road Impacts

The assessment of the impact of the proposed development on the street network has been undertaken.

Due to the limited additional traffic being generated from the development, volumes are not considered material and would have limited impact on the wider road network. As such an assessment of additional road network parameters beyond the site are outside the formal responsibility of this report.

6. Car Parking and Internal Assessment

6.1 Car Parking Provision

The Northern Midlands Council Parking Code incorporates different parking rates for specific land use components. As the development relates to an improvement in facilities, the assessment needs to have regard that a certain number of car spaces be provided for each development type based on a set of criteria, e.g. number of bedrooms per unit in a residential area.

Particular provisions are also included for car parking in residential areas and for those uses that require pick-up, set-down, loading areas, lighting, landscaping and provision for manoeuvring of vehicles on the site. Each use may therefore have a certain number of car parking spaces, as well as access requirements for the use.

The proposed concept plans prepared by Design to Live are presented in the **Appendix**. It can be seen that Council's parking rates have been applied to the net increase in areas associated with each land use component and the following factors are relevant for the main uses:

- The net 15 residences attract a rate of 2 spaces per unit, with 1 dedicated space per 3 dwellings, so that 35 spaces are required;

In response to this, the development proposes 60 spaces. Each dwelling has a 2 car garage, a car park and provision for a visitor car space.

A total of 60 spaces will be accessed via the proposed double entry-exit driveway off Little Mulgrave Street.

6.1.1 Disabled Parking

Nil required.

6.1.2 Access

Access to the site is proposed via a dedicated driveway for the 15 units to Little Mulgrave Street which is a local road. The driveway will be created with the extension of Little Mulgrave Street. Safe Intersection sight distance has been assessed as suitable given the location at the end of a no through road

6.1.3 Internal Design

The internal design complies with the requirements of AS 2890.1 and incorporates the following key components:

- Parking spaces are generally 2.5 metres in width with a minimum aisle width of 5.8 metres;
- Circulation within the site and access to parking spaces can be readily accommodated as demonstrated by swept paths (refer to Figure 13); and

In summary, the internal design is considered satisfactory and will provide a good amenity.

6.2 Development Parking Requirements

The Northern Midlands Council Interim Scheme 2013, Code E6 requires parking for development.

Based on the above, the development complies with requirements.

7. Impact on Road Network

7.1 Impact on Liveability, Safety & Amenity of the Local Area

The traffic introduced by the additional proposal (333 vph) can be easily absorbed by the road at peak times with gaps in the traffic flow. The proposal has negligible impact on the operation of Little Mulgrave Street and Main Road.

Impact on road users is minimal including public transport, pedestrians, cyclists and motorists. Traffic generated by the proposal will not impact above ground services nor will increase environmental impacts such as noise, visual and pedestrian amenity. Both Little Mulgrave Street and Main Road have street lighting and there is no requirement for additional roadside furniture such as directional signs and fencing.

7.2 Parking Assessment

Not required to be assessed.

7.3 Sight Distances

A sight specific assessment on the site was undertaken to review sight distance with consideration of the MVC Planning Scheme requirements and in accordance with Clause 3.4 of *Austrroads Guide to Road Design part 3: Geometric Design*. Sight distance is measured along the carriageway from the approaching vehicle to the conflict point.

The posted speed limit on Little Mulgrave Street is 50km/h. Sight distance requirements are summarised in *Austrroads Guide to Road Design – Part 4A (2010) Table 3.2* and indicate a required SISD of 80m for 50Km/Hr. However, it is noted under item (a) Vehicle speed is the actual or recorded speed of traffic passing along the road and is the speed at or below which 85% of passing vehicles travel. In this case, due to the road dynamics 48.94km/hr, which yields a SISD of 61.31m.

$$SISD = \frac{D_t \times V}{3.6} + \frac{V^2}{254 \times (d + 0.01 \times a)}$$

D _t =	4.5	decision time Table 5.2 (below) + 3s
V =	48.94	km/h
D=	69	Table 5.3 (Below)
A=	2	longitudinal grade in %

$$SISD = \boxed{61.31} \text{ m}$$

As noted from plans, photos and measured on site for the proposed site access, SISD is limited to one direction due to the proposed driveway being located at the end of a no through road. Refer to paragraph 7.6 for further discussion.

7.4 Pedestrian and Bicycle Movements

No changes are proposed for consideration to the external pedestrian and bicycle movements areas.

7.5 Road Safety & Traffic Service

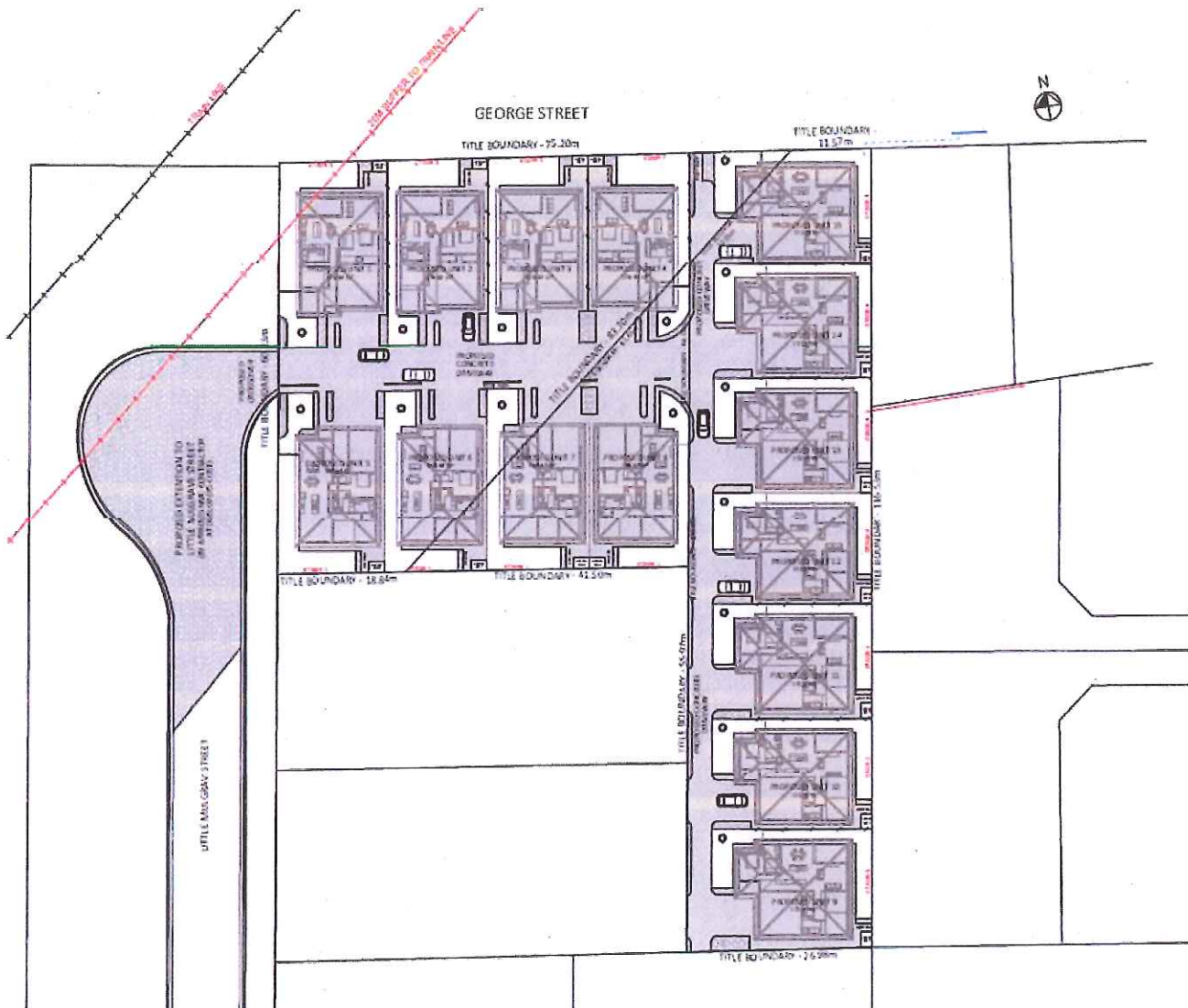
Due to the sight distance deemed to be meet Austrroads guide, road safety appears not to be compromised by the establishment of the road extension and creation of entrance/exit point.

Traffic service is believed to be adequate with the existing infrastructure based on the proposed traffic volumes.

7.6 Intersection Sight Distance

Sight measurements were taken to evaluate the proposed Safe Intersection Sight Distance. Under the Planning Scheme 80m is required each way, however, it should be noted that the site distance is restricted to the left only as the proposed development is located at the end of a no through road. To the left (south) showed approx. 162.5 metres to the controlled intersection with Main Road.

Figure 12 - Concept Layout Plan



7.7 Delivery Vehicles

Not required to be assessed.

7.8 Vehicular Entry to Property

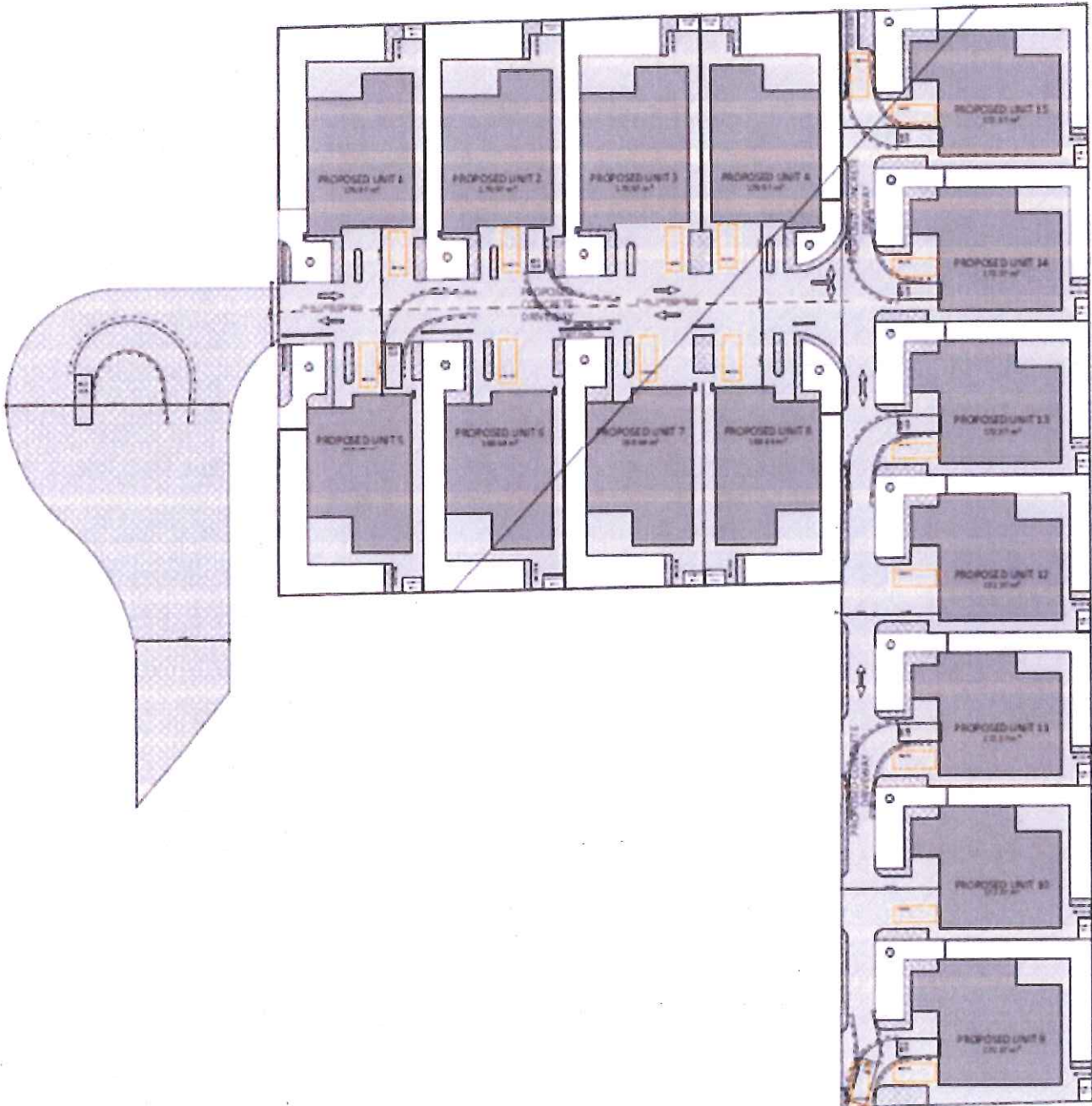
All vehicles will enter the proposed development via a single double driveway on Little Mulgrave Street. This driveway provides shared vehicle access accommodating all dwellings.

In assessing the swept vehicle path a number of factors have been considered: the geometry of the roadway and site; the appropriate vehicle length; and the speed at which the vehicle is approaching the access. As

per Austroads turning paths templates, a vehicle 5.2m in length has been considered with respect to the radius, which covers the capabilities of vehicles that may be expected to operate in this particular location. The low speed when approaching then end of the road has been also been assessed in the turning path study and as such the swept path demonstrates that the intersection will operate efficiently and satisfactorily.

As shown in Figure 13 the safety aspect of vehicle movements is satisfied as the double crossover provides adequate width.

Figure 13 - Vehicle Swept Path



8. Planning Scheme Response

The Northern Midlands Council Interim Scheme 2013, Codes E4.7.2 & E4.7.4 requires addressing of safety issues for traffic for the development. Based on the above the following responses are offered:

E4.7 Development Standards

E 4.7.2 Management of Road Accesses and Junctions

Objective:

To ensure that the safety and efficiency of roads is not reduced by the creation of new accesses and junctions or increased use of existing accesses and junctions.

Acceptable Solutions	Performance Criteria	Response
A1 For roads with a speed limit of 60km/h or less the development must include only one access providing both entry and exit, or two accesses providing separate entry and exit.	P2 For roads with a speed limit of 60km/h or less, the number, location, layout and design of accesses and junctions must maintain an acceptable level of safety for all road users, including pedestrians and cyclists.	Meets A1 As outlined in the plan, the proposal is to create a single new vehicular entry / exit point for the development off a created road extension.

E 4.7.4 Sight Distance at Accesses, Junctions and Level Crossings

Objective:

To ensure that use and development involving or adjacent to accesses, junctions and level crossings allows sufficient sight distance between vehicles and between vehicles and trains to enable safe movement of traffic.

Acceptable Solutions	Performance Criteria	Response
A1 Sight distances at a) an access or junction must comply with the Safe Intersection Sight Distance shown in Table E4.7.4; and b) rail level crossings must comply with AS1742.7 Manual of uniform traffic control devices - Railway crossings, Standards Association of Australia; or c) If the access is a temporary access, the written consent of the relevant authority has been obtained.	P1 The design, layout and location of an access, junction or rail level crossing must provide adequate sight distances to ensure the safe movement of vehicles.	Meets A1 As the intersection does not cross the rail line due to ending short - nil impact.

9. Summary

This Traffic Impact Assessment (TIA) has been prepared by Risdan Knightley of RJK Consulting Engineers on behalf of Design to Live for 49 George Street, Perth within Northern Midlands Council, Tasmania. The intention of the Traffic Impact Assessment is to support a Development Application and provide improved facilities for users of the development.

The report is summarised as follows:

- The assessment has reviewed the adjacent accesses directly affected, the immediate road network serving the area, swept vehicle paths, road conditions and crash history. No significant traffic safety issues were apparent;
- The relative increase in traffic associated with the proposal will be minimal and is acceptable noting adequate SISD is achieved and does not compromise safety;
- The proposed development network is generally in accordance with Northern Midlands Council Interim Planning Scheme;
- The increased traffic experienced by Little Mulgrave Street and Main Road is assessed as within the acceptable range in terms of impact on local amenity. The local road network is assessed as being able to cope with the increased traffic activity;

It is therefore concluded that the proposed development is supportable on traffic planning grounds and the proposed development will operate satisfactorily. This report demonstrates that the proposed development can be satisfactorily accommodated within the existing road network and the future road hierarchy adopted for the area.

10. Regulatory Feedback

10.1 Council Feedback

Traffic count data was made available via website link.

10.2 DSG Feedback

DSG provided crash statistics, with only 2 crashes in the immediate vicinity on Main Road in the last 9 years.

11. Conclusion

This TIA has investigated the potential impacts for the creation of the residential development.

Key conclusions are:

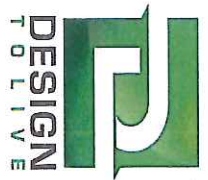
- The proposed new access point and road extension is to be located as per the attached plan. The extension and access is to be constructed to council Urban Standards as per the Tasmanian Standard drawing series;
- Traffic services is deemed adequate for by the road and access arrangements as proposed and will be satisfactory in servicing the development;
- Sight distance for the new access is deemed to comply with the Austroads requirement, and sound SISD is available generally based on the site assessment;
- No other Planning Scheme requirements are outstanding.

I, Ridsen Knightley as a qualified chartered engineer and Fellow of Engineers Australia conclude based on the assessment of information available, that the traffic aspects associated with the development are adequate and meet the requirements for traffic, safety and service. I also note that there appears to be no other potential adverse effects on existing traffic situations, subject to the recommendations and conclusions noted.

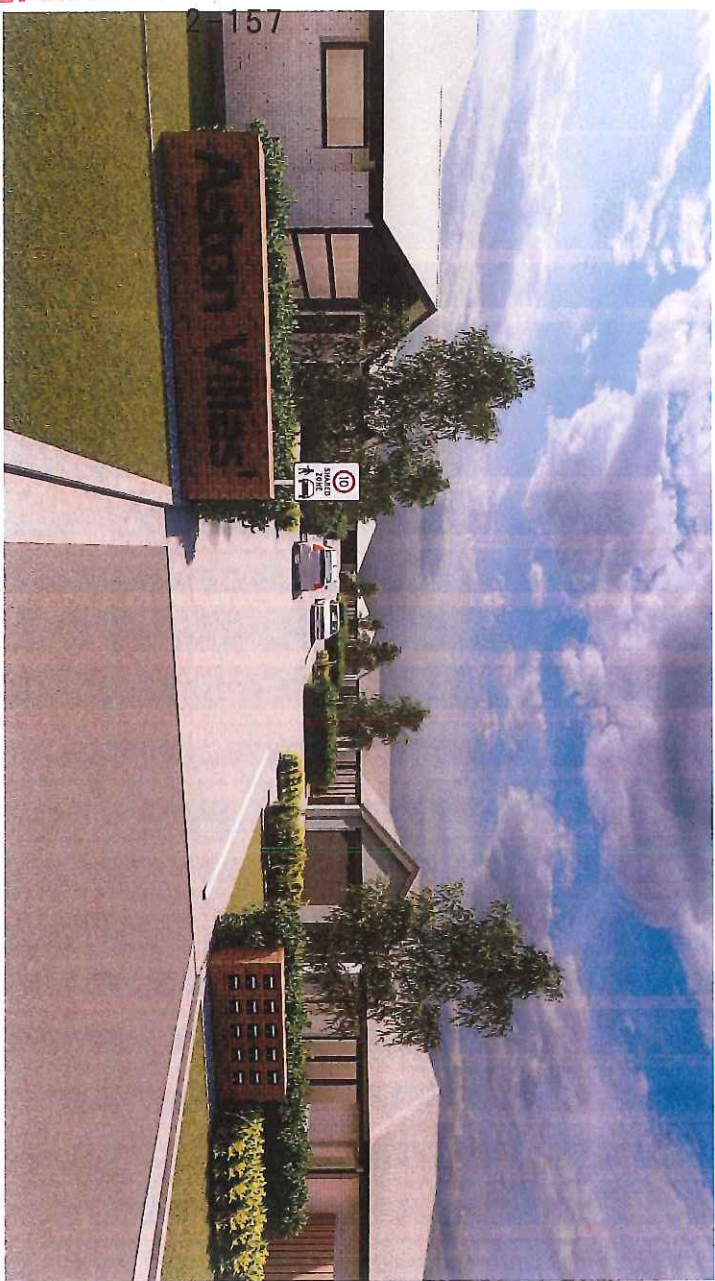


Ridsen Knightley

BE (Civil), Ass Dip Civil Eng, FIEAust, CC 2539X



**PROPOSED TITLE CONSOLIDATION AND
MULTIPLE DWELLINGS**
49 GEORGE STREET,
PERTH, 7300.



DRAWING #	DRAWING
GRC549-1	COVER PAGE
GRC549-2	SITE PLAN
GRC549-3	SITE DETAIL 1
GRC549-4	SITE DETAIL 2
GRC549-5	STRATA PLAN
GRC549-6	GROUND PLAN UNIT TYPE A
GRC549-7	GROUND PLAN UNIT TYPE B
GRC549-8	GROUND PLAN UNIT TYPE C
GRC549-9	EXTERNAL SERVICES
GRC549-10	ELEVATIONS UNIT TYPE A
GRC549-11	ELEVATIONS UNIT TYPE B
GRC549-12	ELEVATIONS UNIT TYPE C
GRC549-13	SITE SECTIONS
GRC549-14	SITE SECTIONS
GRC549-15	LANDSCAPE PLAN
GRC549-16	PARKING AND TURNING
GRC549-17	PERSPECTIVES
GRC549-18	PERSPECTIVES
GRC549-19	PERSPECTIVES

ATTACHMENTS

NOISE ASSESSMENT - GEORGE ST NOISE /DGF

COUNCIL	ZONE	SITE HAZARDS	TBAIN LINE WITHIN ZONE
NORTHERN MIDLANDS	GENERAL RESIDENTIAL	PLANNING OVERLAY	RUSTREE FROWTH URBAN GROWTH
PROPOSED UNIT 1	PROPOSED UNIT 10	LAND TITLE REFERENCE	45877/4, 158071/1&2
PROPOSED UNIT 2	PROPOSED UNIT 11	PROPERTY ID	34289985
PROPOSED UNIT 3	PROPOSED UNIT 12	LOT SITE (M ²)	2835, 927 & 2992
PROPOSED UNIT 4	PROPOSED UNIT 13	ENERGY STAR RATING	TBC
PROPOSED UNIT 5	PROPOSED UNIT 14	BAL RATING	TBC
PROPOSED UNIT 6	PROPOSED UNIT 15	DESIGN WIND CLASS	TBC
PROPOSED UNIT 7		SOIL CLASSIFICATION	TBC
PROPOSED UNIT 8		CLIMATE ZONE	7
PROPOSED UNIT 9		ALPINE AREA	N/A
		CORROSION ENV	N/A

CLIENT/S:	DRAWING COVER PAGE	DATE	DATE
MITCHELL LLOYD ACC # 371799313 48N, 71 615 812 747 PH. 4344 7319 E. info@designtolive.com.au	HARRIS DALEY 1000 GEORGE STREET, PERTH, 7300.	I/VE APPROVE THESE DRAWINGS TO BE CORRECT FOR CONTRACT.	
		SIGNATURE:	DATE:

REV.	DATE	DESCRIPTION	DESIGNER	CHECKED	JOB NUMBER	SCALE
R1	20/10/2020	FOR D.A.	L.S.	M.L.	GRC549	1/19
						NTS

EXHIBITED

EXHIBITED

DESIGN
 P L L
 MICHIEL UYOND
 ACC # 27179913
 ABN 71 415 812 747
 PH 4244 7319
 E: info@designplll.com.au

CLIENT/S:
 HARRIS DALEY
SITE ADDRESS:
 49 GEORGE STREET,
 PERTH, 7200.

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 SITE PLAN

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RT	30/11/2020	FOR D.A.

DESIGNER	M.L.	JOB NUMBER
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CHECKED <td>M.L. <td>2/19</td> </td>	M.L. <td>2/19</td>	2/19
	SCALE (9x2)	1:400

LOCALITY PLAN
 NOT TO SCALE



EXHIBITED

CUSTOMER:
HARRIS DALEY

DRAWING:
SITE DETAIL 1

SIGNATURE:
CORRECT PER CONTRACT

DATE:

DATE:

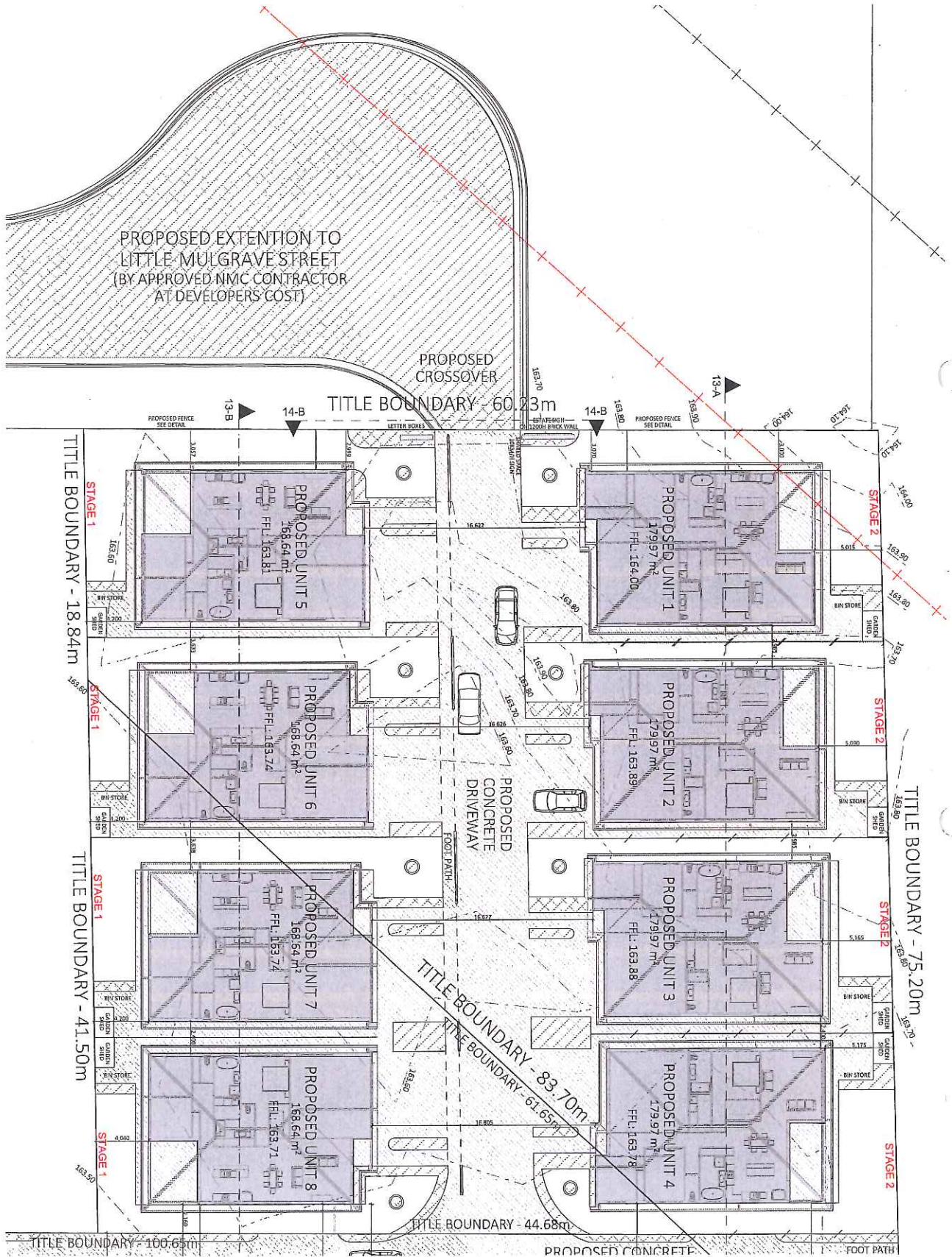
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R1	30/07/2020	FOR D.A.				

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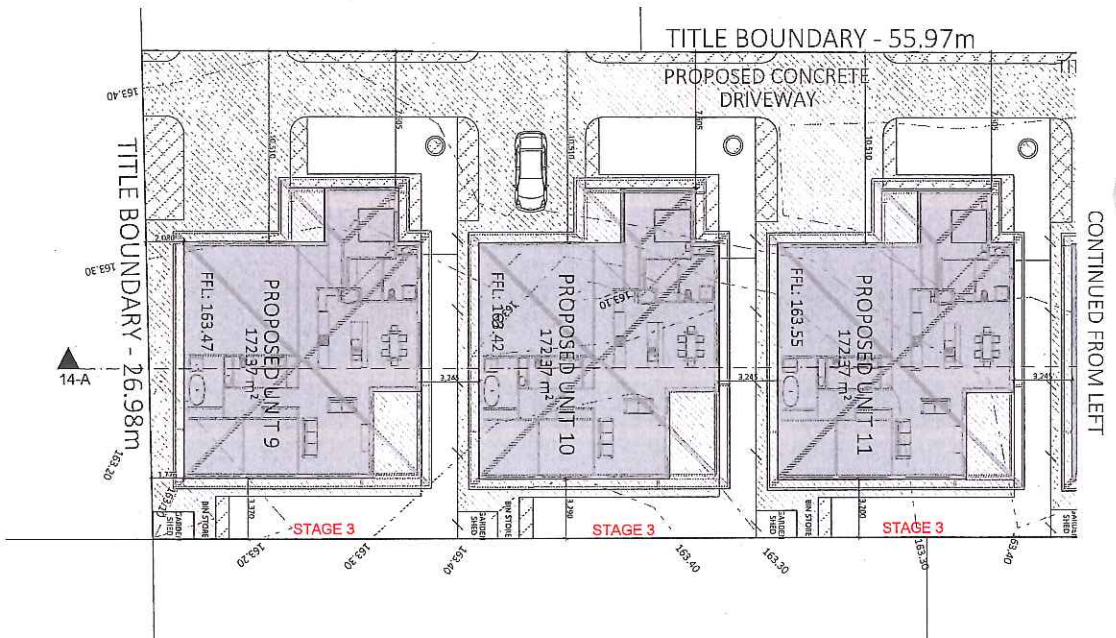
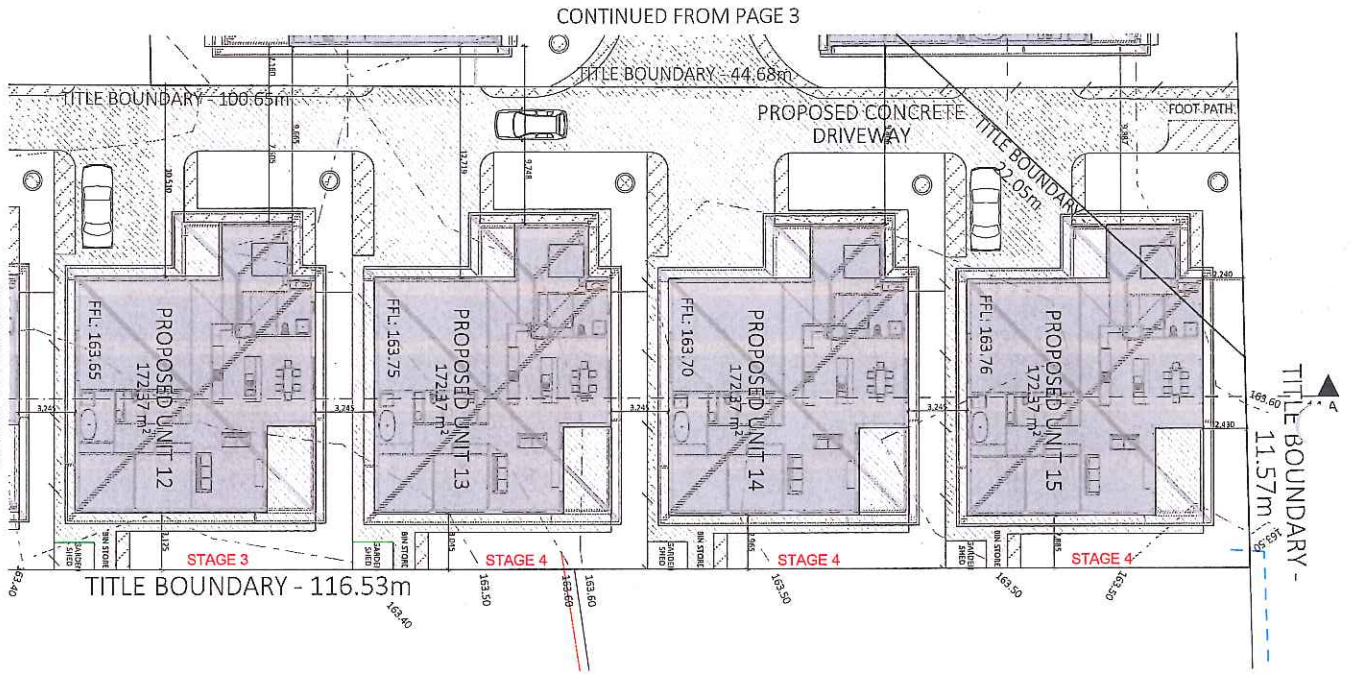
DESIGN
7 DESIGN
MICHILLIORD
ACC # 97729313
ARN 71 615812747
PH 6544 7319
E info@designordie.com.au

CLIENT/S:
HARRIS DALEY
SITE ADDRESS:
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PERTH, 2000.

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			DRAWN	L.S.	DRAWING	4/19
			CHECKED	M.L.	SCALE (@A2)	1:200



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MITCHELL LYOND
ACC # 371799913
ABN 71 615 812 247
PH 6344 7319
E. mlo@mlldesign.com.au

CLIENT/S:
HARRIS DALEY

DRAWING
STRATA PLAN

SITE ADDRESS:
49 GEORGE STREET
PERTH, 7000

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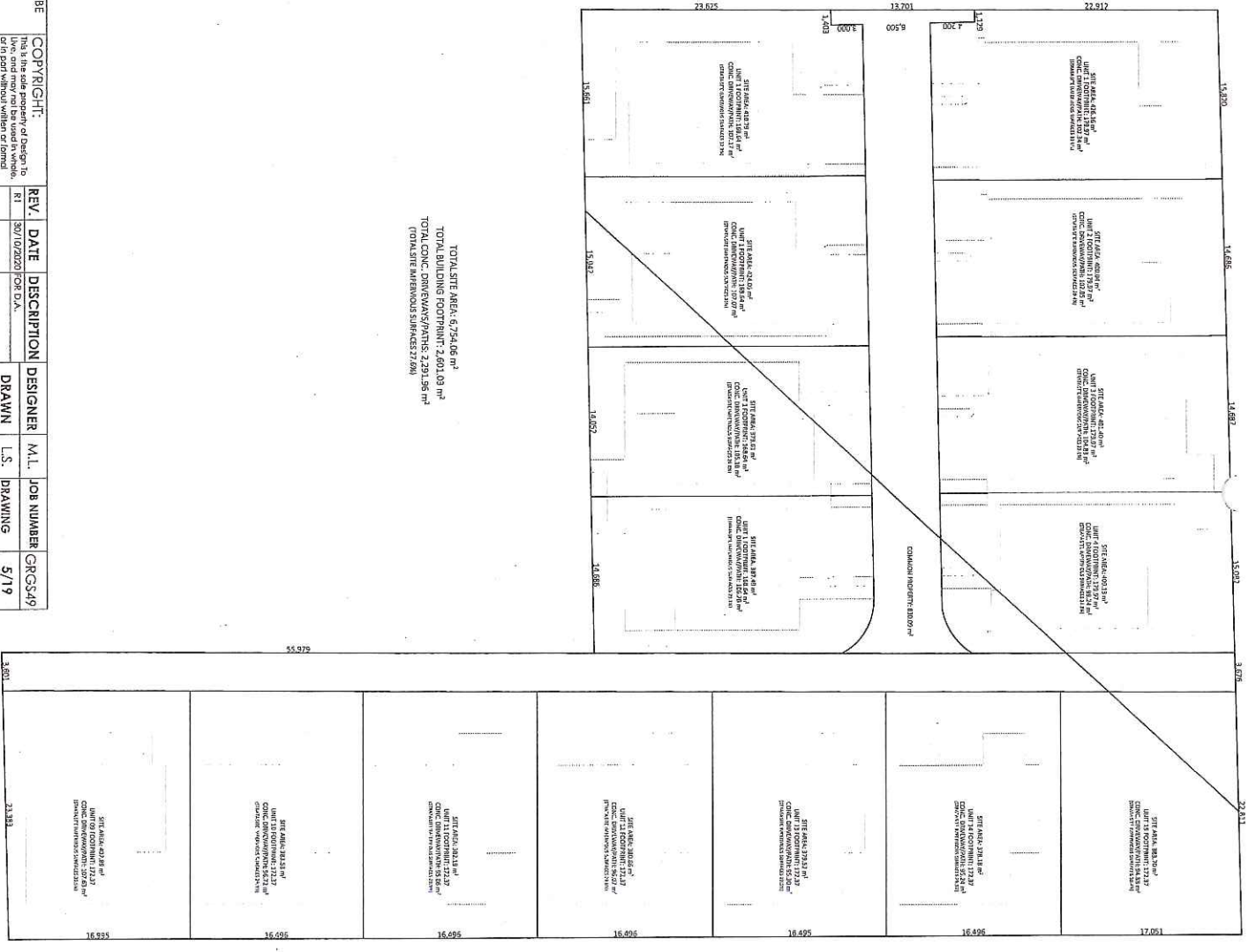
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REV.	DATE	DESCRIPTION	DESIGNER	CHECKED	M.L.L.	JOB NUMBER	CRGS#49
RI	30/09/2020	FOR D.A.					

TOTAL SITE AREA: 6,754.06 m²
TOTAL BUILDING FOOTPRINT: 2,601.03 m²
TOTAL CONC. DRIVEWAYS/PATHS: 2,291.96 m²
(TOTAL SITE IMPERVIOUS SURFACES 7,694)





 MITCHELL LUNDY
 ACC # 97179313
 HARRIS DALEY
 PH. 634.7319
 E: hld@designprive.com
 F: hld@designprive.com

CLIENT/S:
 HARRIS DALEY
 SITE ADDRESS:
 47 GEORGE STREET,
 PERTH, 2600

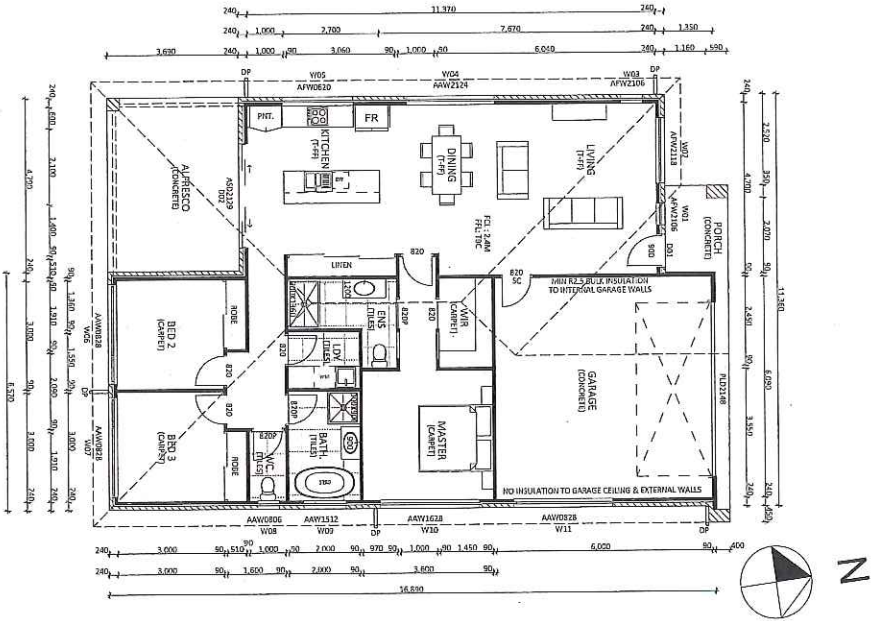
DRAWING
 GROUND PLAN
 UNIT TYPE A
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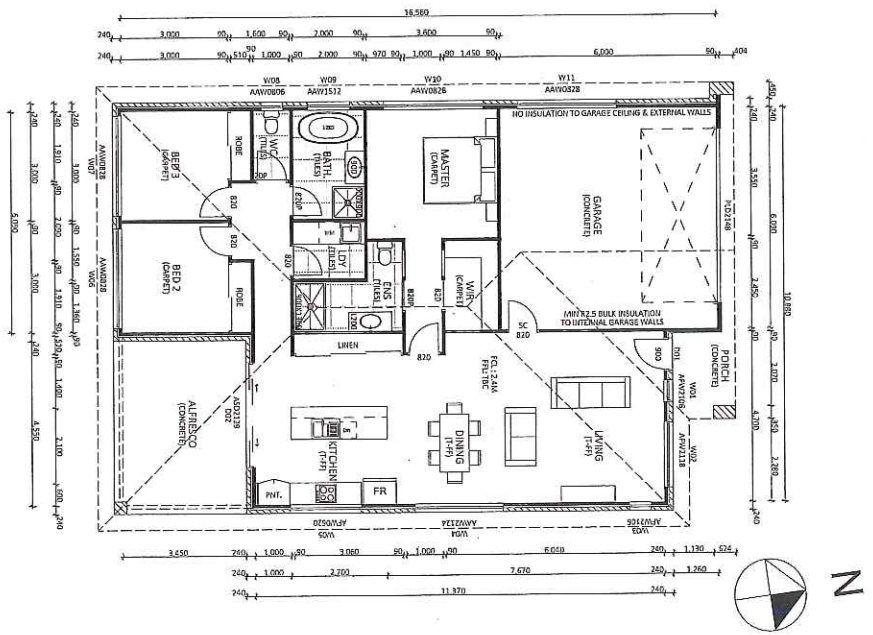
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E1	30/10/2020	FOR D.A.

DESIGNER	M.L.	JOB NUMBER
DRAWN	L.S. <td>6/19</td>	6/19
CHECKED	M.L. <td>SCALE @A2</td>	SCALE @A2

UNIT 1 FFL: 164.00
 UNIT 2 FFL: 163.89
 UNIT 3 FFL: 163.88



UNIT 4 FFL: 163.78



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MITCHELL LLOYD
ACC # 27179913
ASN 271 615 822 747
P/L 644 731 19
E: ml@designhive.com.au

CLIENT/S: HARRIS DALEY
SITE ADDRESS: 47 GEORGE STREET, PERTH, 2000.

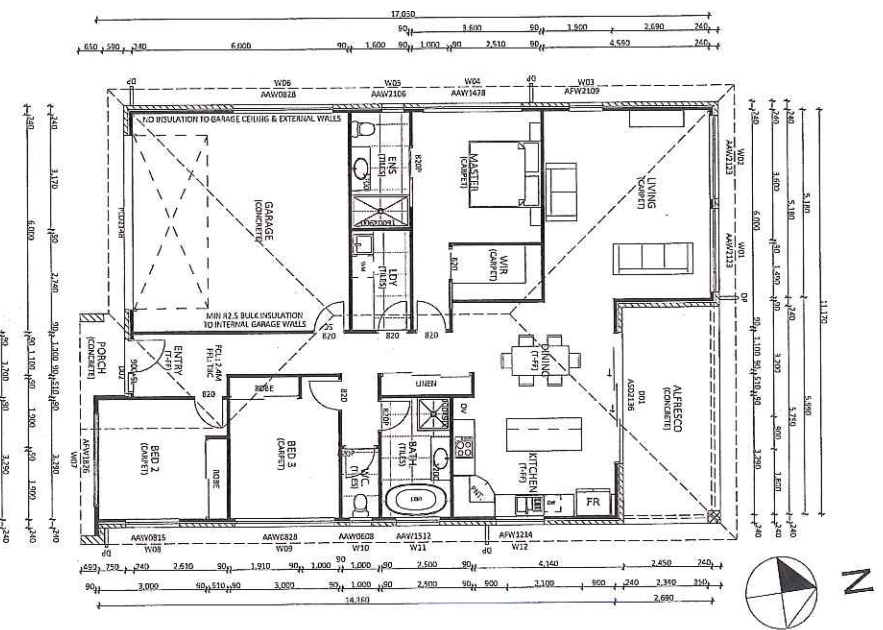
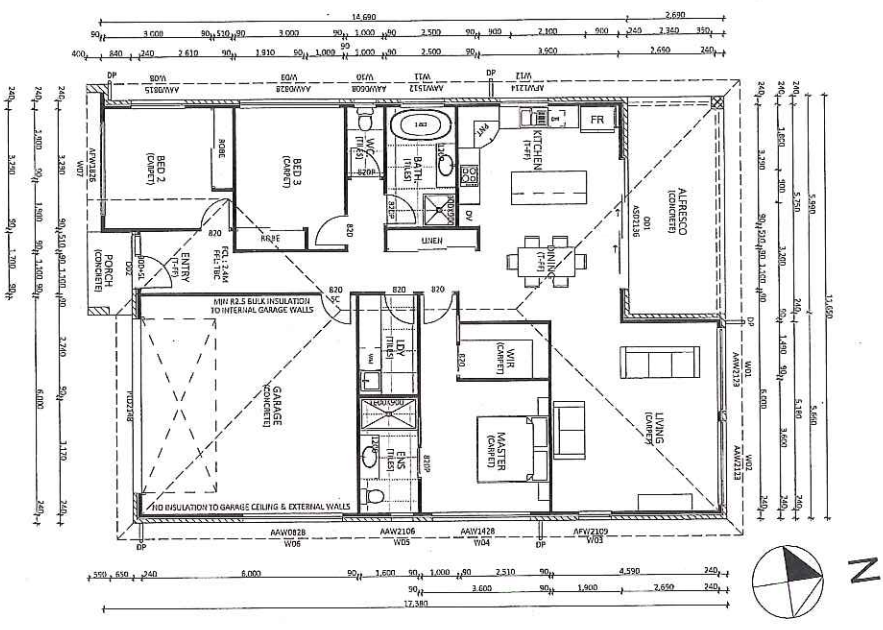
DRAWING: GROUND PLAN
UNIT TYPE: B

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R1	30/10/2020	FOR D.A.	M.L.	L.S.	CRCS49
					7/19
					SCALE @A2
					1:100



- LEGEND**
- B - BASIN
 - Ba - BATH
 - S - SINK (650)
 - 5 - LAUNDRY TUB (650)
 - T - SHOWER (650)
 - SH - SHOWER GEF
 - PC - ROOF WASTE
 - EV - VENT THROUGH TO ROOF
 - IO - INSPECTION OPENING
 - OR - O/FLOW RELIEF GULLY
 - RE - RODDING EYE
 - HW - HOT WATER CYLINDER
 - X - EXTERNAL TAP
 - P - DRAINAGE FIT (450 x 450mm)
 - DP - DOWNPIPE (90°)
 - WE - WEIR BASIS
 - SW - STORMWATER LINE (100mm PVC)
 - SW - STORMWATER LINE (150mm PVC)
 - SE - SEWER LINE (100mm PVC)
 - SE - SEWER LINE (150mm PVC)

INSTALL INSPECTION OPENINGS AT MAJOR BENDS FOR STORMWATER AND ALL LOW POINTS OF DOWNPIPES. PROVIDE SURFACE DRAIN TO BACK OF BULK EXCAVATION TO DRAIN LEVELLED PAD PRIOR TO COMMENCING FOOTING EXCAVATION.

SERVICES
THE HEATED WATER SYSTEM MUST BE DESIGNED AND INSTALLED WITH PART B2 OF THE NCC VOLUME THREE PLUMBING CODE OF AUSTRALIA.

THE THERMAL INSULATION FOR HEATED WATER PIPING MUST:
A) BE PROTECTED AGAINST THE EFFECTS OF WEATHER AND SUNLIGHT; AND
B) BE ABLE TO WITHSTAND THE TEMPERATURES WITHIN THE PIPING; AND
C) USE THERMAL INSULATION IN ACCORDANCE WITH AS/NZS 4855.1

15 HEATED WATER PIPING THAT IS NOT WITHIN A DESIGNATED SPACE MUST BE THERMALLY INSULATED AS FOLLOWS:

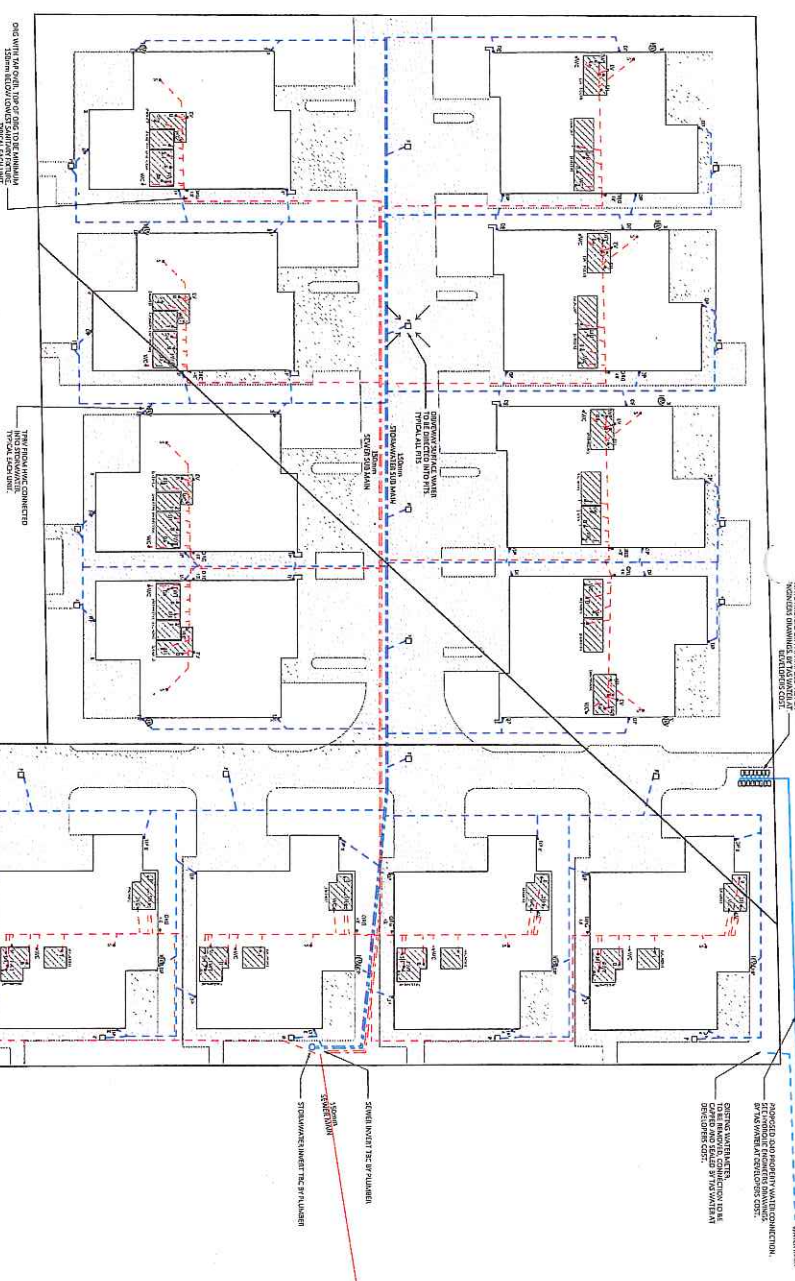
1. INTERNAL PIPING
2. ALL FLOW AND RETURN INTERNAL PIPING THAT IS NOT WITHIN AN INSULATED WALL SPACE
3. BETWEEN AN INTERNAL FLOOR BETWEEN STORES, OR
4. BETWEEN CEILING INSULATION AND A CEILING MUST HAVE A MINIMUM R-VALUE OF 0.2

2. PIPING LOCATED WITHIN A VENTILATED WALL SPACE, AN ENCLOSED BUILDING SUBFLOOR OR A ROOF SPACE
a) ALL FLOW AND RETURN PIPING
b) COLD WATER SUPPLY PIPING AND RELIEF VALVE PIPING WITHIN 500mm OF THE CONNECTION TO CENTRAL WATER HEATING SYSTEM,
MUST HAVE A MINIMUM R-VALUE OF 0.45

3. PIPING LOCATED OUTSIDE THE BUILDING OR IN AN UNENCLOSED BUILDING SUB FLOOR OR ROOF SPACE
a) ALL FLOW AND RETURN PIPING
b) COLD WATER SUPPLY PIPING AND RELIEF VALVE PIPING WITHIN 500mm OF THE CONNECTION TO CENTRAL WATER HEATING SYSTEM
MUST HAVE A MINIMUM R-VALUE OF 0.6

PIPING WITHIN AN INSULATED TIMBER FRAMED WALL, SUCH AS THAT PASSING THROUGH A WALL STUD, IS CONSIDERED TO COMPLY WITH THE ABOVE INSULATION REQUIREMENTS.

ALL DRAINAGE WORK SHOWN IS PROVISIONAL ONLY AND IS SUBJECT TO AMENDMENT TO COMPLY WITH LOCAL AUTHORITIES. ALL WORK IS TO COMPLY WITH AS 3500 AND LOCAL PLUMBING CODE AND SHOULD BE CARRIED OUT BY A LICENSED PLUMBER.



ALL WORKS ARE TO BE IN ACCORDANCE WITH THE WATER SUPPLY CODE OF AUSTRALIA WSA 03-2011-3.1, VERSION 3.1 MRWA EDITION V2.0 AND SEWERAGE CODE OF AUSTRALIA MELBOURNE RETAIL WATER AGENCIES INTERGRATED CODE WSA 02-2014-3.1, VERSION 3.1 MRWA EDITION 2 AND TWSWATER'S SUPPLEMENTS TO THESE CODES.

MICHELL LLOYD
ACC # 37179913
ABN 71 615 812 742
PH: 6544 7319
E: info@designlive.com.au

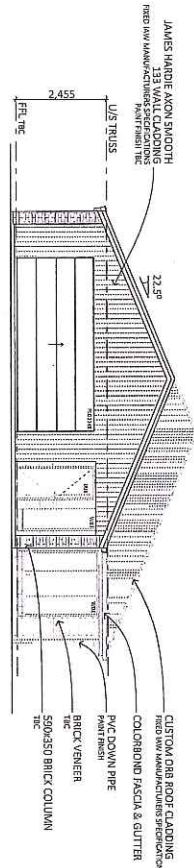
CUSTOMER:
HARIS DALEY
SITE ADDRESS:
49 GEORGE STREET,
PERTH, 2000.

DRAWING EXTERNAL SERVICES
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SIGNATURE: DATE:
SIGNATURE: DATE:

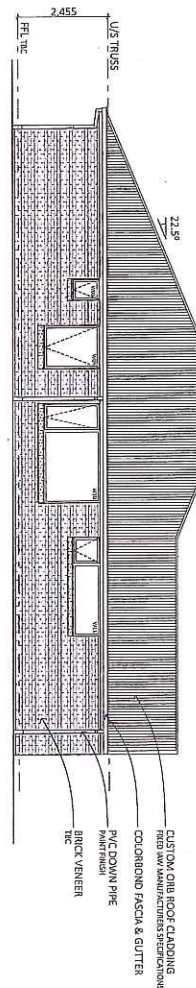
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CHECKED	M.L.L.	SCALE (@A2)	1:300		

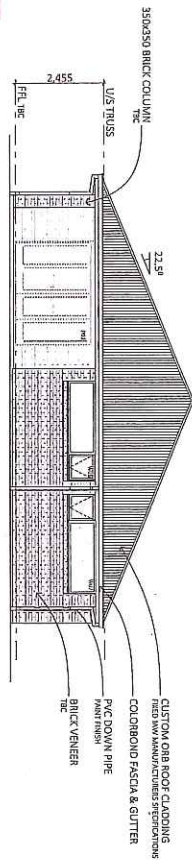
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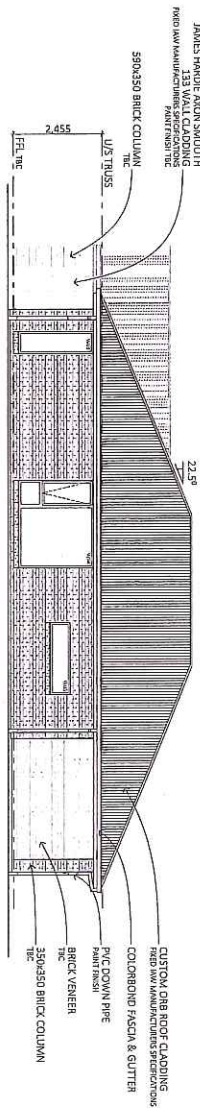
NORTHERN ELEVATION
UNITS 1, 2 & 3
MIRRORED FOR UNIT 4



EASTERN ELEVATION
UNITS 1, 2 & 3
MIRRORED FOR WEST ELEVATION OF UNIT 4



SOUTHERN ELEVATION
UNITS 1, 2 & 3
MIRRORED FOR UNIT 4



WESTERN ELEVATION
UNITS 1, 2 & 3
MIRRORED FOR EAST ELEVATION OF UNIT 4

DESIGN
BY
BLIVE

MICHELL LYDIA
ACC # 97179313
ARN, 71 615 812 747
PH, 6344 7319
E, info@designblive.com.au

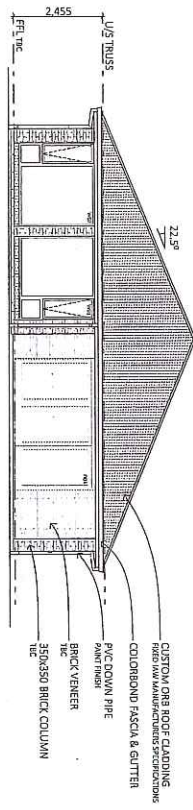
CLIENT/S:
HARRIS DALEY
SITE ADDRESS:
48 GEORGE STREET,
PERTH, 7000.

DRAWING
ELEVATIONS
UNIT TYPE A

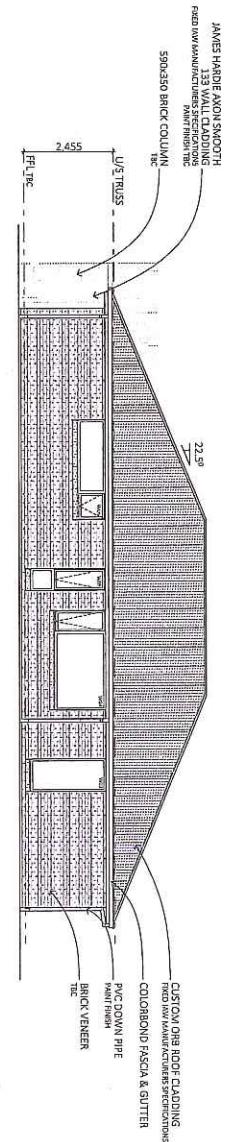
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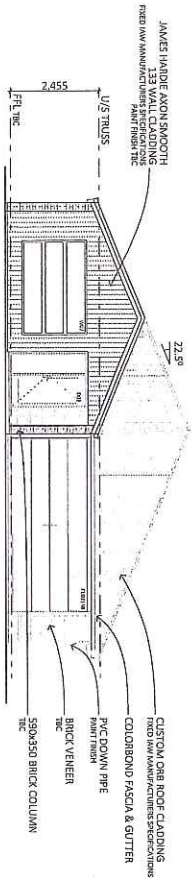
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			CHECKED	M.L.	SCALE (@A2)	1:100



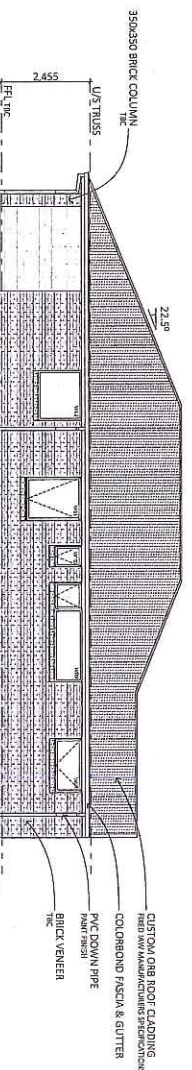
NORTHERN ELEVATION
UNITS 5, 6 & 7
MIRRORED FOR UNIT 8



EASTERN ELEVATION
UNITS 5, 6 & 7
MIRRORED FOR WEST ELEVATION OF UNIT 8



SOUTHERN ELEVATION
UNITS 5, 6 & 7
MIRRORED FOR UNIT 8



WESTERN ELEVATION
UNITS 5, 6 & 7
MIRRORED FOR EAST ELEVATION OF UNIT 8



 MITCHELL LLOYD
 ACC # 921799313
 ABN: 71 615 812 747
 PH: 6344 7319
 E: mlo@designholive.com.au
 PERTH, 2500

CLIENT/S:
 HARRIS DALEY

DRAWING
 ELEVATIONS
 UNIT TYPE B

SITE ADDRESS:
 49 GEORGE STREET
 PERTH, 2500

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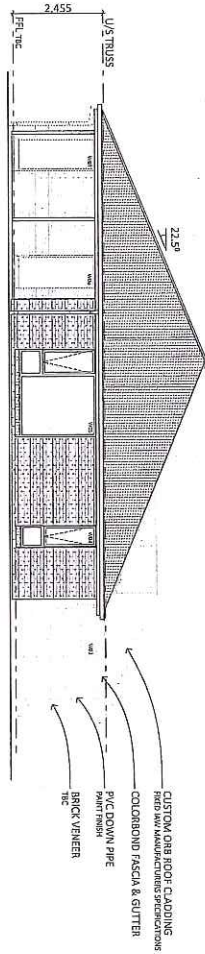
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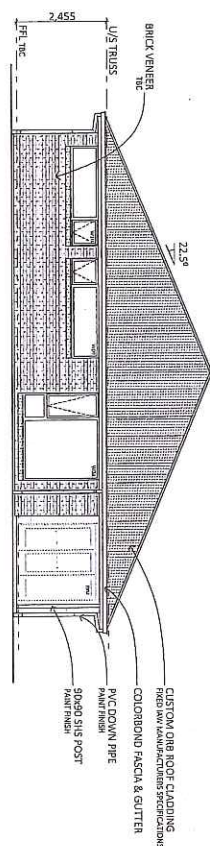
REV	DATE	DESCRIPTION	DESIGNER	M.L.	JOB NUMBER
R1	30/10/2020	FOR D.A.			GRG549

DRAWN: _____ L.S. DRAWING: 11/19
 CHECKED: _____ M.L. SCALE (@A2): 1:100

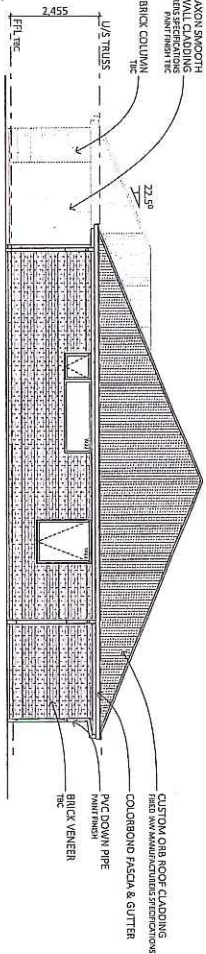
NORTHERN ELEVATION
UNITS 9 - 15



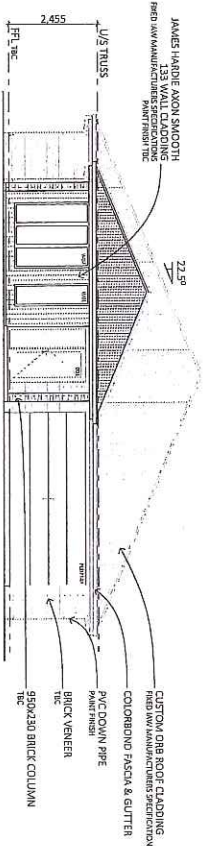
EASTERN ELEVATION
UNITS 9 - 15



SOUTHERN ELEVATION
UNITS 9 - 15



WESTERN ELEVATION
UNITS 9 - 15



0 1 2 3 4 5
DESIGN
DELIVERATIVE
 MITCHELL LYDIO
 ACC # 37179913
 A.B.N. 71 615 812 747
 PH. 6344 7319
 E. info@designolive.com.au

CLIENT/S:
 HABERS DALEY
SITE ADDRESS:
 49 DEERGOLD STREET,
 PERTH, 7500.

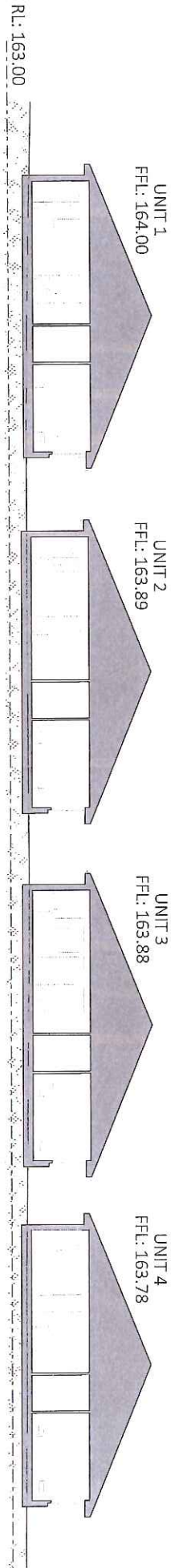
DRAWING
ELEVATIONS
 UNIT TYPE C

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CORRECT PER CONTRACT.
SIGNATURE:
DATE:

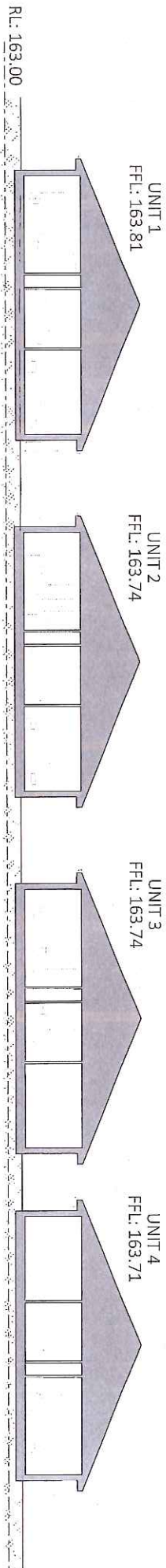
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R1	30/10/2020	FOR D.A.	L.S.			12/19
CHECKED	M.L.	SCALE (@A2)				1:100

EXHIBITED

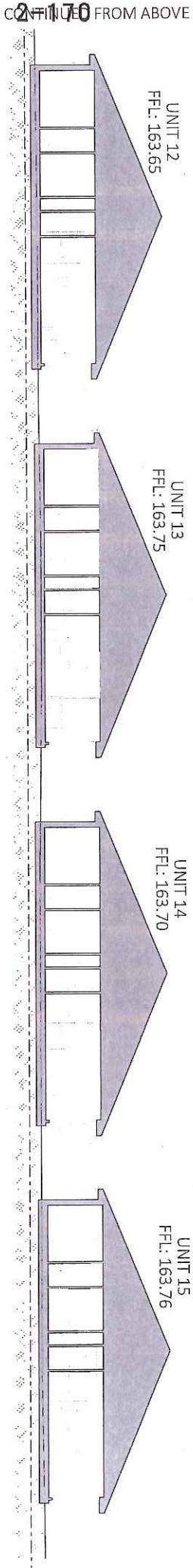
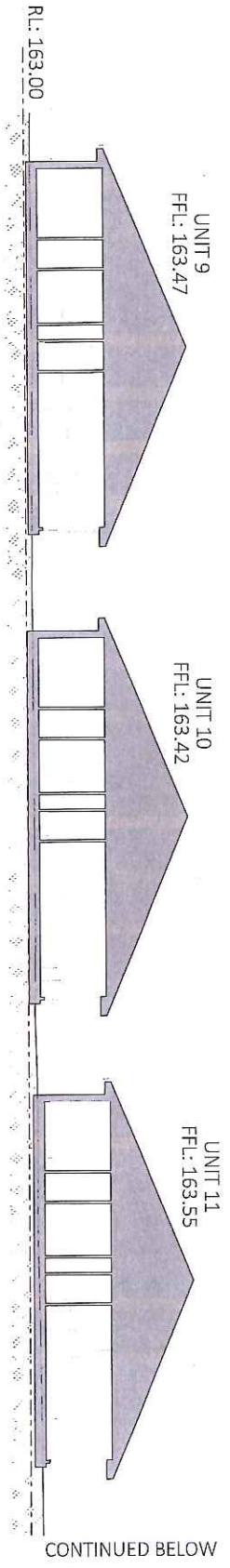


SECTION 13A-A



SECTION 13B-B

		MICHELL LOVIE ACC # 37179313 ASB 71 613 812 747 PH 644 231 7 E mlovedesign@midwest.com.au	
CLIENTS: HARRIS DAIER 49 GEORGE STREET, PERTH, 7000.		DRAWING SITE SECTIONS	
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CHECKED	M.L.	DRAWN	L.S.
JOB NUMBER		GRC349	
SCALE (A2)		1:120	



SECTION 14A-A
SCALE 1:120

SECTION 14B-B
SCALE 1:50



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		MITCHELL LLOYD ACC # 87129913 A88, 71 615 812 742 PH: 6844 7319 E: mitch@designdeliver.com.au	
CLIENT/S: HABIB DALEY		DRAWING SITE SECTIONS	
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R1	30/10/2020	ISSUE D.A.	
CHECKED	M.L.	SCALE (@A2)	VAR.
DRAWN	L.S.	DRAWING	14/19
DESIGNER	M.L.	JOB NUMBER	GRCS49

EXHIBITED



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HARRIS DALEY

DRAWING:
LANDSCAPE
PLAN

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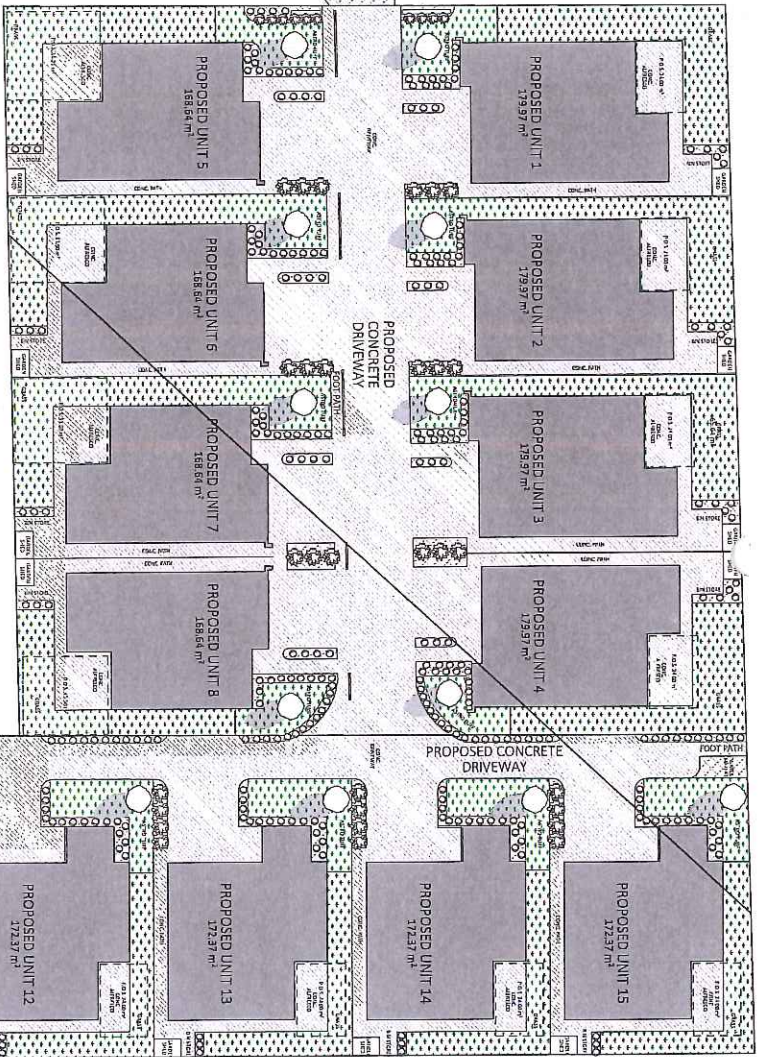
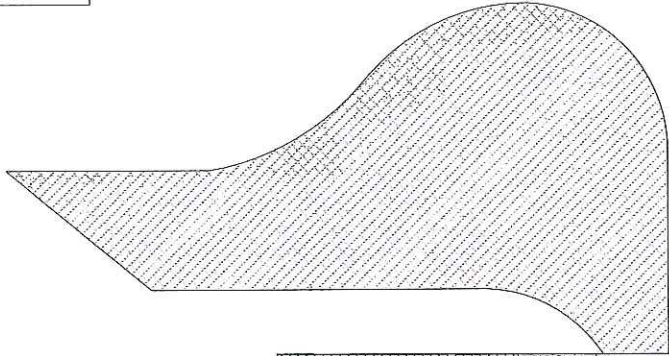
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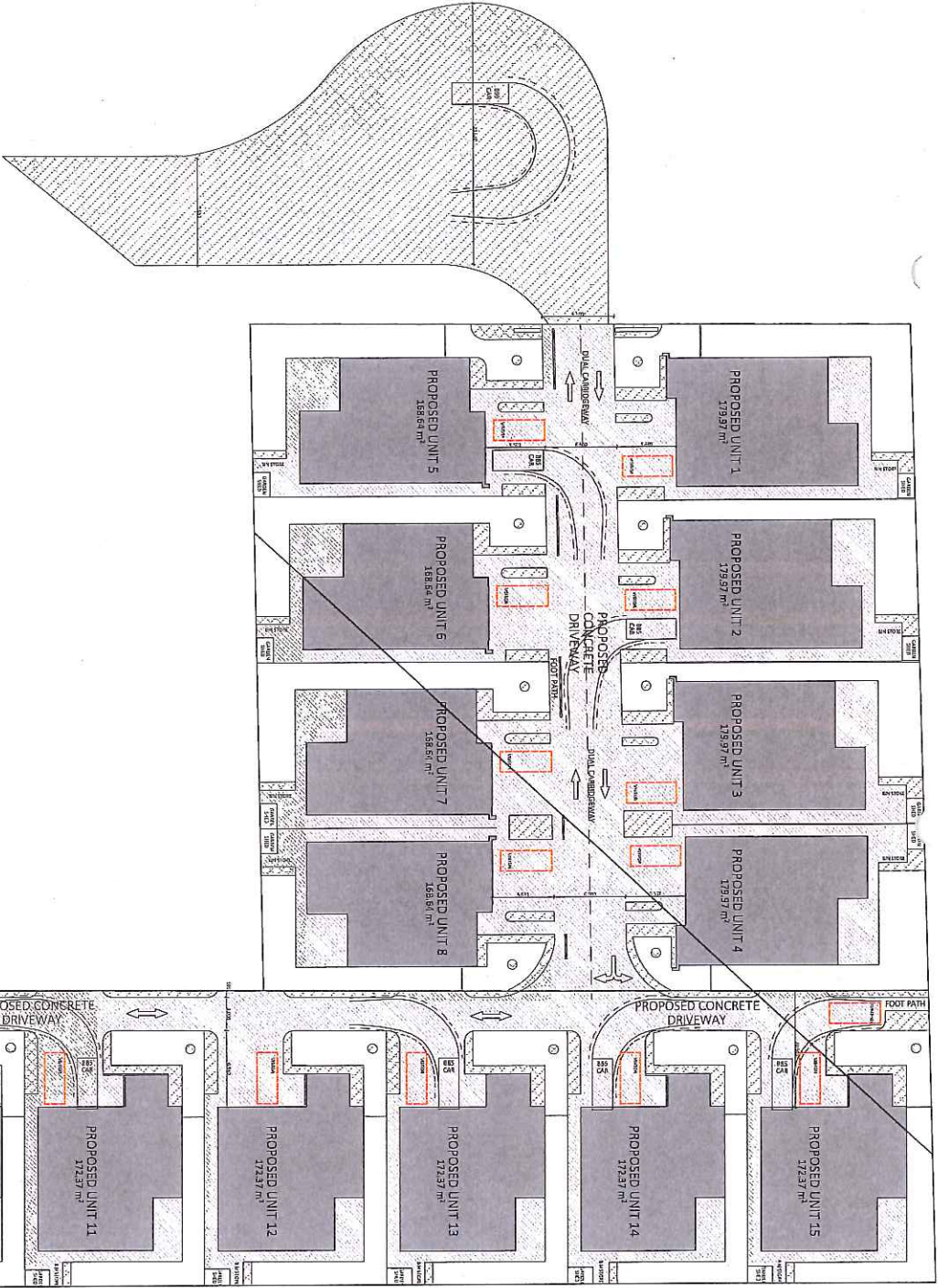
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R1	30/10/2020	FOR D.A.	L.S.	M.L.	CHGS49

KEY

- ASBESTOS STRIPS
- MAINTENANCE STRIP 150mm
- PITTSBOROUGH TREBE PATTERSON
- IKAN KANDE RISHI 7.5m
- ORNAMENTAL PEAR TREE
- MINIMUM PLANTING HEIGHT 7.5m



EXHIBITED



DESIGN
 Mitchell LLOYD
 A/C 1 9179219
 A/B 1 91812747
 F/H 6344 2319
 E: mitchell@designhive.com.au

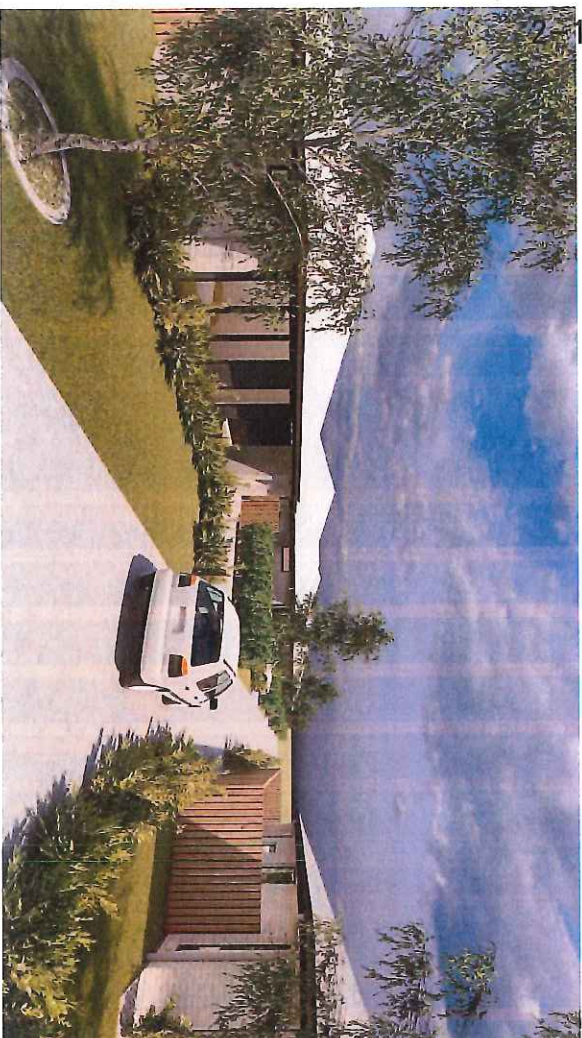
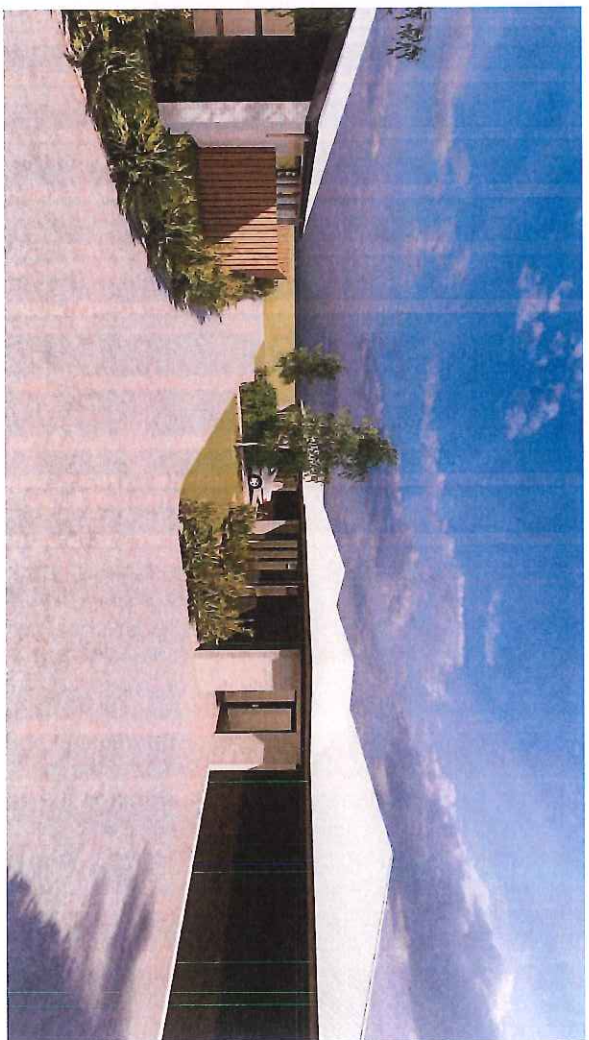
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SITE ADDRESS:
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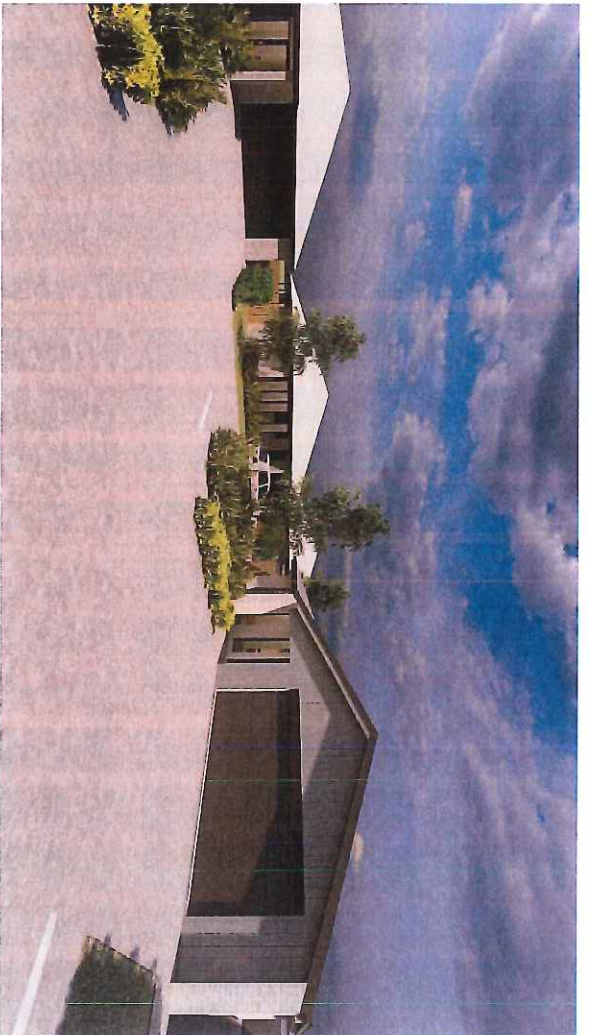
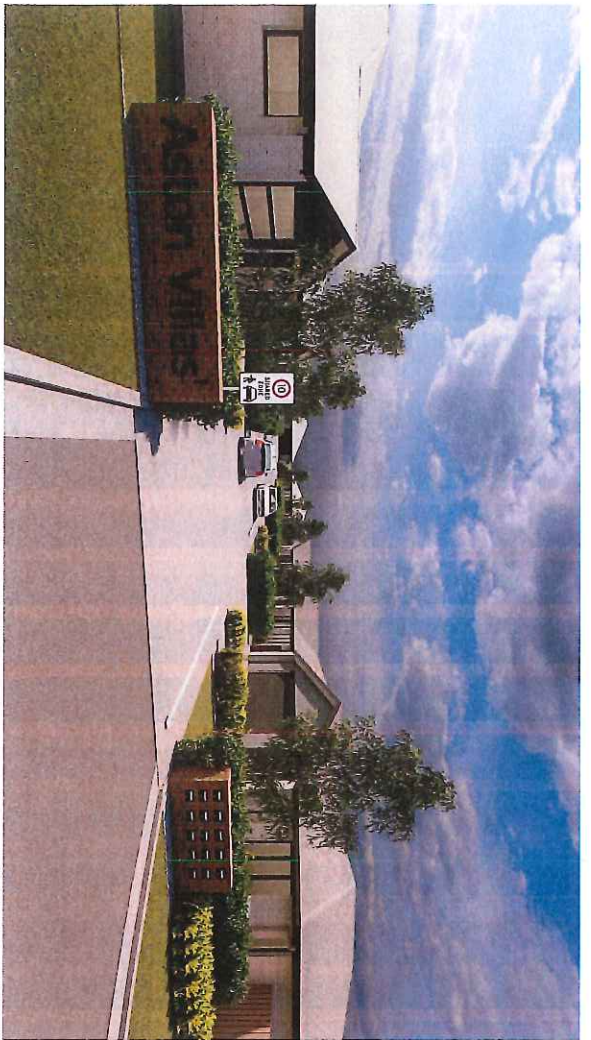
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173

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2-174

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2-175

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	SITE ADDRESS: 49 GEORGE STREET, PERTH, 7200.	SIGNATURE: SIGNATURE:	DATE: DATE:	CHECKED M.L.L.	DRAWN L.S.	DATE 19/19	CHECKED M.L.L.	JOB NUMBER GRCGS49

REFERRAL OF DEVELOPMENT APPLICATION PLN-20-0269 TO WORKS & INFRASTRUCTURE DEPARTMENT

Property/Subdivision No: 105700.307

Date: 21 May 2021

Applicant: Design to Live

Proposal: Multiple Dwellings (15), title consolidation & extension to Little Mulgrave St (Road & Railway Assets Code)

Location: 49 George Street, Perth

W&I referral PLN-20-0269, 49 George Street, Perth

Planning admin: W&I fees paid.

Jonathan - if you require further information, advise planning section as soon as possible – there are only 14 days from receipt of permitted applications and 21 days from receipt of discretionary applications to stop the clock.

Please inspect the property and advise regarding stormwater/drainage, access, traffic, and any other engineering concerns.

Is there is a house on one of the lots?	No
Is it connected to all Council services?	Yes
Are any changes / works required to the house lot?	N/A
Are the discharge points for stormwater, infrastructure that is maintained by Council? (This requires a check to ensure the downstream infrastructure is entirely owned, maintained, operated by Council and have been taken over as Council assets.)	Yes

Stormwater:

Does the physical location of stormwater services match the location shown on the plan? (Requires an on-site inspection)	Yes
Is the property connected to Council's stormwater services?	No
If so, where is the current connection/s?	Developer to install connection
Can all lots access stormwater services?	Yes
If so, are any works required?	No
Is stormwater detention required	Yes
Has a stormwater detention design been submitted	Yes
If so, is it designed for 20- year ARI with overland flow path to road or any other low risk Council approved place of discharge.	Yes
If no to above, has the design for 100 – year ARI been done.	N/A
If yes to any of the above, does it comply with Councils stormwater policy	Yes
Is the design approved by works & infrastructure	Yes
Please quote drawing numbers and any other relate documentation (email etc.)	#:LS135.001-C01
Additional Comments/information	No

Stormwater works required:

Works to be in accordance with Standard Drawing TSD-SW25 – a 100mm stormwater connection.

Is there kerb and gutter at the front of the property?	No
--	----

Are any kerb-and-gutter works required?	<i>Yes, as per approved plan</i>
---	----------------------------------

Road Access:

Does the property have access to a made road?	<i>Yes</i>
If so, is the existing access suitable?	<i>No</i>
Does the new lot/s have access to a made road?	<i>Yes</i>
If so, are any works required?	<i>Yes, as per approved plan</i>
Is off-street parking available/provided?	<i>Yes</i>

Road / access works required:

Works to be in accordance with Standard Drawing TSD R09 - concrete driveway crossover & apron from the edge of Little Mulgrave Street to the property boundary in accordance with the approved plans.

Is an application for vehicular crossing form required?	<i>Yes</i>
Is a footpath required?	<i>Yes</i>
Extra information required regarding driveway approach and departure angles	<i>No</i>
Are any road works required?	<i>Yes, as per approved plans</i>
Are street trees required?	<i>No</i>
Additional Comments:	<i>An Engineer's design is required.</i>

Engineer's comment:

STANDARD CONDITIONS FOR MULTIPLE DWELLINGSW.1 Stormwater

- a) A connection shall be provided to the Council stormwater system in accordance with the approved design plans and the Tasmanian Municipal Standard Drawings.
- b) Stormwater detention must be installed in accordance with the approved design plans and Council's policy for Stormwater Detention in New Developments
- c) Concentrated stormwater must not be discharged into neighbouring properties
- d) Landscaping and hardstand areas must not interfere with natural stormwater run-off from neighbouring properties.
- e) All driveways and hardstand areas must be designed to allow stormwater run-off to be adequately drained to the Council stormwater system.
- f) **Prior to the issue of a building permit, or the commencement of development authorised by this permit, the applicant must design and provide plans for underground stormwater drainage to collect stormwater from the driveways and roofed area of buildings. The system must connect through properly-jointed pipes to the stormwater main, inter-allotment drainage or other lawful point of discharge to the satisfaction of the Plumbing Inspector.**
- g) A plumbing permit is required prior to commencing any plumbing or civil works within the property.

W.2 Access

- a) A concrete driveway crossover and apron must be constructed in accordance with the approved design plan and Council standard Drawing TSD R03.
- b) **Access works must not commence** until an application for vehicular crossing has been approved by Council.

W.2 Roadworks

- c) Kerb and channel shall constructed in accordance with the approved plan and Tasmanian Municipal Standard Drawings
- d) All roadworks shall be hotmix sealed and road pavements and all other works must be constructed in accordance with the Tasmanian Municipal Standard Drawings.

W.2 Bonds

The subdivision shall be subject to a maintenance period of twelve months and a bond shall be held by Council until the completion of a satisfactory final inspection and the end of the maintenance period. The bond shall be calculated based on 5% of the total cost of works based on Council's standard road construction rates.

W.3 Municipal standards & approvals

Unless otherwise specified within a condition, all works must comply with the Municipal Standards including specifications and standard drawings. All works must be constructed to the satisfaction of Council. Where works are required to be designed prior to construction, such designs and specifications must be approved by Council prior to commencement of any *in situ* works.

W.4 Works in Council road reserve

- a) Works must not be undertaken within the public road reserve, including crossovers, driveways or kerb and guttering, without prior approval for the works by the Works Manager.
- b) Twenty-four (24) hours notice must be given to the Works & Infrastructure Department to inspect works within road reserve, and before placement of concrete or seal. Failure to do so may result in rejection of the vehicular access or other works and its reconstruction.

W.5 Pollutants

- a) The developer/property owner must ensure that pollutants such as mud, silt or chemicals are not released from the site.
- b) Prior to the commencement of development authorised by this permit the developer/property owner must install all necessary silt fences and cut-off drains to prevent soil, gravel and other debris from escaping the site. Material or debris must not be transported onto the road reserve (including the nature strip, footpath and road pavement). Any material that is deposited on the road reserve must be removed by the developer/property owner. Should Council be required to clean or carry out works on any of their infrastructure as a result of pollutants being released from the site the cost of these works may be charged to the developer/property owner.

W.7 Nature strips

Any new nature strips, or areas of nature strip that are disturbed during construction, must be topped with 100mm of good quality topsoil and sown with grass. Grass must be established and free of weeds prior to Council accepting the development.

Jonathan Galbraith (Engineering Officer)

Stormwater detention design reviewed by Cam Oakley 25/5/21

Date: 27/5/21

Submission to Planning Authority Notice

Council Planning Permit No.	PLN-20-0269	Council notice date	31/03/2021
TasWater details			
TasWater Reference No.	TWDA 2021/00505-NMC	Date of response	12/04/2021
TasWater Contact	Phil Papps	Phone No.	0474 931 272
Response issued to			
Council name	NORTHERN MIDLANDS COUNCIL		
Contact details	Planning@nmc.tas.gov.au		
Development details			
Address	49 GEORGE ST, PERTH	Property ID (PID)	3428985
Description of development	Multiple Dwellings x 15		
Schedule of drawings/documents			
Prepared by	Drawing/document No.	Revision No.	Date of Issue
Design to Live	Site Plan / 2	R2	17/02/2021
Conditions			
<p>Pursuant to the <i>Water and Sewerage Industry Act 2008 (TAS)</i> Section 56P(1) TasWater imposes the following conditions on the permit for this application:</p> <p>CONNECTIONS, METERING & BACKFLOW</p> <ol style="list-style-type: none"> A suitably sized water supply with a metered connection to the development must be designed and constructed to TasWater's satisfaction to service the proposed domestic and fire (if applicable) demands of the development and be in accordance with any other conditions in this permit. Advice: <i>TasWater will require a suitably sized domestic master meter be installed at the property boundary with DN20 water sub-meters located remotely at each dwelling unit.</i> A suitably sized sewerage system and connection(s) to the development must be designed and constructed to TasWater's satisfaction and be in accordance with any other conditions in this permit. Any removal/supply and installation of water meters and/or the removal of redundant and/or installation of new and modified property service connections including minor water main extensions must be carried out by TasWater at the developer's cost. Prior to use of the development, any water connection utilised for the development must have a backflow prevention device and water meter installed, to the satisfaction of TasWater. Advice: <i>In the event that a sewage lift pump is required to service the development a high hazard backflow prevention device will be required on the water service at the property boundary.</i> <p>ASSET CREATION & INFRASTRUCTURE WORKS (Minor water main extension)</p> <p>In the event that a water main extension is required to service the proposed development the following asset creation conditions will apply;</p> <ol style="list-style-type: none"> Plans submitted with the application for Certificate(s) for Certifiable Work (Building and/or Plumbing must, to the satisfaction of TasWater show, all existing, redundant and/or proposed property services and mains. When submitting the application for a Certificate for Certifiable Work (Building and/or Plumbing the 			

developer must include engineering design plans prepared by a suitably qualified person showing the hydraulic servicing requirements for water and sewerage to TasWater's satisfaction.

7. Prior to the issue of a Certificate of Water and sewerage Compliance (Building and/or Plumbing) all additions, extensions, alterations or upgrades to TasWater's water and sewerage infrastructure required to service the development are to be constructed at the expense of the developer by TasWater.

DEVELOPMENT ASSESSMENT FEES

8. The applicant or landowner as the case may be, must pay a development assessment fee of \$675.71 to TasWater, as approved by the Economic Regulator and the fee will be indexed, until the date paid to TasWater.

The payment is required within 30 days of the issue of an invoice by TasWater.

Advice

General

For information on TasWater development standards, please visit

<http://www.taswater.com.au/Development/Development-Standards>

For application forms please visit <http://www.taswater.com.au/Development/Forms>

Service Locations

Please note that the developer is responsible for arranging to locate the existing TasWater infrastructure and clearly showing it on the drawings. Existing TasWater infrastructure may be located by a surveyor and/or a private contractor engaged at the developers cost to locate the infrastructure.

The location of this infrastructure as shown on the GIS is indicative only.

- (a) A permit is required to work within TasWater's easements or in the vicinity of its infrastructure.
Further information can be obtained from TasWater
- (b) TasWater has listed a number of service providers who can provide asset detection and location services should you require it. Visit www.taswater.com.au/Development/Service-location for a list of companies
- (c) TasWater will locate residential water stop taps free of charge.

Declaration

The drawings/documents and conditions stated above constitute TasWater's Submission to Planning Authority Notice.

Authorised by



Jason Taylor

Development Assessment Manager

TasWater Contact Details

Phone	13 6992	Email	development@taswater.com.au
Mail	GPO Box 1393 Hobart TAS 7001	Web	www.taswater.com.au

Erin Miles

From: Jennifer Jarvis <Jennifer.Jarvis@tasrail.com.au>
Sent: Tuesday, 22 June 2021 8:13 PM
To: Erin Miles
Subject: Referral to TasRail - PLN 20-0269 - Unit development at 49 George Street

Follow Up Flag: Follow up
Flag Status: Flagged

Hello Erin,

Thanks for your time on the phone this afternoon. Following our discussion and taking into consideration the additional information you've provided TasRail amends it's referral response.

TasRail requests the following be considered as conditions to any permit approved by Council:

- TasRail to review/approve the final design of the road extension to Little Mulgrave Street with respect to the interface with/near the boundary of State Rail Network land. TasRail requests that the end of the extension include kerbing and safety bollards be installed to prevent vehicle access to the rail corridor.
- The developer be obligated to inform any prospective tenant or purchaser of the units to undertake their own due diligence of likely exposure to train horn noise levels, particularly given the proximity of the units to railway crossings. TasRail's concern is to ensure appropriate buyer beware precautions and to avoid potential for train horn noise complaints.
- Should there be a requirement for excavation works within 3 metres of the State Rail Network boundary, please note the obligation to give property@tasrail.com.au TasRail a minimum of 7 days notice as per the *Rail Infrastructure Act 2007*, noting a TasRail Permit to authorise the works within this area will be required.

Additionally, TasRail is of the view that the extension of Little Mulgrave Street combined with the 15 Unit development and growth in residential development will trigger a change in conditions/public safety risk associated with unauthorised public access to the operational rail corridor and particularly to mitigate the risks associated with unauthorised pedestrian access/short cutting over the operational rail corridor. TasRail therefore requests that Council agree to the installation of a 1.8m high chain mesh fence being erected along approximately 50 metres of the shared rail corridor boundary from the extended Little Mulgrave Street to the other side of George Street to effectively block off access to the rail corridor. This request is made in the context that TasRail (as the rail operator) and Council (as the road owner) are obligated under *Rail Safety National Law* (and the Safety Interface Agreement between Northern Midlands Council and TasRail) to identify and eliminate or control risk so far as is reasonably practical to do so. TasRail is willing to share the cost with Council to install the requested 50m fencing.

Please note that TasRail intends to contact the land owner (adjoining the rail corridor from the Main Road boundary) to discuss boundary fencing along this section of the rail corridor boundary to link up with the requested 50m of new 1.8m chain mesh fencing.

Should you have any questions or concerns with the above, please don't hesitate to contact me.

Jennifer Jarvis



2-182
Manager Group Property & Compliance | Property
Phone: 03 6335 2603 | Mobile: 0428 139 238
11 Techno Park Drive, Kings Meadows, Tasmania, 7249
Jennifer.Jarvis@tasrail.com.au

'Tasmania's trusted provider of safe and dependable rail logistics solutions'



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John & Tania Gaby
45 George Street
PERTH TAS 7300

2-183

Friday 4 June 2021

To the General Manager

REF: PLN-20-0269 (PROPOSED DEVELOPMENT AT 49 GEORGE ST, PERTH)

We are not opposed to developments, however, when reading the application cover letter, it relates to houses in George Street and Little Mulgrave street but is cramming 15 buildings into a small area, whereas if the development had the same density of buildings in relation to land it would be a far more agreeable outcome.

We oppose the setbacks. The development should adhere to the same setback as the most recent houses that have been constructed on George Street which is a minimum setback of 4.5 metres. The application refers to 48 George Street, a house that was built in the 1800's and when the current owners renovated and extended the house in the 1980's, the owners had to adhere to a setback that was within the planning scheme of that day. Google images will show this setback and the Northern Midlands Council would have records of this. The same situation would also be the same for the houses on Little Mulgrave St.

Since purchasing our property, we have always had water runoff issues from the block known as 47 George Street. After a rain event, most times water would pass through our block towards the house next door 43 George Street.

We approached the Council as they were the landowner in a way to share the cost of the building of a fence and the Northern Midlands Council declined this request and it was at a substantial financial cost to construct the concrete footings and purchase and install the colourbond steel fence that we currently have. The request was for the share of a wooden paling fence and we would contribute the labour and installation. The water runoff issues were substantially reduced with the addition of a hand dug drain and the installation of a 90mm stormwater pipe to the curb at the front of the block that is George Street.

Also, in the past council records will show that we have also paid a private contractor to mow and tidy up 47 George Street and issues raised with the dumping of rubbish by persons on the title known as 49 George Street. We have concerns over privacy with the number of units proposed on our Western boundary.

We have concerns that the water pressure will be reduced with the number of proposed units and also the stability of the land with heavy construction equipment and supplies being delivered. The end of George Street (the grassy area) has been cleared and a number of times and maintained by the ratepayers on George Street. We have not read anything within the application relating to the use of George Street and the impact this may have.

We also have a greater concern relating to sewage line that collects from the houses in George Street and the properties in Monica Place. We are reliably informed that a previous owner and also someone who intended to purchase the block known as 47 George Street that both people had verbal instructions from the Northern Midlands Council that only a maximum of three houses would be able to be constructed on this area due to that the current pipe infrastructure installed.

So, with this we are seeking assurances in writing prior to any construction that the current sewage system that has serviced our properties for 20 years will not be impacted in any way, by the construction and maintenance of sewage for fifteen dwellings.

Regards

John and Tania Gaby

DP & MJ Coombe
41 George Street
Perth TAS 7300
Email: c . .

4 June 2021

The General Manager Northern Midlands council (NMC)

Dear General Manager

Please accept this written representation in regards to Development Application PLN-20-0269, 49 George Street Perth.

We would like to lodge an objection in regards to the proposed variations of setbacks on Page 7 - Design to Live Pty Ltd (for the owner/developer, (10.4.2) Setback and building envelope for all dwellings) on both Little Mulgrave and George streets respectively.

Little Mulgrave Street (proposed primary frontage for the development)

- We believe it is not a valid argument/reason to use the existence of several dwellings (1, 3-Heritage listed cottages & 9), which were all built pre-planning schemes, to justify a reduced setback of proposed dwellings 1 and 5. Other adjacent owners have had to comply in more recent times with the minimum acceptable distance and therefore so should the developer.

George Street (Northern frontage of the development)

- We believe it is not a valid argument/reason to use the existence of 48 George St (across the road, built as a Railway cottage in the late 1800's) which was built pre-planning schemes, to justify a reduced setback of proposed dwelling 15. It is my belief that the current owners of 48 George St added an addition in the 1980's and had to comply with the planning scheme of the day. This saw the addition placed some 4.5m approximately back from their front boundary, so therefore it should be the same for the developer.
- All existing properties built on the Southern side of Western George St have also over time had to comply with Council setback regulations so therefore it should be the same for the developer
- **It is worth noting** that the existing dwellings in western George Street comply with frontal setbacks whilst the proposed development's dwellings would be side setbacks. One could argue that the side setback for the development (3.0m) should be further increased to match the existing 4.5m requirement in keeping with the properties on the Southern side of Western George St

We oppose both of these proposed variations to setbacks and believe they are out of character to both existing streetscapes.

We would like to express our concern that;

No impact assessment was commissioned/done on the Western end of George Street in regards to the impacts on the grassed end of the Street adjacent to 47 & 49 George St and the road surface itself pre, during and post the proposed development. It is our understanding that the property that fronts the old Midland Highway (parallel and next to the railway corridor) and backs onto Little Mulgrave St (adjacent to the development) is privately owned and no permission has been sought for vehicular or tradesperson access.

Further to that the Crown is yet to give permission (to extend and build a turning bay) in Little Mulgrave St which could potentially leave the western end of George St bearing the brunt of tradespersons vehicles, concrete mixers/pump and heavy vehicle deliveries etc for the development. Not also failing mention the time of the day/days that these above could occur and the noise impacts as well.

What assurances will given to the existing residents of George Street in relation to the impacts above in relation to the proposed development?

We would also like to further express our concerns;

- In relation to the density/numbers of dwellings proposed. To be fair to say at between 169-180m square (3 bedroom, 2 bathroom and 2 car garage) each dwelling could be more accurately called a house than a Unit. If the land available was carved up into 750m square blocks that would see approximately 8-9 homes in the area. In our opinion the proposed 15 "unit" development is out of character and density in comparison to the surrounding area/properties. We have serious concerns in the narrowness of access for proposed dwellings 15 through to 9 for the potential owners little lone for Emergency (Fire/Ambulance) vehicles etc. One vehicle potentially blocking the way, adjacent to dwelling 13, would impact dwellings 12-9.
- We have been informed that previous owners and intended purchasers of 47 George street had been given verbal advice by the NMC that the sewerage infrastructure could not handle any more than 3-4 dwellings little lone the proposed 15 dwelling development. What assurances are the NMC/ Taswater and the developer going to give the residents of George St and Monica Place that there will be no impacts during the construction, building and into the future of the proposed development?
- We are concerned about the impact of the development on our existing water pressure and quality of water. What assurances are the NMC/ Taswater going to give if this development (at its current size/capacity) is allowed to progress?

- No indication has been given in the developers plans in relation to fencing the George St boundary of the development. As the boundary will be the back fence of most of the proposed units we believe that the boundary should be adequately fenced to a minimum height of 1.8m for the privacy and screening of all concerned. This area is used by many residents of Perth as a thoroughfare on their way to Perth recreation ground and Perth Primary school. What assurances are NMC and the developer going to give the residents of George St and Perth in general in regards to this?
- Has the Tasmanian Fire Service been consulted with the proposed development? The location of the fire plug has not been noted in any plan and its placement (especially in relation to the development address 47=49 George St) could lead to delay and confusion in managing a fire in any of the units.
- Is it the intention of the NMC and or developer to retitle the development to a more accurate address number in Little Mulgrave St to alleviate any future confusion?

In summing up we would like to state that we are not opposed to developments.

We feel that better communication and discussion from proposed developers with surrounding residents would lead to more acceptable outcomes especially in relation to developments such as this.

Our major concern is the density of the proposed development and the impacts of this on surrounding residents/ratepayers.

The impacts are significant (sewerage/stormwater/water/waste/noise/vehicular traffic etc) not also forgetting the impacts prior and through construction in what may be a drawn out construction phase/stages.

We appreciate the opportunity to be able to lodge this representation and hope that it will be viewed with careful consideration.

Regards

Dale & Melissa Coombe

To Whom It May Concern:

I write to you with regard to the Development Application PLN-20-0269 which proposes multiple dwellings at 49 George St, Perth.

The proposed density of this development ultimately necessitates reduced setbacks to both frontages. In the case of Little Mulgrave St, there is a clear divergence from the front setbacks of the closest properties – numbers 11 and 13.

Compliance issues with clauses 10.4.3 and 10.4.4 of the Northern Midlands Interim Planning Scheme may also arise as a result of the proposal's density.

Clause 10.4.3 A2 specifically requires a private open space of 24m² that has a minimum horizontal dimension of 4m. However, such spaces do not appear to be indicated on the drawings and it is likely the proposal will be reliant on the use of shadow diagrams to demonstrate compliance with relevant clauses.

The requirements for shadow diagrams are detailed in Clause 8.1.3 (c) (vii) of the Scheme and note the need to show proposed buildings and adjacent structures.

The definition of 'building' under the Scheme refers to the Land Use and Planning Approvals Act 1993 which defines a 'building' as including fences, walls, outbuildings, service installations and other appurtenances of a building.

The provided shadow diagrams largely satisfy these requirements. However, it is unclear whether it is intended for any fences or gates dividing the private areas of units 1-8 from the more public driveway side.

Additional information is also evident on the shadow diagrams, but this is illegible due to the poor quality reproduction methodology employed for the purpose of online advertising. The relevance of this information is likely dependent on the accuracy of the shadow diagrams.

Sun altitude and azimuth (angle relative to north) can be obtained for specific locations online via Geoscience Australia which provides the following information for Perth (TAS) on the 21st of June, 2021:

10 am – altitude 18° 03' 52"; azimuth 31° 57' 46"
 Midday – altitude 24° 54' 28"; azimuth 03° 19' 05"
 2 pm – altitude 20° 26' 23"; azimuth 333° 53' 34"

The eastern boundary of the property is given on the survey drawings as being 190° 12' 40" – about 10° east of north.

Orienting the drawings such that the eastern boundary aligns with the survey angle should subsequently result in the shadows aligning closely with the azimuth obtained from Geoscience Australia (allowing for minor discrepancies).

However, an error of around 10° in all shadow diagrams results, suggesting a discrepancy with the sun and/or site orientation relative to north (Figure 1).

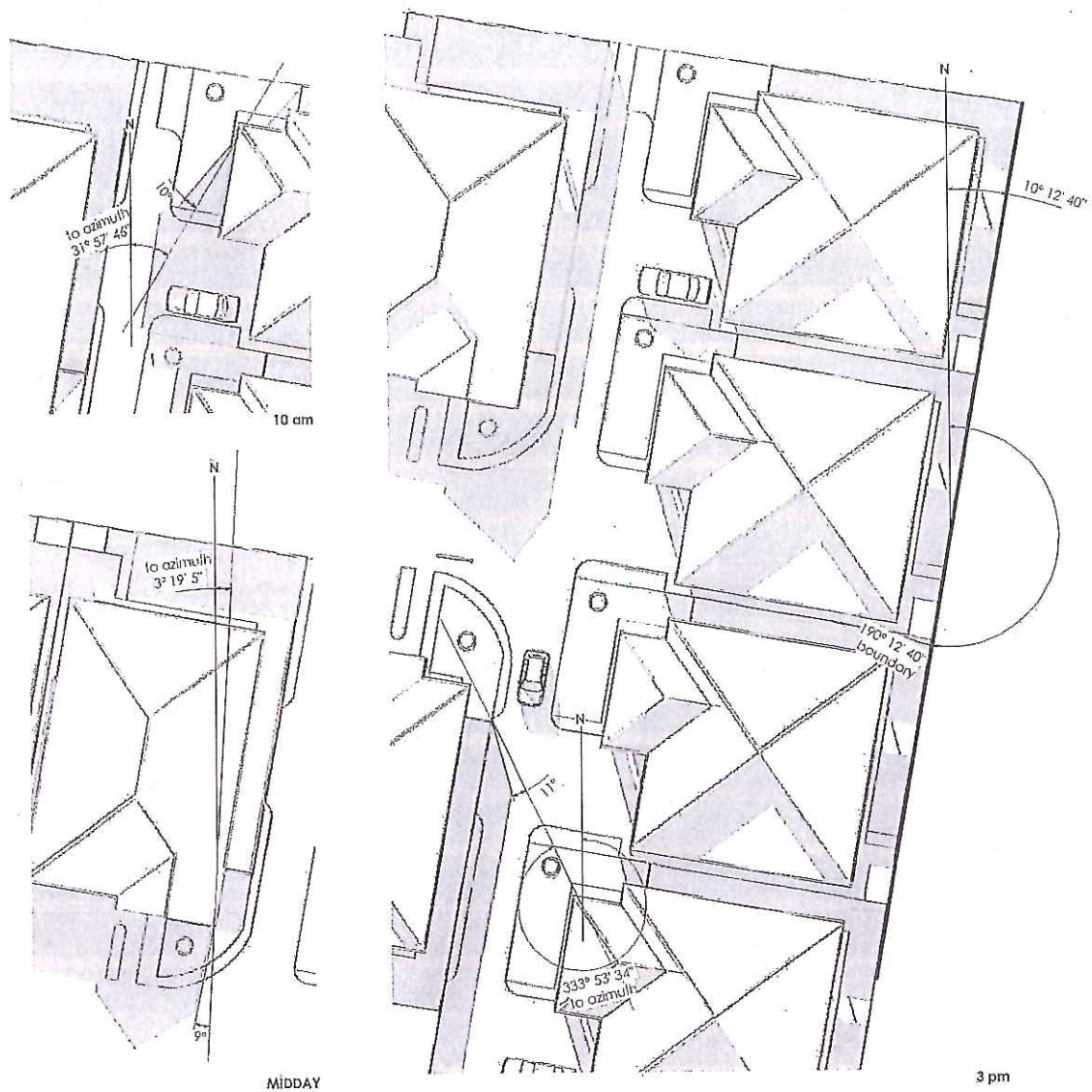


Figure 1 – Comparison of Geoscience Australia azimuth with DA diagrams

On this basis, the private open spaces required in accordance with 10.4.3 A2 for each unit should be indicated and the accuracy of the shadow diagrams verified to ensure the robustness of any assessment against relevant clauses of the Scheme.

Kind regards,

Mark Rhodes

The General Manager
Northern Midlands Council
LONGFORD 7301

9 Little Mulgrave Street
PERTH 7300

3 June 2021

NORTHERN MIDLANDS COUNCIL					
File No.					
Property					
Attachments					
REC'D JUN 2021					
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GM			PLN		
P&DM			BLD		
CSM			MYR		
WM			EA		
HR					
HLT					

Dear Sir or Madam,

Regarding Reference No: PLN-20-0269, currently titled as 49 George Street, Perth, though in reality to have its entrance on Little Mulgrave Street, Perth.

I am not in opposition to this subdivision per se, as I feel it could go ahead successfully with some amendments, which I feel certain other residents in this area will have raised already with the NMC. My concerns relate to Little Mulgrave Street and issues which will impact on residents, locals and tourists.

I take issue with comments in the Development Application that Little Mulgrave Street sustains moderate use as a roadway. How long did the so-called traffic expert spend in this short cul-de-sac?

I live in the middle of Little Mulgrave Street, directly across from the Train Park, and have had over twenty years to observe the strong growth in use of this roadway.

This road is used every day by many more people than those who actually live here, and all because of the park. The possibility of 30-45 extra cars using Little Mulgrave Street every day, will impact on EVERYBODY who uses the park, as well as those who live here, and the many Perthites (both school children and walkers) who use the railway line and then this street as a short-cut while walking.

Not all of these people are driving cars, and very often they are forced to walk in the road because of (a) cars parked along both sides of the street, (b) no footpaths in Little Mulgrave Street, (c) dogs and small children deterring them from walking in or beside the park.

I understand that the creators of new subdivisions must provide a footpath outside of any new houses. Can I ask that this new footpath be extended by the Council along that side of Little Mulgrave Street. This will (a) provide somewhere dry for all the old and new residents to walk, rather than in the middle of the road, and (b) make the area in front of our houses safe to walk on (the ground is currently churned up in winter by the postie bike, and under what looks like smooth grass is pitted earth -)

am still suffering from a strained deltoid ligament from stumbling in one of these ruts last summer).

Another additional usage of Little Mulgrave Street which will impact on residents and park visitors could last at least two years. That is the clogging of the street by tradesmen's utes during the building process. Last year, while 5 Little Mulgrave Street was being constructed, we had as many as 10 tradesmen's utes on both sides of the road, and some even parked on our grass verge, causing more damage to the grass as well as to curbing. I trust the Council will raise this issue with the developer.

I include a list of some of the people I see regularly using the park. Note that this is not specifically weekend usage, but happens every day. The numbers are obviously higher during the 7 or 8 months of better weather (mid-spring through to mid-autumn).

Locals who use the Park, along with Tourists and other People Passing Through:

- Young parents and grandparents minding children
- Walkers
- People who have bought takeaways in the area and need a place to sit and eat them (both locals and out-of-towners)
- Divorced couples swapping over their children
- Food vans and their customers
- At least one mothers' group meets here
- School children on their way home
- Birthday and Christmas parties
- Tourists in cars, campervans, and on bicycles
- The many tradesmen, Coles grocery delivery trucks, etc. who park in Little Mulgrave Street to take their breaks
- People passing through who give their dogs a break when they have been in the car long enough (we even had two horse floats stop and the horses saddled up for some exercise along the street)
- Bus groups of seniors on outings
- People exchanging goods
- School outings from Launceston schools who stop to use the facilities.

Thank you for your time.

Yours sincerely,



Jai Paterson (Dr)

3/6/2021

To:

General Manager

Northern Midlands Council

PO Box 156 Longford Tas 7301

NORTHERN MIDLANDS COUNCIL					
File No.					
Property					
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WM			EA		
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HLT					

Re Development 49 George Street Perth Tasmania. PLN-20-0269

I have lived at Little Mulgrave St Perth for 20+ years. I have seen the activities around the proposed works location in that time. I live opposite red train park.

The street must be one of most popular stop off points in NMC area for travellers and locals especially those with children.

Several issues about this development need consideration.

I note the site location is George Street. Yet traffic reports relate mainly to LM Street. Which entry point is the access site for the multitude of vehicles and equipment during works?

What is time frame required for build to be finished to completion? As site borders neighbours huge amounts of noise dust building runoff and allied annoyance to locals will be ongoing for multiple months maybe years.

The trail walk/ link between George and LM Street is used all year round. By walkers bike riders and schoolchildren to and from park and Main Road. In past it has been mainly neglected and sees waterlogging and long grass litter blackberries and vehicle traffic and parking. Grass is reluctantly cut when at bushfire risk levels.

Design of any development should have the trail path left in. With refurbishment and wet weather ground surface installed. A loss of amenity for local users would occur if site plans blocked access points at either end at George or LM Street. Plus restricted use by homeowners and visitors of site in future would occur.

The traffic and road survey done relating to LM Street makes comments that current road surface is in good condition. As of 3/6/21 on my own inspection I would suggest otherwise.

Road surfaces are cracked slumped and irregular in places. Recent damage from a new home building works over many months has seen delamination of concrete curbs and guttering has fractured. Poor patch ups see cracks all along concrete curbing. As no concrete drives exist in the most of Perth township, driveway edges are broken. Basically the street needs kerbing removed and replaced as per the recent upgrades at Mary Street Perth. Road surface at junction of Main Road LM Street and Elizabeth Street is always damaged. Large semi trailers enter LM Street and cross turning circle onto private land and road is damaged there also. Ongoing access if development occurs for heavy equipment trucks etc will see LM Street worn out even more. The residents already see park users doing 3 point turns into driveways at LM Street constantly. Vs the driver doing a turn around a street end.

Large buses etc cannot turn at end of street. Often they reverse into private driveways to do this. The unconcreted drives see damage from above activity constantly.

If trades workers large trucks etc use LM Street as thoroughfare for development works the damage can only get worse. A condition of any approval must cover Developer's need to repair road damages. Not ratepayers.

No footpaths exist on LM Street. Pedestrians, especially in winter use road itself to progress from George Street end to Main Road via LM Street. With many houses proposed on site, a wet weather footpath on LM Street will keep pedestrians safer. Many of which are older walkers and schoolchildren. With a potential of 45 vehicles exiting the site each day this will be even more important. The post Office motorbike delivery wears a wet muddy track to deliver along street median strip now. The issue of private land near turning circle of LM Street and use / crossing by heavy vehicles has been an ongoing issue. A gravel lane along railway sees many vehicles moving through it. Any building works trucks etc at proposed development should not access it. Much is private land.

Concrete trucks have emptied and washed slurry onto the area in past. Site has been used as a hazardous unfenced rubbish dump on the private land. Vehicle access and leaving via entrance near railway gates I believe is illegal as it is not a public access road. Likewise vehicles crossing through LM turning circle. Requests for strong barriers to restrict this have been lodged with NMC in past. Flimsy white bend posts last a day each time they are replaced.

A delineation of non public access to area via barriers / Armco fencing or poles on cradles is needed to stop issue. Especially if development progresses. These unsuitable illegal vehicle movements will be adjacent to any new houses near railway line at all hours.

If LM Street is only access point for new housing, current residents will see up to 45 house vehicles at site. Plus delivery vehicles garbage trucks parcel delivery trucks plus emergency vehicles added to street movements. Plus allow for park traffic, bus entry to park, Food van parking and customers.

Comments about Development site.

A concern of current nearby residents is that already houses in adjacent block have issues with water runoff. Land is traditionally so flat it pools in backyards. Comments from landowners are to the effect that NMC staff in past advised that site was unsuited to no more than a few homes due to issues with services etc.

More concrete as modern houses are being built sees older grassed blocks losing abilities to drain. The site will have masses of concrete vs current grassed land. I believe current NMC rules need any new works to be able to control runoff efficiently and totally. Runoff to stormwater will load current pipes even more. It is not uncommon to see road drains in LM Street full to gutter tops in strong downpours. George Street is unkerbed at site location now.

Will the service mains in fact need upgrading on the public access roads to take the increase in flows from site? Likewise water mains? Can the current water supply service adequately a fire brigade callout to a fire at proposed site?

Narrowness of roads in site will see vehicles doing 3 point turns to get back out of site. Homes at end of internal lane eg no 7 LM Street will see car headlights directly into their house. Extra high fencing at points such as there would reduce annoyance value.

Lighting at site to not detract from current boundary houses will be needed. Currently it is a dark paddock. As site borders many homes higher fencing also would reduce their intrusion of privacy.

The issue of distance of boundary frontages being shrunk down to 3m ..that of older homes in LM Street appears to be at odds with current standards. The older homes as per mine were from different regulation times. The homes which will be nearest to rail line should not be allowed closer than current standard to make life in them more amenable.

Similarly the idea to place one house title on railway corridor land should not be allowed. For safety reasons and to comply with current railway easement position.

How the site is linked with public land and roads is an issue. Will George Street be improved with bitumened turning circle and gutters? Will service vehicles or residents have any access entry at George Street end?

The access road at proposed extension to LM Street is over a culvert dug to reduce water along area. Does developer pay costs of modifying this public land? What is in place in any changes to allow for the water that accumulates already?

I have covered points that relate to my house and how developments will change the nature of my streetscape for myself and LM Street visitors. Plus changes to public access and pedestrian safety.

Little Mulgrave Street already sees traffic unsuited to its short length and turning circle. Drivers of heavy vehicle access street regularly..Pedestrians compete with these trucks and large buses.

Overall I consider the site unsuited to the number of houses planned. Lessening of house numbers to a more realistic level is required. The low key park visitor lifestyle of the current no through road /LM Street will be lost forever. It is one of the reasons families visit site. Kids often walk on road. Balls are kicked or hit onto road. Kids have little road sense.

Park users ..often children will be competing with traffic moving in and out of site ..

The traffic report highlights.. that the level of use number of vehicles and layout maintains an acceptable level of safety for all road users including pedestrians and cyclists. This proposed development will increase risks.

Infrastructure eg footpaths/ barriers at turning circle etc.. are ways of addressing this. Both in LM and George Street.

Other landowners no doubt will add more specific comments related to their homes that adjoin the site directly.

One comment often heard is that few submissions are usually made to councils from interested parties about an issue.

This development mainly relates to only a few directly impacted homeowners. NMC won't get many replies as a result. Most of Perth township likely know nothing of proposal and it won't impact them, Unless they are park visitors. This does not diminish the value of inputs and concerns of those affected.

I request council consider all of the above points before allowing this development to progress.

Mr Nick Moore

9 Little Mulgrave Street

Perth Tasmania 7300

Mr. Des Jennings,
General Manager,
Northern Midlands Council,
P.O. Box 156,
Longford, Tas 7310

2-194

Via email council@nmc.tas.gov.au

Thank for the opportunity to provide a Development Application Representation Letter for the Planning Application listed below.

Development Application Number: PLN_20_026

Address of Development: 49 George Street, Perth

Details of Representer: Shane & Liz Farquhar

Address: 13 Little Mulgrave Street, Perth

Home Phone:

Mobile Phone Liz:

Mobile Phone Shane:

Email:

NORTHERN MIDLANDS COUNCIL					
File No.					
Property					
Attachments					
REC'D 4 JUN 2021					
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ISSUES
1. LOCATION
2. DELIVERY VEHICLE ACCESS
3. POSTAL DELIVERY
4. PEDESTRIAN ACCESS - GEORGE STREET
5. PEDESTRIAN / CYCLE ACCESS
6. TASRAIL CORRIDOR - BARRIERS
7. TASRAIL - FUTURE IMPACT
8. STORMWATER / RUNOFF
9. FIRE HYDRANTS
10. CONSTRUCTION
11. EARTHWORKS
12. MACHINERY
13. VEHICULAR TRAFFIC
14. EMERGENCY VEHICLES
15. LIGHTING / SOLAR / HEATING
16. HOUSEHOLD WASTE
17. SITE PLAN
18. LANDSCAPING
19. ANIMALS
20. HERITAGE
1. LOCATION
<p>What was the governing factor for determining that access to the development will be from Little Mulgrave Street and not George Street, even though it is described as 49 George Street in the planning application, was this to maximise the number of units that could be constructed on the site.</p> <p>This misnomer creates confusion regarding the physical address of the new development, for visitors, deliveries, waste management and emergency vehicles. Will there be a change regarding the nomenclature or will there be appropriate signage erected to solve this problem. Refer: https://www.northernmidlands.tas.gov.au/source-assets/images/Northern-Midlands-Interim-Planning-Scheme-2013-last-updated-18-March-2021-amendment-01-2020-and-permit-PLN-20-0001-002.pdf PLANNING APPLICATION PLN_20_0269 - E15 Signs Code</p>
2. DELIVERY VEHICLE ACCESS
<p>It is noted that Delivery vehicle access has not been addressed within the planning application. Refer: https://www.northernmidlands.tas.gov.au/source-assets/images/PLN-20-0269_public_exhibition_documents.pdf PLANNING APPLICATION PLN_20_0269 - 7.7 Delivery Vehicles.</p> <p>There is no mention of a concierge arrangement to facilitate deliveries, e.g. grocery click and deliver, Uber eats etc. Meal delivery services have doubled usage in only 18 months, with 19:1% of Australian residents using these services. Refer: http://www.roymorgan.com/findings/8270-food-delivery-services-september-2019-202002030451.</p> <p>According to Roy Morgan research conducted in the year to December 2018, over 5 million Australian grocery buyers (31% of grocery buyers) would consider buying groceries online in 2019, this was an increase from 800,000 for the previous year. Refer: http://www.roymorgan.com/findings/7911-australian-online-grocery-shopping-march-2019-201903220623</p> <p>Obviously the ramifications of COVID are evident from the current exponential leap in use of home delivery.</p> <p>Clarification is required regarding accessibility for deliveries. Commercial vehicle access is constrained within the development, particularly the driveway servicing Units 9 - 15. Commercial vehicle movements should not unreasonably impact on the amenity of occupants of adjoining and nearby dwellings, therefore it would not be suitable for vehicles to congest Little Mulgrave Street and as an alternative if delivery access is viable from George Street, congestion would again be a problem, especially considering there is no mention of the construction of a turning circle at the Western end of George Street.</p>
3. POSTAL DELIVERY

ISSUES

As a percentage of total Australian retail, online sales for 2020 accounted for 16.3% and Tasmania has incurred a 37.3% increase for March 2021. As social distancing, border closures and lockdowns became commonplace, Australians have increasingly turned to online purchases.

Australia Post - Online Shopping Report, Refer: <https://auspost.com.au/business/marketing-and-communications/access-data-and-insights/ecommerce-trends>

There are already residential concerns with regard to the damage incurred to the grass verge by Australia Post deliveries in Little Mulgrave Street, with the addition of 15 new households and increasing online purchases necessitating more frequent deliveries, this damage will only increase in conjunction with the additional pedestrian use by the new residents.

The planning application advises that no changes are proposed for consideration to the external pedestrian and bicycle movements area.

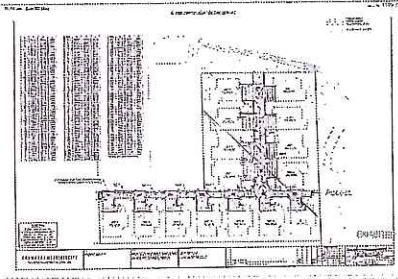
Refer: https://www.northernmidlands.tas.gov.au/source-assets/images/PLN-20-0269_public_exhibition_documents.pdf
PLANNING APPLICATION PLN_20_0269 - 7.4 Pedestrian and Bicycle Movements

Can you advise who will be responsible for the remedial work which will be required and to stop ongoing damage, if a footpath similar to the existing pedestrian network in Main Road and Elizabeth Street, will be constructed.

4. PEDESTRIAN ACCESS GEORGE STREET

Can clarification be provided regarding security, (lockable entry/exit) for the pedestrian access from George Street, as the corner of George Street and Little Mulgrave Street has traditionally been used as a shortcut to the Main Road and if open access is still available will no doubt continue to be used.

There is therefore a conundrum, will the developers have locked access in the George Street area to prevent trespass or will it be open to allow resident access, possibly waste bin movement, and deliveries to reduce the possible congestion in Little Mulgrave Street. Will there be access from George Street to Little Mulgrave Street to replace the shortcut or would this now be unavailable because of the encroachment of the development onto the Rail corridor.



5. PEDESTRIAN / CYCLE ACCESS

ISSUES

No changes are proposed in the planning application for consideration to the external pedestrian and bicycle movements area.
Refer: https://www.northernmidlands.tas.gov.au/source-assets/images/PLN-20-0269_public_exhibition_documents.pdf
PLANNING APPLICATION PLN_20_0269 - 7.4 Pedestrian and Bicycle Movements

The existing pedestrian infrastructure in the surrounding road network is not adequate and would need upgrading, with reference to the focus on accessibility through improved walking and cycling networks and integrated public transport included in the Regional Land Use Strategy. As a minimum Little Mulgrave Street should contain a footpath on both sides.

If the George Street access point to the development is locked to restrict illegal access will there be a shared user path (pedestrian path or cycle track) between the development and the rail corridor, to provide shortcut access.

AND will there be compliance with any policies from Regional Land Use Strategy (RLUS), Regional Infrastructure Network Policy
Refer: https://www.planningreform.tas.gov.au/data/assets/pdf_file/0006/418506/Northern_Tasmania_Regional_Land_Use_Strategy_-_27_June_2018.pdf Page 40

RIN- P6 Facilitate and encourage active modes of transport through land use planning.
RIN-P7 Facilitate an efficient and convenient public transport system through land use planning.

In particular attention should be paid to RIN-A20
Subdivision design is to provide interconnected road layouts, minimises the use of cul-de-sacs, and promotes an efficient and contiguous public transport service, including cyclist and pedestrian movement.

6. TASRAIL CORRIDOR - BARRIERS

If there is not a shared user path between the development and the rail corridor, will there be a physical barrier, which could include landscaping or a regulation height fence (minimum 1.8m) provided by the developer which would ensure privacy, safety and noise abatement for the development and existing homeowners, without compromising the line of sight from trains.

The issue of noise from Rail usage has been addresses by Pitt and Sherry, with the provision of acoustic glass required in Unit 1, which should solve the problems of usual rail noise, however it has not addressed, that subject to topography and weather train horn noise emissions can extend well beyond 30 metres and exceed the noise limits. It is standard practice for Tas Rail to use warnings, for Arthur Street level crossing, again at Little Mulgrave Street and the Main Road level crossing, this will effect all units located on 49 George Street,

The fence could also provide a secondary purpose as a permanent throw protection screen for the Rail corridor. Maintenance availability would be required for this area (requiring a setback from the rail corridor) and any repairs, cleaning and graffiti removal should be the responsibility of the development.

Refer: E4 Road and Railway Assets Code - ensure that use or development on or adjacent to a road or railway will not compromise the safety and efficiency of the road or rail network <https://www.northernmidlands.tas.gov.au/source-assets/images/Northern-Midlands-Interim-Planning-Scheme-2013-last-updated-18-March-2021-amendment-01-2020-and-permit-PLN-20-0001-002.pdf>

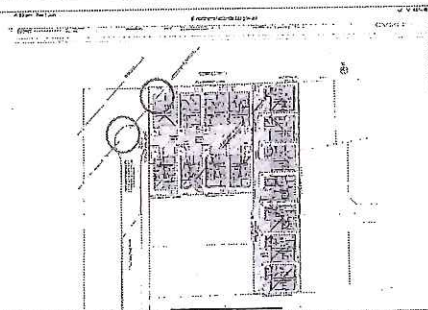
7. TASRAIL - FUTURE IMPACT

Is there a caveat to ensure the new development does not adversely impact on current and future transport operations and will not restrict future rail widening, alignment and upgrades. It is noted that the turning circle extension for Little Mulgrave Street, also Unit 1 and the boundaries for Little Mulgrave and George Street all encroach on the 20m TasRail corridor.

Will there be a continuation of the footpath from Little Mulgrave Street to George Street to replace the shortcut previously used or would this now be unavailable because of the incursion into the TasRail corridor.

Will a risk inspection report be provided assessing the impact of the new development on existing rail traffic, as there will be a change to the risk profile due to the increase in pedestrian activity in the area as a result of the development.

Refer: E4 Road and Railway Assets Code - maintain opportunities for future development of road and rail infrastructure
<https://www.northernmidlands.tas.gov.au/source-assets/images/Northern-Midlands-Interim-Planning-Scheme-2013-last-updated-18-March-2021-amendment-01-2020-and-permit-PLN-20-0001-002.pdf>

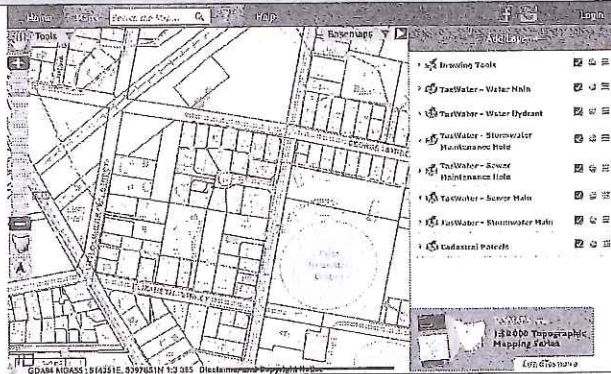


8. STORMWATER / RUNOFF

There are concerns that the stormwater management system may be inadequate to manage the runoff from the entire development (including pervious and impervious areas) during the construction phase and on completion. Will this be addressed by provision of Hydraulic calculations to confirm that the receiving drainage system has the capacity to accept the discharge from the proposed development, both grey and black water.

Refer: <https://maps.thelist.tas.gov.au/listmap/app/list/map>

ISSUES



10. CONSTRUCTION

Will a Demolition and Construction Management Plan be provided for adjacent homeowners to determine the impact and disruption during the construction. Preferably the plan should include proposals for the development objectives, performance and monitoring requirements for management of drainage, waste management (effluent), material stockpiles, equipment storage, fencing and hoardings, operating hours, noise and vibration controls (especially in consideration of those residents nearby with ill health) air and dust management, public safety, neighbour amenity, site security and traffic management.

11. EARTHWORKS

Excavation, earthworks and construction can cause subsidence and stress changes in the soil and rock especially if there is poor compressive strength, structural defects or groundwater seepage. It is expected that excavation works should be located, sited, designed and managed to minimise potential impacts such as slippage, slumping, creation of fissures or cracks, rock or earth falls, exacerbated ground movement, water inflows and structural failure. The concern is the impact or deterioration that the development earthworks will have on adjacent structures and how this will be avoided. Will a geotechnical assessment be completed and a Soil and Water Management Plan (SWMP) be provided to identify risks and mitigation techniques.

Refer: Soil and Water Management on Building Sites

<https://epa.tas.gov.au/epa/water/stormwater/soil-and-water-management-on-building-sites> - SWMP

Tasmanian Municipal Standard Specifications Part 2 Earthworks

https://www.lgat.tas.gov.au/data/assets/pdf_file/0029/700895/Tasmanian-Municipal-Standard-Specifications-March-2020.pdf

Tasmanian Subdivision Guidelines - 16. Erosion and Sediment Control during Construction

https://www.lgat.tas.gov.au/data/assets/pdf_file/0020/322616/Subdivision-Guidelines-21-10-13-with-coverpage.pdf.

12. MACHINERY

Cranes, concrete pumps and other heavy equipment capable of moving into or across the ground and airspace in the vicinity of the development may cause safety and other issues if their operation is not strictly monitored and will impact heavily on the residential and visitor vehicle use of Little Mulgrave Street. The volume of heavy machinery traffic, pre and post construction may also have a detrimental effect on road assets, public infrastructure and other assets.

It is recommended that by constructing a Stabilised Site Access that can be documented on the Demolition and Construction Management Plan, the developer will reduce actions which are likely to cause an environmental nuisance. Also by monitoring emissions including noise and traffic movement, smoke, odour, dust and illumination and strictly adhering to time restrictions this will assist in ensuring the well being of adjacent residents.

Refer: Soil and Water Management on Building Sites

<https://epa.tas.gov.au/epa/water/stormwater/soil-and-water-management-on-building-sites> - Stabilised Site Access

13. VEHICULAR TRAFFIC

Machinery will have an impact on residential vehicle access, but the additional traffic created by the development in what is a neighbourhood cul de sac will certainly compromise accessibility and safety for the numerous visitors to the Lions 'Train' Park, especially the children who utilise the park, for play and birthday parties.

Refer link: <https://perthtasmania.tas.lions.org.au/projects>

The Lions 'Train' Park also provides the most accessible of the three toilet facilities in Perth, as indicated on the National Public

ISSUES

The Lions Park Unit also provides the most accessible of the three toilet facilities in form, as indicated on the National Toilet Map App, which is an Australian Government Continence initiative. Refer: <https://toiletmap.gov.au> and this location is the site for several very popular Mobile food vans, which of course generate additional traffic.

Visitors tend to utilise the on street / kerb parking area in Little Mulgrave Street as there is welcome shade from the trees during summer and trucks, mobile homes and caravans have adequate room for parking, plus there is no need for Northbound users to cross the busy road.

Unfortunately this popularity has caused problems with vehicular accessibility in Little Mulgrave Street, with regular use by camper vans, tour buses, caravans, trucks and semi trailers who do not wish to park on the Main Road. However the vehicles that do park on the Main Road contribute to the difficulties with sight lines when trying to re enter the Main Road from Little Mulgrave Street.

Refer: Traffic Impact Assessment Guidelines August 2020 - Traffic capacity analysis
<https://www.transport.tas.gov.au/?a=108491Thpp>

The completion of the Perth Bypass has alleviated some traffic, however the introduction of additional traffic from the development will again create impediments.

There is no historic traffic count data for Little Mulgrave Street, therefore an estimate only has been provided in the Traffic Impact Assessment supplied by RJK Consulting Engineers for Harley Commercial Ltd, the site developers and of course the expected traffic generation by residents in the new development are also estimates only.

The 15 proposed units each have a 2 car garage and the availability of 2 additional resident / visitor spaces, this equates to a possibly higher volume of traffic, without taking into consideration taxis, municipal and commercial vehicles.

Movement for service vehicles should be restricted for use between 7.00am and 7.00pm Monday to Friday and 8.00am to 6.00pm Saturday and Sunday to avoid unreasonable impact on the adjacent residents.

There will also be additional exposure to vehicle noise from the driveway utilised by Units 12 - 15 and this has a major impact on No. 7, 11 & 13 Little Mulgrave Street. There will of course also be a resultant loss of privacy and quiet enjoyment for those residents.

14. EMERGENCY VEHICLES

Restricted vehicle access within the development (in particular the access driveway for Units 9 - 15) could affect the time crucial accessibility for Emergency Services i.e. Fire Brigade and Ambulance. Provision should also be made for service and emergency vehicles to safely turn at the end of George Street cul-de-sac in addition to the turning circle located on Little Mulgrave Street. Refer: Austroads Turning templates. <https://austroads.com.au/publications/road-design/ap-g34>

Should there be a fire incident within the development, fire hydrants are located adjacent to Lions Park on the Main Road, near or adjacent to 8 Monica Place, near or adjacent to 39 George Street, and there is an additional hydrant (not recorded) located approximately halfway between the western end and No 45 George Street, there is not a water hydrant located in the proposed access road, Little Mulgrave Street. Even by using access through adjacent properties the hose lay from hydrants may not be adequate, so tanker service may be required. The width of the rear single lane driveway varies from 3.601 to 3.676, with a very restricted turnaround. The smallest tanker in use by Perth Volunteers is 2.5m wide, 5.2m length,

<https://austroads.com.au/publications/road-design/ap-g34>
https://www.planning.tas.gov.au/_data/assets/pdf_file/0006/586896/Northern-Midlands-Council-section-35-report-on-initiation-and-certification-2-August-2019.PDF8

15. LIGHTING / SOLAR / HEATING

The external lighting including flood lights or security lights used on the site should not unreasonably impact on the amenity of adjoining properties and all direct light should be contained or baffled to make sure that it is not emitted outside the boundaries of the site and not be directly visible from a room window of an adjoining dwelling.

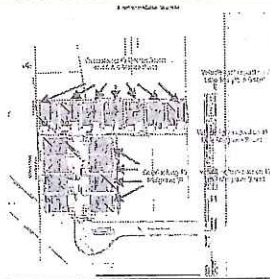
Several properties will also be affected by vehicle headlights of residents, in particular 7, 11, and 13 Little Mulgrave Street will be impacted from traffic using the rear single lane driveway, however additional height fences may rectify this problem.

Also adequate solar access should be maintained for adjacent dwellings, therefore the development building's height and its location on the property should not overshadow the living spaces and solar collectors of neighbouring properties and should not cause a reduction in sunlight to a habitable room (other than a bedroom) or overshadow the private open space of a dwelling on an adjoining lot.

There is also the possible loss of passive solar for living spaces situated along the northern (and, to a lesser extent, western) elevation of a building which require more than three hours of direct sunlight striking the thermal mass to be effective. Is the developer able to provide a shadow diagram not just for the site but one depicting the shadow footprint which impinges on the adjacent properties.

No mention is made of heating within the planning application, there is a concern that fuel burning heaters (wood, pellet, oil) may be retrofitted to units, with subsequent smoke pollution, and problems associated with wood, fuel or pellet storage. Obviously councils do not make inspections of heater installations but do require notification of installation. Are there any restrictions regarding installation, worse case example all 15 units with fuel burning heaters.

ISSUES



16. HOUSEHOLD WASTE

STORAGE - It is noted that individual bin storage areas will be created within the rear al fresco area of each unit, these areas will need to be at least 1.5 m². The site plan indicates that adjacent residents could be affected by several issues, including noise and odour, one property in particular will have 8 bins in close proximity to their boundary fence.

Bin storage is excluded from the area directly in front of dwellings. As a suitable alternative bins could be held in a communal storage area, accessible to all development residents. Regulations dictate that the location should have a setback of at least 4.5m from the frontage; and the area would need to be at least 5.5 m from any dwelling.

To reduce the visual impact on adjacent residents the area could be screened from both the frontage and any dwelling by a wall to a height of at least 1.2 m above the finished surface level.

NOISE - The main sources of noise associated with domestic waste management is the emptying of recyclable materials into individual bins and collection vehicles, also vehicle noise and reversing alarms on collection vehicles. An additional concern with rear storage is the bin-carting route for each dwelling, will there be appropriate surfacing materials that will assist in minimising noise for pathways and driveways that bins will need to be wheeled over.

By relocating bin stores, bays and collection points far enough away from adjacent residents this should reduce the impact of the noise of bin use and waste collection and also ensure that containers require a minimum of movement between the interim storage to the collection point.

ODOUR - problems especially during summer months would need to be minimised by having well-ventilated waste storage areas and the air flowing from storage areas should not exit close to adjacent residences. Ventilation openings should of course be protected against flies and vermin. Enclosed storage is not indicated on the planning application. With the availability of a Communal bin storage area/s a wash down area can be incorporated with access to water (a tap and a hose) and correct drainage to the sewer to assist with reduction of odour.

COLLECTION - Will there only be a bin collection point in Little Mulgrave Street or will there be an additional one in George Street. There needs to be a minimum of 30cm gap in between each bin so that the collection truck arm's can easily pick up, with the possibility of 30 bins and gaps of an indeterminate size, the bins will take up a considerable area adjacent to the kerb and may prove to be inaccessible for collection.

https://www.planning.tas.gov.au/_data/assets/pdf_file/0005/582224/PD4-1_-_Revised_18Jun2014.pdf

Refer: Northern Midlands Council Waste and recycling collection - E11 Environmental Impacts and Attenuation Code
<https://www.northernmidlands.tas.gov.au/source-assets/images/Northern-Midlands-Interim-Planning-Scheme-2013-last-updated-18-March-2021-amendment-01-2020-and-permit-PLN-20-0001-002.pdf>

Refer: Northern Midlands Council Waste and recycling collection
<https://www.northernmidlands.tas.gov.au/living/waste-management/waste-and-recycling-kerbside-collection>

17. SITE PLAN

It has been noted that the number of units has been reduced from the standard regulation density level of 325sqm (20 units) to 446sq (15 units), this appears to be indicative not of a concern regarding the density of units in the area. It is more than likely driven by a commercial decision, to provide lots with areas and dimensions that enable the appropriate siting and construction of a 3 bedroom, 2 car garage dwelling with private open space, vehicle access / maneuverability and additional site features. Listed on realestate.com for \$435,000.

There are currently several such developments within Northern Tasmania all of which have these dimensions, it appears as though Real Estate demand and the resultant exponential increase in revenue has dictated this amendment. 325sqm would only be suitable for 2 bed, 1 bath, 1 garage unit similar to the Marlborough Street, Longford development which had a sell price of \$315,000

https://www.planning.tas.gov.au/_data/assets/pdf_file/0005/582224/PD4-1_-_Revised_18Jun2014.pdf
https://www.planning.tas.gov.au/_data/assets/pdf_file/0006/586896/Northern-Midlands-Council-section-35-report-on-initiation-and-certification-2-August-2019.PDF
<https://www.realestate.com.au/property-house-tas-perth-135085930>

There are significant concerns with the density of the proposed site, with major concerns regarding the proximity of Unit 1 to the Rail area, the extremely narrow single lane access to Units 9 - 15, difficulties with access for Emergency vehicles, lack of undeveloped land for runoff, concerns regarding appropriate waste storage, and loss of privacy for adjacent residents.

18. LANDSCAPING

ISSUES

Because of disruption during construction it would be expected and confirmation required that any new nature strips, or areas of existing nature strip that are disturbed, will require remedial work, to be topped with good quality topsoil and sown with grass.

The Landscape Concept Plan does not indicate areas allocated for clothes drying, which could have a negative impact on visual aesthetics for development owners and adjacent residents, confirmation is required that appropriate facilities and locations will be defined.

19. ANIMALS

Obviously animals should be kept in accordance with the Northern Midlands Council and Dog Control Act requirements and will also be dependant on the distance to the closest dwelling. Suitable facilities and shelters should be provided, guidelines for the size, layout and construction of animal shelters are produced by the animal welfare organisations and the design should be primarily dictated by the available outdoor area. Refer: <https://www.northernmidlands.tas.gov.au/living/animals/dog-control> and Dog Control Act <https://www.legislation.tas.gov.au/view/html/inforce/current/sr-2010-039>

There is concerns that any animals located within the development may create unsanitary or unhealthy conditions, attract vermin and insects i.e flies, lice or fleas, create offensive noise and odours and create waste disposal problems, (both liquid and solid) all of which will have a detrimental effect on the development residents and existing local residents.

This is not addressed in the planning application, therefore clarification is required if restrictions will be included in the Body Corporate (if applicable) to mitigate what could be a potential problem.

20. HERITAGE

NORTHERN MIDLANDS Interim Planning Scheme 2013
E13-2 Local Heritage Places Outside Precincts.

1 Little Mulgrave Street, Perth No. 363 Cottage NMC NT THR
3 Little Mulgrave Street, Perth No. 362 Cottage NMC NT THR

The following abbreviations indicate the source of the Place's recognition status at the time of compiling the consolidated list [2007]:

NMC Northern Midlands Council Planning Scheme 1995 Schedule RNE Register of the National Estate
<https://www.northernmidlands.tas.gov.au/source-assets/images/Northern-Midlands-Interim-Planning-Scheme-2013-last-updated-18-March-2021-amendment-01-2020-and-permit-PLN-20-0001-002.pdf>

NT National Trust of Australia Register
<https://www.nationaltrust.org.au>

THR Tasmanian Heritage Register
https://heritage.tas.gov.au/Documents/THR_Data_2019-08-27_PdfOnline.pdf

These properties have relevance with regard to setback distances, as they are referenced in the planning application to justify the amendments to the legal setback distances for Unit 1 and 5 from 4.5m to 3m. The properties located in Little Mulgrave Street all complied with the appropriate regulations in existence at the time of construction.